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**Town Planning**  
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## **TOWN AND COUNTRY PLANNING ACT 1990**

### **Notice of Decision on Hybrid Planning Application P/2022/0212/HYBR**

- Proposal:** Hybrid planning application seeking:
- Full planning permission and permission for relevant demolition in a conservation area for proposed demolition and site preparation works; and
  - Outline planning permission for development of a mix of uses, comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community & learning uses (Use Class F1(b-e) and F2(b)); and Sui Generis uses, with associated access, servicing, parking, public realm and landscaping, with all matters (Access, Appearance, Landscaping, Layout and Scale) reserved for future determination.
- Location:** Land Bound By Corporation Street To The North, St Helens Central And Rail Lines To The East, St Helens Canal To The South And The Town Centre, Broadly Defined By Bickerstaffe Street And Market Street, To The West.,

### **DECISION:**

**GRANT FULL PERMISSION FOR:** relevant demolition in a conservation area for proposed demolition and site preparation works;

Subject to the Following Conditions:

1. The works hereby permitted must be begun within 3 years of the date of this decision notice.
2. The demolition works hereby approved shall be carried out in accordance with the following plans and documents:
  - Site Location Plan MPS\_04\_1100 Rev. A
  - Demolition Plan MPS\_04\_1211 Rev. C
  - Existing Elevations to be Demolished (Sheet 1) MP\_07\_1100 Rev. B
  - Existing Elevations to be Demolished (Sheet 2) MP\_07\_1101 Rev. B
  - Existing Elevations to be Demolished (Sheet 3) MP\_07\_1102 Rev. B

**Signed:** 

**Dated:** 31/03/2023

3. No part of the demolition works hereby approved shall commence until a drawing identifying the phases of demolition / development for the entire site has been submitted to and approved in writing by the Local Planning Authority.

The demolition works shall be carried out in accordance with the approved phasing drawing unless otherwise agreed in writing.

4. No phase of the demolition works hereby approved shall commence until a detailed scheme for that phase (as defined by Condition 3) indicating procedures, measures and mitigation for the management of flood risk assets and flood water routing during and after demolition has been submitted to and approved in writing by the Local Planning Authority.
5. No phase of the demolition works hereby approved (as defined by Condition 3), shall commence until a Tree Protection Plan for the retained trees within the relevant demolition phase (as shown in Arboricultural Impact Assessment in Support of Hybrid Planning Application July 2022, Document Ref 9228.001. Version 5) has been submitted to and approved in writing by the Local Planning Authority. The tree protection shall be installed in accordance with timescales submitted within the Tree Protection Plan.
6. Prior to the commencement of each phase of demolition hereby approved (as defined by Condition 3) a Demolition Management Plan comprehensively detailing the phasing and logistics of demolition for that phase shall be submitted to and approved in writing by the Local Planning Authority. The Demolition Management Plan shall include, but not be limited to:

Contractors and associated works vehicles traffic routes, including provision for access to the site;  
Entrance / exit from the site for visitors / contractors / deliveries;  
Siting of temporary containers;  
Parking for contractors;  
Identification of working space and extent of areas to be temporarily enclosed and secured during each phase of demolition;  
Temporary road / areas of hard standing;  
Schedule and temporal restrictions for large vehicles delivering / exporting materials to and from site;  
Waste management;  
Details of street sweeping / street cleansing / wheel wash;  
Details of measures to minimise the spread of airborne dust;  
Details of any temporary generators;  
Hours of working;  
Phasing of works;  
Details of including any interim measures for any displaced car, cycle parking and taxi ranks; Details of including any interim measures for public access and service areas for adjacent buildings and sites during the times of the demolition;  
Details of turning facilities for use by construction / demolition vehicles; and  
The plan shall also include interim measures, where necessary, for the continued operation of traders during the demolition.

The demolition shall be carried out in full accordance with the approved Demolition Management Plan, unless otherwise agreed in writing with the Local Planning Authority.

7. No tree felling, or demolition works are to take place during the period 1 March to 31 August inclusive. If it is necessary to undertake works during the bird breeding season, then all buildings and trees are to be checked first by an appropriately experienced ecologist to ensure no breeding birds are present. If present, details of how they will be protected are required to be submitted for approval in advance of works taking place.



8. No part of the demolition of 39-41 Hall Street hereby approved shall take place until a Written Scheme of Investigation (WSI) for a programme of historic building investigation works of these properties has been submitted to and approved in writing by the Local Planning Authority. The WSI must include the following:
- A programme and methodology of site investigation and recording.
  - A programme for post-investigation reporting to include production of a final report of the significance of the historic building.
  - Provision for appropriate publication and dissemination of the archaeology and history of the historic building.
  - Provision for archive deposition of the report and records of the site investigation.
  - Nomination of a competent person or persons / organisation to undertake the works set out within the approved WSI.
9. Buildings with bat roost potential shall be demolished during the winter months November to February inclusive. If this is not possible a licensed bat ecologist is required to directly supervise the roof slates and other potential roost features as set out within the submitted Ecological Assessment version 1.4, TEP July 2022.
10. Prior to the commencement of each phase of demolition hereby approved (as defined by Condition 3) a Local Employment Scheme for the phase of demolition works shall be submitted to and approved in writing by the Local Planning Authority. The submitted Local Employment Scheme shall demonstrate how the development will use all reasonable endeavours to recruit labour from within the Borough of St Helens focusing on the most deprived Super Output Areas and comply with the Councils Construction Charter.

The Scheme shall include the following:

- a. Details of how the initial staff/employment opportunities will be advertised and how liaison with the Council and other bodies will take place in relation to maximising the access of the local workforce to information about employment opportunities;
- b. Details of how sustainable training opportunities will be provided for those recruited to fulfil staff/employment requirements including the provision of apprenticeships or an agreed alternative;
- c. A procedure setting out criteria for employment, and for matching of candidates to the vacancies;
- d. Measures to be taken to offer and provide college and/or work placement opportunities to students within the locality;
- e. Details of the promotion of the Local Employment Scheme and liaison with contractors engaged in the approved demolition works to ensure that they also apply the Local Employment Scheme so far as practicable having due regard to the need and availability for specialist skills and trades and the programme for the demolition of the development;
- f. A procedure for monitoring the Local Employment Scheme and reporting the results of such monitoring to the Council including details of the origins qualifications numbers and other details of candidates; and,
- g. A timetable for the implementation of the Local Employment Scheme

## REASONS:

1. Section 91 (as amended) of the Town and Country Planning Act 1990.
2. For the avoidance of doubt.
3. To ensure that the development does not detrimentally affect the amenity of neighbouring residents occupiers of commercial properties by causing unacceptable disturbance and restrictions and in the interest of highway safety in accordance with Policies LPD01 and LPA06 of St Helens Local Plan
4. To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution in accordance with Policy LPC12 of the St Helens Local Plan
5. To protect existing trees in accordance with Policy LPC10 of the St Helens Local Plan
6. To ensure that the development does not detrimentally affect the amenity of neighbouring residents occupiers of commercial properties by causing unacceptable disturbance and restrictions and in the interest of highway safety in accordance with Policies LPD01 and LPA06 of St Helens Local Plan.
7. To protect ground breeding bird habitat in accordance with Policy LPC06 of the St Helens Borough Local Plan.
8. To allow a record of the historic feature of the s buildings to be demolished to be recorded in accordance with Policy LPC11 of the St Helens Local Plan
9. To protect bats in accordance with Policy LPC06 of the St Helens Borough Local Plan.
10. To increase local economic activity in accordance with Policy LPA03 of the St Helens Borough Local Plan.

**GRANT OUTLINE PERMISSION FOR: development of a mix of uses, comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community & learning uses (Use Class F1(b-e) and F2(b)); and Sui Generis uses, with associated access, servicing, parking, public realm and landscaping, with all matters (Access, Appearance, Landscaping, Layout and Scale) reserved for future determination.**

Subject to the Following Conditions:

11. Application for approval of the first reserved matters must be made not later than the expiration of three years beginning with the date of this permission. Application for the last approval of reserved matters must be made within seven years of the date of this permission. The development relevant to each reserved matter must be begun not later than the expiration of two years from the approval of that reserved matter
12. Before any phase of the development hereby granted outline permission is commenced, approval shall be obtained from the Local Planning Authority with respect to the reserved matters for that phase, namely (the layout, scale, appearance, access, and landscaping including details of landscape management and maintenance (hereinafter called "the reserved matters")). For clarification phases are those as defined by Condition 16.
13. The development hereby approved in outline shall be carried out in accordance with the following plans and documents:

Site Location Plan MPS\_04\_1100 Rev. A

Parameters Plan Maximum Development Heights MPS\_04\_2212 Rev. L

Parameters Plan Minimum Development Heights MPS\_04\_2213 Rev. E

14. This planning permission is for a maximum of 423 residential units.
15. Notwithstanding the provisions of the Town and Country Planning (Use Classes) Order 2021 (as amended) or any subsequent Orders or statutory provision re-enacting the provisions of these Orders, no units shall be permitted to change use from its first use, except with the prior grant of a further planning permission.
16. The first reserved matter(s) application(s) shall include a drawing identifying the phases of development / demolition for the entire site.

All reserved matter application(s) (including the first) should identify which phase(s) of the development/demolition they are within and where appropriate an updated plan which includes details of which reserved matters applications for each phase have been approved.

All phasing within supporting reports and plans submitted with all reserved matters applications should be in accordance with the submitted drawing.

All reserved matter application(s) should relate to entire phases of development, and not parts of phases of development. To clarify more than one entire phase of development can be included within the same reserved matter application(s).

17. No above ground construction of any phase (as defined by Condition 16) shall commence until a detailed materials schedule for that phase has been submitted to and approved in writing by the Local Planning Authority. The schedule shall include a strategy for the entire site including precise details of any materials previously approved as part of this application. Details of materials of adjacent sites shall also be included for context and to allow a full assessment.
18. All reserved matters application(s) for layout, scale, access, and landscaping shall be accompanied by details of the proposed ground and floor levels for the relevant phase (as defined by Condition 16), including the finished floor levels of all buildings previously approved as part of this application. The submitted details shall include a number of sections across the site, which shall indicate existing and proposed ground levels, together with the finished floor levels of any proposed dwellings/buildings through which the sections run and shall extend beyond the site boundaries to include any surrounding adjacent properties. The development shall thereafter be implemented in accordance with the approved details.
19. All the reserved matters for layout, scale, access, and landscaping shall be accompanied by a scheme(s) identifying areas of motor vehicular parking, cycle parking, service areas and access routes, taxi facilities, drop-off and pick-up areas within the relevant phase (as defined by Condition 16).

The scheme(s) shall include but not be limited to the following:

- a. A strategy for all parking provision across the entire site;
- b. Details of location;
- c. Details of access;
- d. Details of drainage, surfacing and markings; and
- e. A Management Strategy identifying phasing and triggers for the implementation of parking, designated spaces / areas for traders and the public, and measures to be implemented to enforce the strategy.

The areas and facilities so provided for the relevant phase (as defined by Condition 16) shall be installed, managed, and retained in accordance with the approved scheme.

20. All the reserved matter(s) applications for layout, scale, access, and landscaping shall be accompanied by a detailed highway scheme for that phase (as defined by Condition 16). The scheme shall include but not be limited to the following:
- a. The proposed highway layout including the highway boundary
  - b. A timetable including triggers for the implementation of all the proposed works
  - c. Dimensions of all existing and proposed carriageway, cycleway, footway, and verges including details of any proposed alterations
  - d. Details of existing and proposed visibility splays
  - e. The proposed buildings and site layout, including levels and vehicular and pedestrian access
  - f. Cycle infrastructure (lanes, crossings)
  - g. Pedestrian crossings
  - h. All types of existing and proposed surfacing (including tactile paving), kerbing and edging
  - i. Full working drawings for any structures which affect or form part of the highway network
  - j. Details of any carriageway reallocation
  - k. Highway arrangements and raised tables
  - l. Delivery and servicing arrangements
  - m. Speed reductions (and any necessary locations for new Traffic Regulation Orders)
  - n. An updated Transport Statement relevant to that phase
21. Prior to the first use or occupation of any building, an Interim Travel Plan shall be submitted to and approved in writing by the Local Planning Authority.

Following approval of the Interim Plan each individual occupier that employs 10 or more persons shall, prior to occupation, appoint a Travel Plan Coordinator. The Travel Plan Coordinator shall be responsible for the implementation, delivery, monitoring and promotion of the Travel Plan, including the day-to-day management of the steps identified to secure the sustainable transport initiatives. The details (name, address, telephone number and email address) of the Travel Plan Coordinator shall be notified to the Council as Local Planning Authority upon appointment and immediately upon any change.

Within six months of occupation, each Travel Plan Coordinator shall submit their own Travel Plan to the Local Planning Authority for approval and the approved plans shall thereafter be implemented within one month of approval by the Local Planning Authority.

The Travel Plan(s) shall include immediate, continuing, and long-term measures to promote and encourage alternative modes of transport to the single-occupancy car. For the avoidance of doubt, the travel plan shall include but not be limited to:

- a. Production of SMART targets to reduce single-occupancy car traffic movements to and from the respective site/planning unit;
- b. Updating of timetables and fare information on a regular basis; and,
- c. Involvement of employees;
- d. Information on existing transport policies, services and facilities, travel behaviour and attitudes;
- e. Access by all modes of transport;
- f. Resource allocation including Travel Plan Coordinator appointment and confirmation of the name holder and budget;
- g. A marketing and communications strategy;
- h. Appropriate measures and actions to reduce car dependence and encourage sustainable travel;
- i. An action plan including a timetable for the implementation of each such element of the above; and
- j. Mechanisms for monitoring, reviewing, and implementing the travel plan.

The approved Travel Plan(s) shall be implemented in accordance with the timetable contained therein and shall continue to be implemented as long as any part of the development is occupied and in use.

An Annual Report shall be submitted to the Council no later than 1 month following the anniversary of the first occupation of the development for a period of 5 years. The Annual Report shall include a review of the travel plan measures, monitoring data and an updated action plan.

22. All the reserved matter application(s) for layout, scale, access, and landscaping shall be accompanied by a detailed drainage scheme for the relevant phase (as defined by Condition 16). No development (excluding demolition) shall take place until a scheme for the disposal of foul and surface water from the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The approved development shall be constructed in accordance with the approved scheme. The scheme shall be based on the hierarchy of drainage options contained in the Planning Practice Guidance and St Helens Council adopted SuDS Guidance, where relevant, shall demonstrate compliance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national and local standards. The scheme shall include but not be restricted to the following:
- a. A drainage strategy referencing and using the Councils SuDS Guidance and Checklist as a base which takes account of adjacent phases.
  - b. Assessment of possible improvement opportunities to the drainage run-off and storm water storage for the Town Centre and downstream water catchment.
  - c. Assessment of the SuDS Drainage Hierarchy
  - d. Evidence of an assessment of the site conditions to include site investigation (including contamination) and test results to confirm drainage outfall methods and infiltrations rates if directed to ground or other permeable features.
  - e. Information about the lifetime of the development design storm period and intensity (1 in 30 and 1 in 100 year, plus allowance for climate change and urban creep), discharge rates and volumes (both pre and post development), temporary storage facilities, the methods employed to delay and control surface water discharged from the site, and the measures taken to prevent flooding and pollution of the receiving groundwater and/or surface waters, including watercourses.
  - f. Any works required off-site to ensure adequate discharge of surface water without causing flooding or pollution (which should include refurbishment of existing culverts and headwalls or removal of unused culverts where relevant).
  - g. Flood water exceedance routes, both on and off site.
  - h. Detailed design drawings including; details of inlets, outlets, and flow controls / long and cross section drawings of proposed drainage system(s), including design levels / details of appropriate water quality treatments; Designs to be accompanied with relevant storm water simulations assessments (both report form and electronic software files used).
  - i. A timetable for implementation, including phasing as applicable.
  - j. Detailed construction plan scheme must be provided indicating procedures and measures for the management of flood risk assets and surface water flood outting and storm exceedance routes during construction.
  - k. Details of a management and maintenance plan for the drainage system after completion, including any arrangements for adoption by an appropriate public body or statutory undertaker
23. No part of the development hereby permitted in outline shall commence until a Phase 2 site investigation and risk assessment for the relevant phase (as defined by Condition 16) has been submitted to and approved in writing by the Local Planning Authority.

The assessment shall include a strategy for addressing piling or any other foundation designs using penetrative methods, if proposed.

Should the site investigation and risk assessment identify any remediation requirements then a remedial strategy including a validation methodology, should be included.

If any remediation works are required, then prior to the first use/ occupation of any part of the building sited within the defined area of remediation a validation/closure report shall be submitted and approved in writing by the Local Planning Authority.

All such reports shall be completed by a competent person in accordance with government and Environment Agency guidance, namely "Land Contamination: Risk Management" (<https://www.gov.uk/guidance/land-contamination-how-to-manage-the-risks>).

24. All the reserved matter application(s) for layout and access shall be accompanied by a coal mining report for the relevant phase (as defined by Condition 16), if applicable. The report shall include a scheme of intrusive site investigations, designed by a competent person and adequate to properly assess the ground conditions on the site and establish the risks posed to the development by past coal mining activity (shallow mining and mine entries).

No part of the development hereby permitted in outline shall commence until a report of findings arising from the intrusive site investigations and any remedial and / or measures necessary, including the submission of the proposed layout plan which identifies the location of any on-site mine entries (if found present) including appropriate zones of influence for all mine entries, and the definition of

suitable 'no-build' zones, associated with the relevant phase (as defined by Condition 16), has been submitted to and approved in writing by the Local Planning Authority. The report shall include a timetable for the implementation of the remedial works.

25. All reserved matters application(s) for layout or scale shall include a Renewable and Low Carbon Energy Statement which details how the proposal will minimise carbon emissions equivalent to CSH level 4, i.e. 19% carbon reduction against Part L 2013 for the relevant phase (as defined by Condition 16). The development thereafter shall be carried out in accordance with the approved details
26. All reserved matter(s) applications for scale, layout and appearance and access shall be accompanied by updated noise assessments for the relevant phase (as defined by Condition 16).
27. All reserved matter application(s) for appearance for any building that may include a commercial kitchen shall evidence how the design of the building can include provision for external flues/ventilation.
28. All reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by a schedule for the hours of operation, servicing, and deliveries of the relevant building/phase (as defined by Condition 16). The proposal shall be operated in full accordance with the approved scheme.
29. Prior to any above ground works hereby approved a scheme for the signage strategy for all buildings within the relevant phase (as defined by Condition 16) shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be fully implemented and retained at all times unless otherwise approved in writing by the Local Planning Authority.

The scheme may need to be accompanied by an application for advertisement consent.

30. All the reserved matters application(s) for layout, scale, access, appearance, and landscaping shall be accompanied by details of any proposed lighting within the relevant phase (as defined by Condition 16). Details submitted should relate to existing and proposed lighting within the phase and should include but not be limited to the following:
- a. A strategy for the entire application site
  - b. Detailed lighting designs;
  - c. Luminaire technical details;
  - d. Luminaire locations;
  - e. Illuminance levels;
  - f. Lighting standard achieved;

- g. Control methods; and
- h. Proposed times of operation of lighting
- i. Details of timescales/triggers for installation

The scheme shall be installed and retained in accordance with the approved scheme.

31. Details of any external CCTV to be installed shall be submitted with the reserved matter application(s) for layout, scale, access, appearance, and landscaping of the relevant phase (as defined by Condition 16). The scheme shall include details of location and appearance and should be plotted on drawings that show existing and proposed streetlighting and trees.

The approved scheme shall be fully implemented at all times the development is in operation.

32. All the reserved matter application(s) for layout, access and appearance shall be accompanied by a scheme for waste management and storage for the relevant phase (as defined by Condition 16). The scheme shall relate to public and commercial waste and shall include but not be limited to the following:
- a. A strategy for the entire site
  - b. Location of proposed and existing refuse bins/ storage areas
  - c. Design of bins/ means of enclosure of storage areas
  - d. Details of collection points
  - e. Strategy for collection of waste

The approved scheme shall be fully implemented at all times the development is in operation.

33. All reserved matter application(s) for landscaping for each phase of the development hereby permitted (as defined by Condition 16) and any reserved matter application(s) for layout and scale that includes residential units within the phase shall be accompanied by a Public Open Space Strategy. The Strategy shall include but not be restricted to details of management and maintenance, phasing, and triggers for implementation
34. All the reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by a tree survey and Arboricultural Impact and Method Statement, relevant to that phase (as defined by Condition 16).
35. All the reserved matters application(s) for layout and landscaping for each phase of the development hereby permitted (as defined by Condition 16) must include an updated Biodiversity Net Gain Assessment, including up to date DEFRA Biodiversity Net Gain Metric, which demonstrates that there is no net loss of biodiversity across the site as a result of the proposals within any reserved matters application, relevant to that phase (as defined by Condition 16).
36. All the reserved matters application(s) for layout and landscaping must include a Precautionary Working Method Statement (PWMS) which incorporates prevention and mitigation measures for protected species and retained habitats.
37. All the reserved matters applications for siting and landscaping shall be accompanied by a Method Statement for the removal/control of any invasive plant species (as defined within the Wildlife and Countryside Act 1981, as amended) which fall within the application site. Development shall be in accordance with the Method Statement approved. The Method Statement shall include:
- a. A plan showing the location of the invasive species, the plan should include the boundaries of the phases (as defined by Condition 16).
  - b. The methods that will be used to prevent the plant/s spreading further, including demarcation.
  - c. The methods of control that will be used, including details of post-control monitoring and
  - d. How the plants will be disposed of after treatment/ removal.

All subsequent reserved matter application(s) for siting and landscaping which contain invasive species shall be accompanied by an updated Method Statement.

38. Prior to occupation, details of the design, number and location of proposed bat and bird boxes relevant to that phase (as defined by Condition 16) and timescales/triggers for installation shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
39. All the reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by an updated Heritage Assessment for the relevant phase (as defined by Condition 16). The updated study shall take account of approved parameters and precise details of all buildings approved as part of this application.
40. No part of the development hereby approved in outline shall commence until a Construction Management Plan comprehensively detailing the phasing and logistics of construction for the relevant phase (as defined by Condition 16) has been submitted to and approved in writing by the Local Planning Authority.

The Construction Management Plan shall include, but not be limited to:

Contractors and associated works vehicles traffic routes, including provision for access to the site;  
Entrance / exit from the site for visitors / contractors / deliveries;  
Siting of temporary containers;  
Parking for contractors;  
Identification of working space and extent of areas to be temporarily enclosed and secured during each phase of construction or associated works;  
Schedule for large vehicles delivering / exporting materials to and from site;  
Waste management;  
Details of street sweeping / street cleansing / wheel wash;  
Details of measures to minimise the spread of airborne dust;  
Details of any piling works and measures to mitigate associated noise;  
Details of any temporary generators;  
Hours of working;  
Phasing of works;  
Details of including any interim measures for any displaced car, cycle parking and taxi ranks;  
Details of including any Interim measures for public access and service areas for adjacent buildings and sites during the times of the construction or associated works operation;  
Details of turning facilities for use by construction vehicles;  
The plan shall also include interim measures, where necessary, for the continued operation of traders during the construction or associated works operation;  
Interim measures for traders parking service areas during times of the construction or associated works operation

The construction and associated works operation shall be carried out in full accordance with the approved Construction Management Plan, unless otherwise agreed in writing with the Local Planning Authority.

41. No phase of construction works, excluding demolition, shall take place until a Written Scheme of Investigation (WSI) for a programme of archaeological works for the relevant phase (as defined by Condition 16) has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
42. All reserved matters application(s) for layout, scale and landscaping shall be accompanied by an updated Wind Assessment



43. All reserved matters application(s) for layout, scale landscaping and access shall include an updated Air Quality Report for the relevant phase (as defined by Condition 16) setting out any necessary mitigation measures. Any mitigation identified as necessary shall be implemented as part of the development hereby approved.
44. Prior to the construction of each phase, a Local Employment Scheme(s) for the construction of the relevant phase (as defined by condition 16) shall be submitted to and approved in writing by the Local Planning Authority. The submitted Local Employment Scheme(s) shall demonstrate how the development will use all reasonable endeavours to recruit labour from within the Borough of St Helens focusing on the most deprived Super Output Areas and comply with the Councils Construction Charter.

Prior to the first use or operation of each of the buildings (excluding residential) further Local Employment Scheme(s) shall be submitted to and approved in writing by the Local Planning Authority.

The Scheme(s) shall include the following:

- a. Details of how the initial staff/employment opportunities at the development will be advertised and how liaison with the Council and other bodies will take place in relation to maximising the access of the local workforce to information about employment opportunities;
- b. Details of how sustainable training opportunities will be provided for those recruited to fulfil staff/employment requirements including the provision of apprenticeships or an agreed alternative;
- c. A procedure setting out criteria for employment, and for matching of candidates to the vacancies;
- d. Measures to be taken to offer and provide college and/or work placement opportunities at the Development to students within the locality;
- e. Details of the promotion of the Local Employment Scheme and liaison with contractors engaged in the construction of the Development to ensure that they also apply the Local Employment Scheme so far as practicable having due regard to the need and availability for specialist skills and trades and

the programme for constructing the development;

- f. A procedure for monitoring the Local Employment Scheme and reporting the results of such monitoring to the Council including details of the origins qualifications numbers and other details of candidates;
  - g. Details of reasonable endeavours to use local suppliers of materials and services in the construction phase(s); and,
  - h. A timetable for the implementation of the Local Employment Scheme.
45. No part of the development which includes residential uses shall be commenced unless and until all interests in that part of the development are bound by a section 106 agreement, relating to the required education contribution, in the form appended to the Section 111 Agreement dated 30<sup>th</sup> March 2023 made between St Helens Metropolitan Borough Council and English Cities Fund (acting by its general partner ECF (General Partner) Limited)

## **REASONS:**

11. Section 91 (as amended) of the Town and Country Planning Act 1990.
12. For the avoidance of doubt.

13. For the avoidance of doubt.
14. For the avoidance of doubt.
15. To allow a full assessment of the potential change in accordance with Policies LPD01 and LPA06 of the St Helens Local Plan
16. To ensure that the development does not detrimentally affect the amenity of neighbouring residents occupiers of commercial properties by causing unacceptable disturbance and restrictions and in the interest of highway safety in accordance with Policies LPD01 and LPA06 of St Helens Local Plan
17. To ensure a quality development in accordance with Policy LPD01 of the St Helens Local Plan.
18. To allow a full assessment of the design of the proposal in accordance with Policies LPD01 and LPA06 of the St Helens Local Plan.
19. To ensure that the development does not detrimentally affect the amenity of neighbouring residents by causing disturbance and in the interest of parking and highway safety in accordance with Policies LPD01 and LPA06 of St Helens Local Plan.
20. In the interest of highway safety in accordance with Policy LPA06 of the St Helens Local Plan.
21. in the interest of sustainable travel in accordance with Policy LPA06 of St Helens Local Plan.
22. To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution in accordance with Policy LPC12 of the St Helens Local Plan
23. To ensure that any contamination is treated to the satisfaction of the Local Planning Authority in accordance with the National Planning Policy Framework and to accord with Policy LPD01 of St Helens Local Plan.
24. To allow a full assessment of the scheme in accordance with the National Planning Policy Framework and to accord with Policy LPD01 of St Helens Local Plan.
25. in the interest of sustainable development and in accordance with Policies LPC13 and LPD01 of St Helens Local Plan.
26. To ensure that the development does not detrimentally affect the amenity of occupiers within and neighbouring the application site in accordance with Policy LPD01 of the St Helens Local Plan.
27. in the interest of the amenity of occupiers of buildings within and neighbouring the application site and visual appearance in accordance with Policy LPD01 of the St Helens Local Plan.
28. To ensure that the development does not detrimentally affect the amenity of neighbouring and proposed residents by causing disturbance and in the interest of parking and highway safety in accordance with Policies LPD01 and LPA06 of the St Helens Local Plan.
29. To ensure that the development has a satisfactory appearance, in accordance with Policy LPD01 of the St Helens Local Plan
30. In the interest of visual amenity and safety in accordance with Policy LPD01 of the St Helens Local Plan.
31. In the interest of visual amenity and safety in accordance with Policy LPD01 of the St Helens Local Plan.
32. In the interest of ensuring a quality development and highway safety in accordance with Policies LPD01 and LPA07 of the St Helens Local Plan.
33. To secure appropriate Open Space in accordance with Policy LPC05 of the St Helens Local Plan.

34. To allow the impact on existing trees to be fully assessed in accordance with Policy LPC10 of the St Helens Local Plan
35. In the interest of Biodiversity in accordance with Policy LPC06 of the St Helens Local Plan.
36. To secure the protection of retained habitats and protected species in accordance with Policy LPC06 of the St Helens Local Plan.
37. to prevent the spread of Invasive species in accordance with Policy LPC06 of the St Helens Local Plan
38. To provide additional habitats for bats and birds within the proposed development in accordance with Policy LPC06 of the St Helens Borough Local Plan.
39. To allow the impact on the George Street Conservation Area and setting of Listed Buildings and Structures to be fully assessed in accordance with Policy LPC11 of the St Helens Local Plan
40. To ensure that the development does not detrimentally affect the amenity of neighbouring residents occupiers of commercial properties by causing unacceptable disturbance and restrictions and in the interest of highway safety in accordance with Policies LPD01 and LPA06 of St Helens Local Plan
41. To allow a record of any archaeological features to be recorded in accordance with Policy LPC11 of the St Helens Local Plan
42. To allow a full assessment of the design of the proposal in accordance with Policies LPD01 and LPA06 of the St Helens Local Plan.
43. to allow a full assessment in accordance with Policies LPD01 and LPD09 the St Helens Local Plan.
44. To increase local economic activity in accordance with Policy LPA03 of the St Helens Borough Local Plan.
45. For the avoidance of doubt and to secure contributions towards education in accordance with Policy LPA07 of the St Helens Local Plan

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Dated: 30<sup>TH</sup> MARCH 2023

- (1) ST HELENS METROPOLITAN BOROUGH COUNCIL
- (2) ENGLISH CITIES FUND (ACTING BY ITS GENERAL PARTNER ECF  
(GENERAL PARTNER) LIMITED)

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Agreement

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Section 111 of the Local Government Act 1972 and section 1 of the Localism Act 2011  
relating to proposed residential development at St Helens town centre

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THIS AGREEMENT is made as a deed on the 30<sup>th</sup> day of March 2023

**BETWEEN:**

- (1) **ENGLISH CITIES FUND** a limited partnership registered in England and Wales under the Limited Partnership Act 1907 under registration number LP007969 whose principal place of business is at One Coleman Street, London, EC2R 5AA acting by its general partner **ECF (GENERAL PARTNER) LIMITED** a company registered in England and Wales under Company Number 04335331 whose registered office is at One Coleman Street, London, EC2R 5AA ("**the Developer**"), and
- (2) **ST HELENS METROPOLITAN BOROUGH COUNCIL** of Wesley House, Corporation Street, St Helens WA10 1HF ("**the Council**")

**BACKGROUND**

- (A) The Council is the Local Planning Authority for the purposes of the Planning Act for the area within which the Obligation Land is situated and is also the owner of the freehold of the Obligation Land
- (B) The Developer is intending to acquire a long leasehold interest in the Obligation Land and has entered into a development agreement to develop the Obligation Land ("**the Development Agreement**").
- (C) The Developer intends to carry out the development of the Obligation Land and has submitted the Planning Application (which includes the Obligation Land and other land) to the Council for that purpose.
- (D) The Council considers and the Developer accepts that the Development on the Obligation Land should not take place without the Developer entering into this Agreement.

**OPERATIVE PROVISIONS:**

1. **DEFINITIONS AND INTERPRETATION**

1.1 In this Agreement the following expressions shall have the following meanings:

"**Commencement of Development**" means the earliest date on which any of the material operations (as defined by Section 56(4) of the Planning Act) pursuant to the implementation of the Development on the Obligation Land is begun save that irrespective of the provisions of Section 56(4) of the Act none of the following operations shall constitute a material operation for the purposes of constituting Commencement of Development on the Obligation Land:

- (i) site investigations or surveys including exploratory boreholes or excavations;
- (ii) site clearance;
- (iii) the demolition of any buildings or structures on the Obligation Land;
- (iv) the construction of temporary site access and service roads;

- (v) the erection of construction fencing or hoarding;
- (vi) the erection of construction accommodation and facilities;
- (vii) works for the provision of drainage or mains services to prepare the Obligation Land for development;
- (viii) works associated with ecological mitigation;
- (ix) the construction of internal site roads and drainage infrastructure serving the Development on the Obligation Land pursuant to the Planning Permission, and
- (x) the construction of off-site highways works and improvements and junction works

and "Commence", "Commencement" and "Commenced" shall be construed accordingly

**"Development"**

those parts of the development comprised in the Planning Application and to be carried out pursuant to the Planning Permission on which the Housing Units are to be constructed

**"Development Agreement"**

the Overarching Development Agreement dated 3 December 2020 entered into between the Council and the Developer relating to the regeneration of St Helens town centre

**"Disposal"**

the grant of a long lease or (if more than one) long leases in the Obligation Land by the Council to the Developer pursuant to the Development Agreement

**"Housing Units"**

that part of the development permitted by the Planning Permission comprising residential development within Use Class C3

**"Obligation Land"**

the land on which the Development is to be carried out shown edged red on the Plan

**"Plan"**

the Plan annexed hereto as **Appendix 1**

**"Planning Act"**

the Town and Country Planning Act 1990 (as amended)

**"Planning Agreement"**

the agreement to be entered into between the parties hereto pursuant to section 106 of the Planning Act in the form annexed as **Appendix 3** (subject to such minor revisions or modifications as are agreed by the parties as being appropriate)

**"Planning Application"**

the planning application submitted to the Council and allocated reference number P/2022/0212/HYBR applying for:

Full planning permission and permission for relevant demolition in a conservation area for proposed demolition works, and

Outline planning permission for development of a mix of uses comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community and learning uses (Use Class F1(b-e) and F2(b)); and sui generis uses, with associated access, servicing, parking, public realm and landscaping with all matters reserved for future determination

**"Planning Permission"**

the planning permission to be granted pursuant to the Planning Application and appended to this Agreement in draft form at **Appendix 2**

**"Use Class"**

the use classes set out in the Town and Country Planning (Use Classes) Order 1987 (as amended)

- 1.2 Words in the singular includes the plural and vice versa;
- 1.3 References to clauses Schedules and paragraphs are references to clauses Schedules and paragraphs in this Agreement except where otherwise specified;
- 1.4 Title headings to the clauses Schedules and paragraphs are for convenience only and shall not affect the interpretation of this Agreement;
- 1.5 References to any statute or statutory instrument shall except where otherwise specifically provided include reference to any statutory modification or re-enactment thereof for the time being in force.

**2. STATUTORY AUTHORITY**

- 2.1 This Agreement is made pursuant to the provisions of section 111 of the Local Government Act 1972 and section 1 of the Localism Act 2011 and all other powers enabling which may be relevant for the purpose of giving validity hereto or facilitating the enforcement of the obligations herein contained with intent to bind the Developer and the Council to observe and perform the obligations or their respective parts as hereinafter contained.

**3. COMMENCEMENT**

- 3.1 The provisions of this Agreement shall have immediate effect upon the completion of this Agreement.

**4. COVENANT BY THE DEVELOPER**

- 4.1 The Developer covenants to provide the Council with:
- 4.1.1 executed engrossments of the Planning Agreement; and
- 4.1.2 authority to complete the same,

in relation to both **clause 4.1.1** and **clause 4.1.2** by all parties to the Agreement save for the Council, together with reasonable and proper fees, the completed undertaking in the form annexed hereto as **Appendix 4** and such ancillary documents necessary to facilitate completion of the Planning Agreement pursuant



to **clause 4.2** of this Agreement, at least five (5) working days prior to completing the acquisition of the Obligation Land (or such other timescale as may be agreed between the parties).

4.2 The Developer covenants that upon completion of the Disposal of the Obligation Land, the Council may complete the Planning Agreement in respect of the Obligation Land.

4.3 The Developer covenants not to permit or allow the Commencement of Development unless and until the Planning Agreement has been completed pursuant to **clauses 4.1 and 4.2** above.

5. **COUNCIL'S COVENANT**

5.1 The Council shall within two working days of the completion of this Agreement grant the Planning Permission.

6. **MISCELLANEOUS**

6.1 Nothing herein contained or implied shall prejudice or affect the rights discretionary powers duties and obligations of the Council under all statutes by-laws statutory instruments orders and regulations in the exercise of its functions as a local planning authority.

6.2 If the Planning Permission is quashed or revoked or otherwise withdrawn or modified by any statutory provision without the consent of the Developer or if the Planning Permission expires this Agreement shall cease to have effect (but without prejudice to the rights of either party against the other in respect of any antecedent breach).

6.3 In the event that any of the provisions of this Agreement are unenforceable this shall not affect the enforceability of the remaining provisions hereof.

6.4 No waiver (whether express or implied) by the Council or any breach or default by the Developer in performing or observing any of the obligations terms or conditions of this Agreement shall constitute a continuing waiver and no such waiver shall prevent the Council from enforcing any of the said obligations terms or conditions or from acting upon any subsequent breach or default in respect thereto by the Developer.

7. **NOTICES**

7.1 All notices requests demands or other written communications to or upon the parties pursuant to this Agreement shall be deemed to have been properly given or made if dispatched by first class recorded delivery letter or facsimile transmission to the party to which such notice request demand or other written communication is to be given or made under this Agreement and addressed as follows:

7.1.1 if to the Council to the Head of Planning (or such other officer for the time being performing those functions);

7.1.2 if to the Applicant to its address specified at the commencement hereof;

7.1.3 or such other address for service as shall have been previously notified to the other party.

7.2 Any notice request or demand or other written communication shall be deemed to have been served as follows:

- 7.2.1 if posted recorded delivery at the expiration of 48 hours after the envelope containing the same was delivered into the custody of the postal authority within the United Kingdom;
- 7.2.2 if sent by facsimile transmission at the time of the successful transmission provided that if any means of service shall be outside normal working hours such service shall be deemed to have taken place upon the day the recipient's office is next open for business;

and in proving such service it shall be sufficient to prove that the envelope containing such notice request demand or other written communication was properly addressed and delivered into the custody of the postal authority in a pre-paid first class or recorded delivery envelope (as appropriate) or that the facsimile was successfully transmitted on a tested line as the case may be.

## 8. DETERMINATION BY EXPERT

8.1 Notwithstanding any specific provision in this Agreement in the event of any dispute between the Developer and the Council concerning this Agreement including any dispute as to whether or not an obligation has been performed or matter to be agreed under any of the provisions of this Agreement the matter may at the written option of any relevant party (notice of which shall be given to the other party or parties) be referred to such expert as they may agree or (in default of agreement within 20 working days of the date of giving of the notice) appointed by the President for the time being of the Law Society whose appointment shall be conducted on the following terms:

- 8.1.1 The person to be appointed pursuant to **Clause 8.1** shall if possible be a person having ten years or more relevant post-qualification experience of the issue in dispute and projects comprising works of the scale and nature of the Development and of the particular issue in dispute.
- 8.1.2 The reference to the expert shall be on terms that:
  - 8.1.2.1 the expert shall afford the parties to the dispute an opportunity to make representations to him/her in writing and if he/she so directs to make submissions on one another's representation;
  - 8.1.2.2 the expert shall be able to stipulate periods of time for the making of such submissions and representations;
  - 8.1.2.3 the expert shall be bound to have regard to the said submissions and representations;
  - 8.1.2.4 the expert shall have the power to award the costs of the determination in favour of either party at the expense of the other in the event that the expert shall consider that the said other party has acted unreasonably and the extent of the costs awarded shall reflect the extent and effect of said unreasonable behaviour;
  - 8.1.2.5 the expert shall be limited in his findings to the proposals put by either party or a proposal falling between both of them; and
  - 8.1.2.6 the findings of the expert shall save in the case of manifest material error be final and binding on the Applicant and the Borough Council save that the parties retain the right to refer to the Courts on a matter of law.

9. **CONTRACTS (RIGHTS OF THIRD PARTIES) ACT 1999**

- 9.1 It is hereby agreed and declared that unless specifically agreed the Contract (Rights of Third Parties) Act 1999 shall not apply to this Agreement.

10. **LEGAL COSTS**

- 10.1 The Developer shall prior to completion of this Agreement pay the Council's legal and administrative costs in connection with the negotiation and completion of this Agreement up to a maximum of £2,250.

11. **COUNTERPARTS**

- 11.1 This Agreement may be executed in counterpart, each of which so executed will be an original, but together will constitute one and the same instrument.

12. **JURISDICTION**

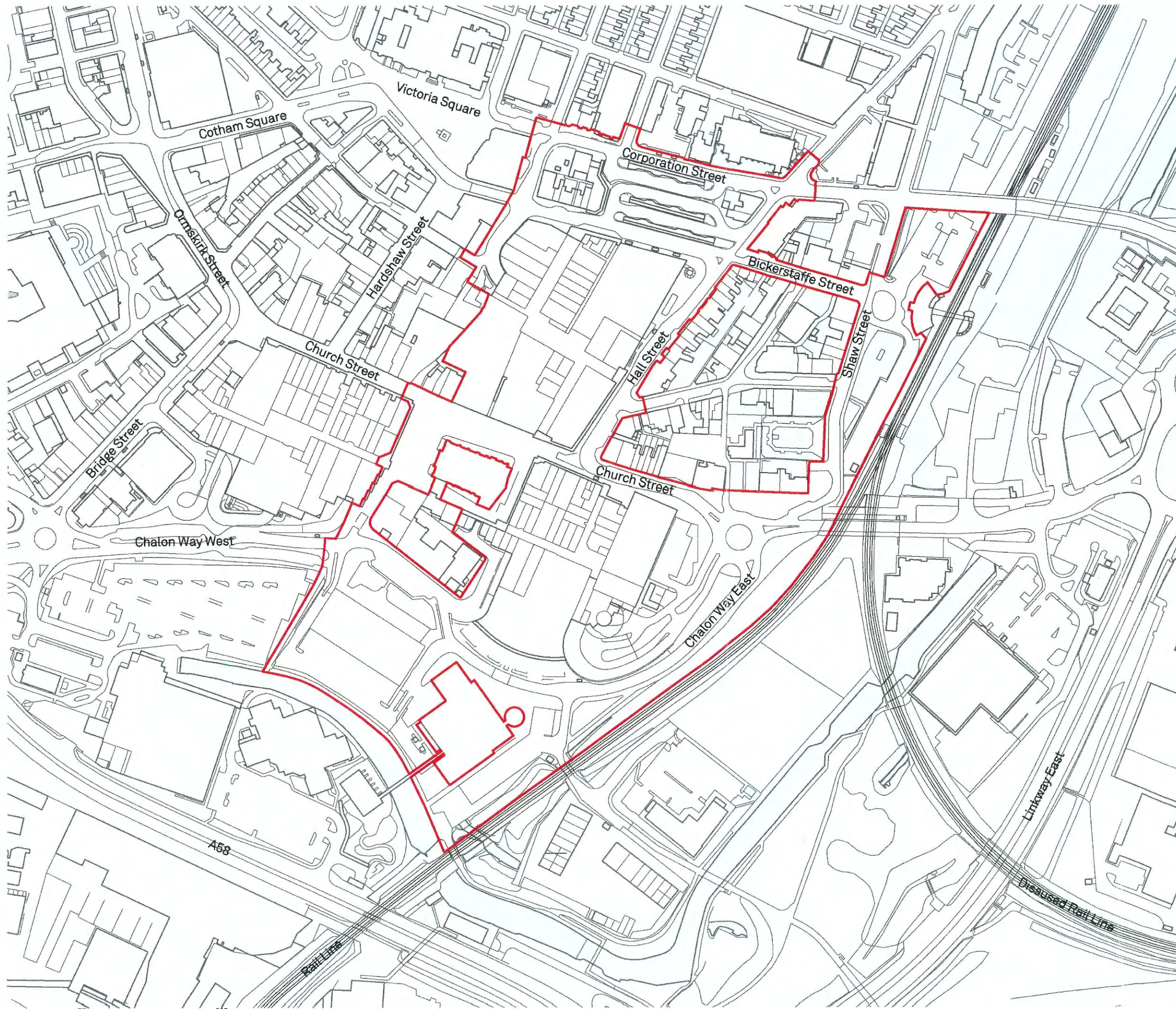
- 12.1 This Agreement is governed by and interpreted in accordance with the Law of England and Wales and the parties submit to the exclusive jurisdiction of the courts of England in relation to any disputes between the parties arising out of or related to this Agreement.

This document is executed as a deed and delivered on the date stated at the beginning of this document.

## **APPENDIX 1**

### **Plan**






Copyright of this drawing is vested in Jon Matthews Architects and it must not be copied or reproduced without written consent. Figured dimensions only are to be taken from this drawing. Do not scale from this drawing.

All Contractors must visit the site and are responsible for taking and checking all dimensions relative to their work. Jon Matthews Architects are to be advised of any variation between drawings and site conditions.

Electronic data/ drawings issued as 'read only' and should not be interrogated for measurement. All dimensions and levels should be 'read only' from those values stated in text, on the drawings.

 CONSTRUCTION - It is considered that the proposed works are within the scope of a competent Contractor and as such no unusual hazards have been identified, refer to relevant Key/Schedule/Designers Risk Assessment

Key Plan:



Key  
 Planning Application Boundary

Rev:	Date:	Details:

**Jon Matthews Architects**  
22 City Tower, Picadilly Plaza, New York St, Manchester, M1 4BT

Project:  
St Helens Masterplan

Title:  
Site Location Plan

Scale:	Origin Date:	Drawn:	Checked:
1:2500 @ A3	19.01.2022	SA	SA

Status:  
Information

Project:	Drawing No:	Rev:
139	MPS_04_1100	A





## **APPENDIX 2**

### **Draft Planning Permission**



**Draft Conditions for P/2022/0212/HYBR**

Grant Full planning permission for relevant demolition in a conservation area subject to the following conditions:

- 1 The works hereby permitted must be begun within 3 years of the date of this decision notice.
- 2 The demolition works hereby approved shall be carried out in accordance with the following plans and documents:
  - Site Location Plan MPS\_04\_1100 Rev. A
  - Demolition Plan MPS\_04\_1211 Rev. C
  - Existing Elevations to be Demolished (Sheet 1) MP\_07\_1100 Rev. B
  - Existing Elevations to be Demolished (Sheet 2) MP\_07\_1101 Rev. B
  - Existing Elevations to be Demolished (Sheet 3) MP\_07\_1102 Rev. B
- 3 No part of the demolition works hereby approved shall commence until a drawing identifying the phases of demolition / development for the entire site has been submitted to and approved in writing by the Local Planning Authority.

The demolition works shall be carried out in accordance with the approved phasing drawing unless otherwise agreed in writing.
- 4 No phase of the demolition works hereby approved shall commence until a detailed scheme for that phase (as defined by Condition 3) indicating procedures, measures and mitigation for the management of flood risk assets and flood water routing during and after demolition has been submitted to and approved in writing by the Local Planning Authority.
- 5 No phase of the demolition works hereby approved (as defined by Condition 3), shall commence until a Tree Protection Plan for the retained trees within the relevant demolition phase (as shown in Arboricultural Impact Assessment in Support of Hybrid Planning Application July 2022, Document Ref 9228.001. Version 5) has been submitted to and approved in writing by the Local Planning Authority. The tree protection shall be installed in accordance with timescales submitted within the Tree Protection Plan.
- 6 Prior to the commencement of each phase of demolition hereby approved (as defined by Condition 3) a Demolition Management Plan comprehensively detailing the phasing and logistics of demolition for that phase shall be submitted to and approved in writing by the Local Planning Authority. The Demolition Management Plan shall include, but not be limited to:
  - Contractors and associated works vehicles traffic routes, including provision for access to the site;
  - Entrance / exit from the site for visitors / contractors / deliveries;
  - Siting of temporary containers;
  - Parking for contractors;



- Identification of working space and extent of areas to be temporarily enclosed and secured during each phase of demolition;
- Temporary road / areas of hard standing;
- Schedule and temporal restrictions for large vehicles delivering / exporting materials to and from site;
- Waste management;
- Details of street sweeping / street cleansing / wheel wash;
- Details of measures to minimise the spread of airborne dust;
- Details of any temporary generators;
- Hours of working;
- Phasing of works;
- Details of including any interim measures for any displaced car, cycle parking and taxi ranks;
- Details of including any interim measures for public access and service areas for adjacent buildings and sites during the times of the demolition;
- Details of turning facilities for use by construction / demolition vehicles; and
- The plan shall also include interim measures, where necessary, for the continued operation of traders during the demolition.

The demolition shall be carried out in full accordance with the approved Demolition Management Plan, unless otherwise agreed in writing with the Local Planning Authority.

- 7 No tree felling, or demolition works are to take place during the period 1 March to 31 August inclusive. If it is necessary to undertake works during the bird breeding season, then all buildings and trees are to be checked first by an appropriately experienced ecologist to ensure no breeding birds are present. If present, details of how they will be protected are required to be submitted for approval in advance of works taking place.
- 8 No part of the demolition of 39-41 Hall Street hereby approved shall take place until a Written Scheme of Investigation (WSI) for a programme of historic building investigation works of these properties has been submitted to and approved in writing by the Local Planning Authority. The WSI must include the following:
  - A programme and methodology of site investigation and recording.
  - A programme for post-investigation reporting to include production of a final report of the significance of the historic building.
  - Provision for appropriate publication and dissemination of the archaeology and history of the historic building.
  - Provision for archive deposition of the report and records of the site investigation.
  - Nomination of a competent person or persons / organisation to undertake the works set out within the approved WSI.
- 9 Buildings with bat roost potential shall be demolished during the winter months November to February inclusive. If this is not possible a licensed bat ecologist is required to directly supervise the roof slates and other potential roost features as set out within the submitted Ecological Assessment version 1.4, TEP July 2022.
10. Prior to the commencement of each phase of demolition hereby approved (as defined by Condition 3) a Local Employment Scheme for the phase of demolition works shall be submitted to and approved in writing by the Local





Planning Authority. The submitted Local Employment Scheme shall demonstrate how the development will use all reasonable endeavours to recruit labour from within the Borough of St Helens focusing on the most deprived Super Output Areas and comply with the Councils Construction Charter.

The Scheme shall include the following:

- a. Details of how the initial staff/employment opportunities will be advertised and how liaison with the Council and other bodies will take place in relation to maximising the access of the local workforce to information about employment opportunities;
- b. Details of how sustainable training opportunities will be provided for those recruited to fulfil staff/employment requirements including the provision of apprenticeships or an agreed alternative;
- c. A procedure setting out criteria for employment, and for matching of candidates to the vacancies;
- d. Measures to be taken to offer and provide college and/or work placement opportunities to students within the locality;
- e. Details of the promotion of the Local Employment Scheme and liaison with contractors engaged in the approved demolition works to ensure that they also apply the Local Employment Scheme so far as practicable having due regard to the need and availability for specialist skills and trades and the programme for the demolition of the development;
- f. A procedure for monitoring the Local Employment Scheme and reporting the results of such monitoring to the Council including details of the origins qualifications numbers and other details of candidates; and,
- g. A timetable for the implementation of the Local Employment Scheme

Grant outline planning permission for development of a mix of uses, comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community & learning uses (Use Class F1(b-e) and F2(b)); and Sui Generis uses, with associated access, servicing, parking, public realm, and landscaping, with all matters (Access, Appearance, Landscaping, Layout and Scale) reserved for future determination. Subject to the following conditions and the completion of a satisfactory Section 106 legal agreement to be approved by the Head of Planning under delegated powers.

- 11 Application for approval of the first reserved matters must be made not later than the expiration of three years beginning with the date of this permission. Application for the last approval of reserved matters must be made within seven years of the date of this permission. The development relevant to each reserved matter must be begun not later than the expiration of two years from the approval of that reserved matter.



- 12 Before any phase of the development hereby granted outline permission is commenced, approval shall be obtained from the Local Planning Authority with respect to the reserved matters for that phase, namely (the layout, scale, appearance, access, and landscaping including details of landscape management and maintenance (hereinafter called "the reserved matters")). For clarification phases are those as defined by Condition 16.
- 13 The development hereby approved in outline shall be carried out in accordance with the following plans and documents:
- Site Location Plan MPS\_04\_1100 Rev. A
  - Parameters Plan Maximum Development Heights MPS\_04\_2212 Rev. L
  - Parameters Plan Minimum Development Heights MPS\_04\_2213 Rev. E
  - Parameters Plan Development Plots MPS\_04\_2209 Rev. I
  - Parameters Plan Ground Floor Uses MPS\_04\_2210 Rev. J
  - Parameters Plan Upper Floor Uses MPS\_04\_2211 Rev. J
  - Development Summary Schedule 08.07.22
- 14 This planning permission is for a maximum of 423 residential units.
- 15 Notwithstanding the provisions of the Town and Country Planning (Use Classes) Order 2021 (as amended) or any subsequent Orders or statutory provision re-enacting the provisions of these Orders, no units shall be permitted to change use from its first use, except with the prior grant of a further planning permission.
- 16 The first reserved matter(s) application(s) shall include a drawing identifying the phases of development / demolition for the entire site.
- All reserved matter application(s) (including the first) should identify which phase(s) of the development/demolition they are within and where appropriate an updated plan which includes details of which reserved matters applications for each phase have been approved.
- All phasing within supporting reports and plans submitted with all reserved matters applications should be in accordance with the submitted drawing.
- All reserved matter application(s) should relate to entire phases of development, and not parts of phases of development. To clarify more than one entire phase of development can be included within the same reserved matter application(s).
- 17 No above ground construction of any phase (as defined by Condition 16) shall commence until a detailed materials schedule for that phase has been submitted to and approved in writing by the Local Planning Authority. The schedule shall include a strategy for the entire site including precise details of any materials previously approved as part of this application. Details of materials of adjacent sites shall also be included for context and to allow a full assessment.
- 18 All reserved matters application(s) for layout, scale, access, and landscaping shall be accompanied by details of the proposed ground and floor levels for the relevant phase (as defined by Condition 16), including the finished floor levels of all buildings previously approved as part of this application. The submitted details shall include a number of sections across the site, which shall indicate





existing and proposed ground levels, together with the finished floor levels of any proposed dwellings/buildings through which the sections run and shall extend beyond the site boundaries to include any surrounding adjacent properties. The development shall thereafter be implemented in accordance with the approved details.

- 19 All the reserved matters for layout, scale, access, and landscaping shall be accompanied by a scheme(s) identifying areas of motor vehicular parking, cycle parking, service areas and access routes, taxi facilities, drop-off and pick-up areas within the relevant phase (as defined by Condition 16).

The scheme(s) shall include but not be limited to the following

- a. A strategy for all parking provision across the entire site;
- b. Details of location;
- c. Details of access;
- d. Details of drainage, surfacing and markings; and
- e. A Management Strategy identifying phasing and triggers for the implementation of parking, designated spaces / areas for traders and the public, and measures to be implemented to enforce the strategy.

The areas and facilities so provided for the relevant phase (as defined by Condition 16) shall be installed, managed, and retained in accordance with the approved scheme.

- 20 All the reserved matter(s) applications for layout, scale, access, and landscaping shall be accompanied by a detailed highway scheme for that phase (as defined by Condition 16). The scheme shall include but not be limited to the following:

- a. The proposed highway layout including the highway boundary
- b. A timetable including triggers for the implementation of all the proposed works
- c. Dimensions of all existing and proposed carriageway, cycleway, footway, and verges including details of any proposed alterations
- d. Details of existing and proposed visibility splays
- e. The proposed buildings and site layout, including levels and vehicular and pedestrian access
- f. Cycle infrastructure (lanes, crossings)
- g. Pedestrian crossings
- h. All types of existing and proposed surfacing (including tactile paving), kerbing and edging
- i. Full working drawings for any structures which affect or form part of the highway network
- j. Details of any carriageway reallocation
- k. Highway arrangements and raised tables
- l. Delivery and servicing arrangements
- m. Speed reductions (and any necessary locations for new Traffic Regulation Orders)
- n. An updated Transport Statement relevant to that phase

- 21 Prior to the first use or occupation of any building, an Interim Travel Plan shall be submitted to and approved in writing by the Local Planning Authority.



Following approval of the Interim Plan each individual occupier that employs 10 or more persons shall, prior to occupation, appoint a Travel Plan Coordinator. The Travel Plan Coordinator shall be responsible for the implementation, delivery, monitoring and promotion of the Travel Plan, including the day-to-day management of the steps identified to secure the sustainable transport initiatives. The details (name, address, telephone number and email address) of the Travel Plan Coordinator shall be notified to the Council as Local Planning Authority upon appointment and immediately upon any change.

Within six months of occupation, each Travel Plan Coordinator shall submit their own Travel Plan to the Local Planning Authority for approval and the approved plans shall thereafter be implemented within one month of approval by the Local Planning Authority.

The Travel Plan(s) shall include immediate, continuing, and long-term measures to promote and encourage alternative modes of transport to the single-occupancy car. For the avoidance of doubt, the travel plan shall include but not be limited to:

- a. Production of SMART targets to reduce single-occupancy car traffic movements to and from the respective site/planning unit;
- b. Updating of timetables and fare information on a regular basis; and,
- c. Involvement of employees;
- d. Information on existing transport policies, services and facilities, travel behaviour and attitudes;
- e. Access by all modes of transport;
- f. Resource allocation including Travel Plan Coordinator appointment and confirmation of the name holder and budget;
- g. A marketing and communications strategy;
- h. Appropriate measures and actions to reduce car dependence and encourage sustainable travel;
- i. An action plan including a timetable for the implementation of each such element of the above; and
- j. Mechanisms for monitoring, reviewing, and implementing the travel plan.

The approved Travel Plan(s) shall be implemented in accordance with the timetable contained therein and shall continue to be implemented as long as any part of the development is occupied and in use.

An Annual Report shall be submitted to the Council no later than 1 month following the anniversary of the first occupation of the development for a period of 5 years. The Annual Report shall include a review of the travel plan measures, monitoring data and an updated action plan.

- 22 All the reserved matter application(s) for layout, scale, access, and landscaping shall be accompanied by a detailed drainage scheme for the relevant phase (as defined by Condition 16). No development (excluding demolition) shall take place until a scheme for the disposal of foul and surface water from the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The approved development shall be constructed in accordance with the approved scheme. The scheme shall be based on the hierarchy of drainage options contained in the Planning Practice Guidance and St Helens Council adopted SuDS Guidance, where relevant, shall demonstrate compliance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement





national and local standards. The scheme shall include but not be restricted to the following:

- a. A drainage strategy referencing and using the Councils SuDS Guidance and Checklist as a base which takes account of adjacent phases.
  - b. Assessment of possible improvement opportunities to the drainage run-off and storm water storage for the Town Centre and downstream water catchment.
  - c. Assessment of the SuDS Drainage Hierarchy
  - d. Evidence of an assessment of the site conditions to include site investigation (including contamination) and test results to confirm drainage outfall methods and infiltrations rates if directed to ground or other permeable features.
  - e. Information about the lifetime of the development design storm period and intensity (1 in 30 and 1 in 100 year, plus allowance for climate change and urban creep), discharge rates and volumes (both pre and post development), temporary storage facilities, the methods employed to delay and control surface water discharged from the site, and the measures taken to prevent flooding and pollution of the receiving groundwater and/or surface waters, including watercourses.
  - f. Any works required off-site to ensure adequate discharge of surface water without causing flooding or pollution (which should include refurbishment of existing culverts and headwalls or removal of unused culverts where relevant).
  - g. Flood water exceedance routes, both on and off site.
  - h. Detailed design drawings including; details of inlets, outlets, and flow controls / long and cross section drawings of proposed drainage system(s), including design levels / details of appropriate water quality treatments; Designs to be accompanied with relevant storm water simulations assessments (both report form and electronic software files used).
  - i. A timetable for implementation, including phasing as applicable.
  - j. Detailed construction plan scheme must be provided indicating procedures and measures for the management of flood risk assets and surface water flood outting and storm exceedance routes during construction.
  - k. Details of a management and maintenance plan for the drainage system after completion, including any arrangements for adoption by an appropriate public body or statutory undertaker
- 23 No part of the development hereby permitted in outline shall commence until a Phase 2 site investigation and risk assessment for the relevant phase (as defined by Condition 16) has been submitted to and approved in writing by the Local Planning Authority.

The assessment shall include a strategy for addressing piling or any other foundation designs using penetrative methods, if proposed.

Should the site investigation and risk assessment identify any remediation requirements then a remedial strategy including a validation methodology, should be included.

If any remediation works are required, then prior to the first use/ occupation of any part of the building sited within the defined area of remediation a validation/closure report shall be submitted and approved in writing by the Local Planning Authority.





All such reports shall be completed by a competent person in accordance with government and Environment Agency guidance, namely "Land Contamination: Risk Management" (<https://www.gov.uk/guidance/land-contamination-how-to-manage-the-risks>).

- 24 All the reserved matter application(s) for layout and access shall be accompanied by a coal mining report for the relevant phase (as defined by Condition 16), if applicable. The report shall include a scheme of intrusive site investigations, designed by a competent person and adequate to properly assess the ground conditions on the site and establish the risks posed to the development by past coal mining activity (shallow mining and mine entries).

No part of the development hereby permitted in outline shall commence until a report of findings arising from the intrusive site investigations and any remedial and / or measures necessary, including the submission of the proposed layout plan which identifies the location of any on-site mine entries (if found present) including appropriate zones of influence for all mine entries, and the definition of suitable 'no-build' zones, associated with the relevant phase (as defined by Condition 16), has been submitted to and approved in writing by the Local Planning Authority. The report shall include a timetable for the implementation of the remedial works.

- 25 All reserved matters application(s) for layout or scale shall include a Renewable and Low Carbon Energy Statement which details how the proposal will minimise carbon emissions equivalent to CSH level 4, i.e. 19% carbon reduction against Part L 2013 for the relevant phase (as defined by Condition 16). The development thereafter shall be carried out in accordance with the approved details.
- 26 All reserved matter(s) applications for scale, layout and appearance and access shall be accompanied by updated noise assessments for the relevant phase (as defined by Condition 16).
- 27 All reserved matter application(s) for appearance for any building that may include a commercial kitchen shall evidence how the design of the building can include provision for external flues/ ventilation.
- 28 All reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by a schedule for the hours of operation, servicing, and deliveries of the relevant building/phase (as defined by Condition 16). The proposal shall be operated in full accordance with the approved scheme.
- 29 Prior to any above ground works hereby approved a scheme for the signage strategy for all buildings within the relevant phase (as defined by Condition 16) shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be fully implemented and retained at all times unless otherwise approved in writing by the Local Planning Authority.

The scheme may need to be accompanied by an application for advertisement consent.

- 30 All the reserved matters application(s) for layout, scale, access, appearance, and landscaping shall be accompanied by details of any proposed lighting within the relevant phase (as defined by Condition 16). Details submitted





should relate to existing and proposed lighting within the phase and should include but not be limited to the following:

- a. A strategy for the entire application site
- b. Detailed lighting designs;
- c. Luminaire technical details;
- d. Luminaire locations;
- e. Illuminance levels;
- f. Lighting standard achieved;
- g. Control methods; and
- h. Proposed times of operation of lighting
- i. Details of timescales/triggers for installation

The scheme shall be installed and retained in accordance with the approved scheme.

- 31 Details of any external CCTV to be installed shall be submitted with the reserved matter application(s) for layout, scale, access, appearance, and landscaping of the relevant phase (as defined by Condition 16). The scheme shall include details of location and appearance and should be plotted on drawings that show existing and proposed streetlighting and trees.

The approved scheme shall be fully implemented at all times the development is in operation.

- 32 All the reserved matter application(s) for layout, access and appearance shall be accompanied by a scheme for waste management and storage for the relevant phase (as defined by Condition 16). The scheme shall relate to public and commercial waste and shall include but not be limited to the following:

- a. A strategy for the entire site
- b. Location of proposed and existing refuse bins/ storage areas
- c. Design of bins/ means of enclosure of storage areas
- d. Details of collection points
- e. Strategy for collection of waste

The approved scheme shall be fully implemented at all times the development is in operation.

- 33 All reserved matter application(s) for landscaping for each phase of the development hereby permitted (as defined by Condition 16) and any reserved matter application(s) for layout and scale that includes residential units within the phase shall be accompanied by a Public Open Space Strategy. The Strategy shall include but not be restricted to details of management and maintenance, phasing, and triggers for implementation.

- 34 All the reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by a tree survey and Arboricultural Impact and Method Statement, relevant to that phase (as defined by Condition 16).

- 35 All the reserved matters application(s) for layout and landscaping for each phase of the development hereby permitted (as defined by Condition 16) must include an updated Biodiversity Net Gain Assessment, including up to date DEFRA Biodiversity Net Gain Metric, which demonstrates that there is no net



loss of biodiversity across the site as a result of the proposals within any reserved matters application, relevant to that phase (as defined by Condition 16).

- 36 All the reserved matters application(s) for layout and landscaping must include a Precautionary Working Method Statement (PWMS) which incorporates prevention and mitigation measures for protected species and retained habitats.
- 37 All the reserved matters applications for siting and landscaping shall be accompanied by a Method Statement for the removal/control of any invasive plant species (as defined within the Wildlife and Countryside Act 1981, as amended) which fall within the application site. Development shall be in accordance with the Method Statement approved. The Method Statement shall include:
- A plan showing the location of the invasive species, the plan should include the boundaries of the phases (as defined by Condition 16).
  - The methods that will be used to prevent the plant/s spreading further, including demarcation.
  - The methods of control that will be used, including details of post-control monitoring and
  - How the plants will be disposed of after treatment/ removal.

All subsequent reserved matter application(s) for siting and landscaping which contain invasive species shall be accompanied by an updated Method Statement.

- 38 Prior to occupation, details of the design, number and location of proposed bat and bird boxes relevant to that phase (as defined by Condition 16) and timescales/triggers for installation shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
- 39 All the reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by an updated Heritage Assessment for the relevant phase (as defined by Condition 16). The updated study shall take account of approved parameters and precise details of all buildings approved as part of this application.
- 40 No part of the development hereby approved in outline shall commence until a Construction Management Plan comprehensively detailing the phasing and logistics of construction for the relevant phase (as defined by Condition 16) has been submitted to and approved in writing by the Local Planning Authority.

The Construction Management Plan shall include, but not be limited to:

- Contractors and associated works vehicles traffic routes, including provision for access to the site;
- Entrance / exit from the site for visitors / contractors / deliveries;
- Siting of temporary containers;
- Parking for contractors;
- Identification of working space and extent of areas to be temporarily enclosed and secured during each phase of construction or associated works;





- Schedule for large vehicles delivering / exporting materials to and from site;
- Waste management;
- Details of street sweeping / street cleansing / wheel wash;
- Details of measures to minimise the spread of airborne dust;
- Details of any piling works and measures to mitigate associated noise;
- Details of any temporary generators;
- Hours of working;
- Phasing of works;
- Details of including any interim measures for any displaced car, cycle parking and taxi ranks;
- Details of including any Interim measures for public access and service areas for adjacent buildings and sites during the times of the construction or associated works operation;
- Details of turning facilities for use by construction vehicles;
- The plan shall also include interim measures, where necessary, for the continued operation of traders during the construction or associated works operation;
- Interim measures for traders parking service areas during times of the construction or associated works operation

The construction and associated works operation shall be carried out in full accordance with the approved Construction Management Plan, unless otherwise agreed in writing with the Local Planning Authority.

- 41 No phase of construction works, excluding demolition, shall take place until a Written Scheme of Investigation (WSI) for a programme of archaeological works for the relevant phase (as defined by Condition 16) has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
- 42 All reserved matters application(s) for layout, scale and landscaping shall be accompanied by an updated Wind Assessment
- 43 All reserved matters application(s) for layout, scale landscaping and access shall include an updated Air Quality Report for the relevant phase (as defined by Condition 16) setting out any necessary mitigation measures. Any mitigation identified as necessary shall be implemented as part of the development hereby approved.
- 44 Prior to the construction of each phase, a Local Employment Scheme(s) for the construction of the relevant phase (as defined by condition 16) shall be submitted to and approved in writing by the Local Planning Authority. The submitted Local Employment Scheme(s) shall demonstrate how the development will use all reasonable endeavours to recruit labour from within the Borough of St Helens focusing on the most deprived Super Output Areas and comply with the Councils Construction Charter.

Prior to the first use or operation of each of the buildings (excluding residential) further Local Employment Scheme(s) shall be submitted to and approved in writing by the Local Planning Authority.

The Scheme(s) shall include the following:



- a. Details of how the initial staff/employment opportunities at the development will be advertised and how liaison with the Council and other bodies will take place in relation to maximising the access of the local workforce to information about employment opportunities;
  - b. Details of how sustainable training opportunities will be provided for those recruited to fulfil staff/employment requirements including the provision of apprenticeships or an agreed alternative;
  - c. A procedure setting out criteria for employment, and for matching of candidates to the vacancies;
  - d. Measures to be taken to offer and provide college and/or work placement opportunities at the Development to students within the locality;
  - e. Details of the promotion of the Local Employment Scheme and liaison with contractors engaged in the construction of the Development to ensure that they also apply the Local Employment Scheme so far as practicable having due regard to the need and availability for specialist skills and trades and the programme for constructing the development;
  - f. A procedure for monitoring the Local Employment Scheme and reporting the results of such monitoring to the Council including details of the origins qualifications numbers and other details of candidates;
  - g. Details of reasonable endeavours to use local suppliers of materials and services in the construction phase(s); and,
  - h. A timetable for the implementation of the Local Employment Scheme.
- 45 No part of the development which includes residential uses shall be commenced unless and until all interests in that part of the development are bound by a section 106 agreement, relating to the required education contribution, in the form appended to the Section 111 Agreement dated [ ] made between St Helens Metropolitan Borough Council and English Cities Fund (acting by its general partner ECF (General Partner) Limited)

## **APPENDIX 3**

### **Draft Planning Agreement**

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**Dated:** 2023

- (1) **ST HELENS METROPOLITAN BOROUGH COUNCIL**
- (2) **ENGLISH CITIES FUND (ACTING BY ITS GENERAL PARTNER  
ECF (GENERAL PARTNER) LIMITED)**
- 

**Agreement**

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Under section 106 Town and Country Planning Act 1990 relating to the  
regeneration of St Helens Town Centre

Application Number P/2022/0212/HYBR

**BETWEEN:**

- (1) **ST HELENS METROPOLITAN BOROUGH COUNCIL** of Wesley House, Corporation Street, St Helens WA10 1HF ("**the Council**"), and
- (2) **ENGLISH CITIES FUND** a limited partnership registered in England and Wales under the Limited Partnership Act 1907 under registration number LP007969 whose principal place of business is at One Coleman Street, London, EC2R 5AA acting by its general partner **ECF (General Partner) Limited** a company registered in England and Wales under Company Number 04335331 whose registered office is at One Coleman Street, London, EC2R 5AA ("**the Developer**")

**(1) BACKGROUND**

- (A) For the purposes of the 1990 Act, the Council is the local planning authority for the area within which the Land is located and the person who is entitled to enforce the obligations contained in this Agreement.
- (B) The Council owns the freehold of the Land.
- (C) The Developer has a long leasehold interest in the Land.
- (D) On 18<sup>th</sup> March 2022 the Developer submitted the Application for the Development to the Council. The Application was validated on the 29<sup>th</sup> March 2022.
- (E) On 27<sup>th</sup> September 2022 the Council resolved to grant the Planning Permission pursuant to the Application subject to the Developer entering into an agreement with the Council in which it agreed to enter into this Section 106 Agreement upon obtaining a long leasehold interest in the Land.
- (F) The parties have agreed to enter into this Agreement with the intention that the obligations contained in this Agreement may be enforced by the Council against the Developer and its respective successors in title in accordance with the terms of this Agreement.

**OPERATIVE PROVISIONS**

**1. INTERPRETATION**

- 1.1 In this Agreement, the following words and expressions have the following meanings:

"1990 Act"	the Town and Country Planning Act 1990
"Application"	the application for part detailed and part outline planning permission for the Development made by the Developer allocated the reference P/2022/0212/HYBR

**"Commencement Date"**

the date of the carrying out of a material operation on the Land as defined in section 56(4) of the 1990 Act but excluding:

- (i) site investigations or surveys including exploratory boreholes or excavations;
- (ii) site clearance;
- (iii) the demolition of any buildings or structures on the Land;
- (iv) the construction of temporary site access and service roads;
- (v) the erection of construction fencing or hoarding;
- (vi) the erection of construction accommodation and facilities;
- (vii) works for the provision of drainage or mains services to prepare the Land for development;
- (viii) works associated with ecological mitigation;
- (ix) the construction of internal site roads and drainage infrastructure serving the Development on the Land pursuant to the Planning Permission, and
- (x) the construction of off-site highways works and improvements and junction works

and "Commence", "Commencement" and "Commenced" shall be construed accordingly

**"Cost of Primary Places"**

the base cost of providing a primary school place as published by the Department for Education in its document "Local Authority School Places Scorecard Costs" (2019) in place at the point of Occupation of the relevant Phase and as may be updated from time to time

<b>"Cost of Secondary Places"</b>	the base cost of providing a secondary school place as published by the Department for Education in its document "Local Authority School Places Scorecard Costs" (2019) being in place at the point of Occupation of the relevant Phase and as may be updated from time to time
<b>"Development"</b>	the development of the Land for Housing Units comprising part of the wider development permitted under the Planning Permission
<b>"Education Contributions"</b>	the payments to be made by the Developer to the Council comprising the Primary Education Contribution and the Secondary Education Contribution in accordance with the provisions in Schedule 3
<b>"Family Houses"</b>	Housing Units with 2 or more bedrooms
<b>"Housing Units"</b>	that part of the development permitted by the Planning Permission comprising residential development within Use Class C3
<b>"Interest"</b>	interest at three per cent (3%) above the base lending rate of the Co-operative Bank
<b>"Land"</b>	that part of the site for which the Planning Permission has been granted on which the Housing Units are to be constructed which is shown in blue on the Plan referenced MPS_04_2209 and being the land against which this Agreement may be enforced
<b>"Local Area"</b>	in the case of primary schools means a radius of 2 miles from the Land and in the case of secondary schools means a radius of 3 miles from the Land
<b>"New Permission"</b>	any planning permission authorising the development of the Land in a manner which would, if such development were carried out, cause the Developer to be in breach of any or all of the provisions contained in this Agreement
<b>"Occupy"</b>	the physical occupation of a Housing Unit in the Development or where the context requires a Housing Unit in any part of the Development but excluding occupation by personnel engaged in construction, marketing, fitting out, testing, security or estate management and the words "Occupation" and "Occupied" shall be construed accordingly

<b>"Parties"</b>	the Developer and the Council
<b>"Phase"</b>	the Development comprised in a Reserved Matters application which includes Housing Units
<b>"Planning Permission"</b>	<p>the planning permission for the Development in the form appended to the Section 111 Agreement for development comprising:</p> <p>Full planning permission and permission for relevant demolition in a conservation area for proposed demolition works, and</p> <p>Outline planning permission for development of a mix of uses comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community and learning uses (Use Class F1(b-e) and F2(b)); and sui generis uses, with associated access, servicing, parking, public realm and landscaping with all matters reserved for future determination</p>
<b>"Plan"</b>	the plan found at Schedule 1 to this Agreement
<b>"Primary Places"</b>	a primary school place in the locality of the Development
<b>"Primary Education Contribution"</b>	means the sum to be determined prior to Occupation of the Housing Units of the relevant Phase based upon the calculation at paragraph 2.2 of Schedule 3
<b>"Reserved Matters"</b>	the matters which have been reserved for subsequent approval and to be submitted pursuant to the Planning Permission
<b>"Section 111 Agreement"</b>	the agreement made under section 111 of the Local Government Act 1972 and section 2 of the Local Government Act 2000 dated [    ] March 2023 entered into by the Developer and the Council in relation to the Planning Permission
<b>"Secondary Places"</b>	a secondary school place in the locality of the Development
<b>"Secondary Contribution"</b>	<p><b>School</b> means the sum to be determined prior to Occupation of the Housing Units of the relevant Phase based upon the calculation at paragraph 2.5 of Schedule 3</p>
<b>"Specialist"</b>	has the meaning given to it in clause 10.2



**"Use Class"**

the use classes set out in the Town and Country Planning (Use Classes) Order 1987 (as amended)

1.2 In this Agreement:

- 1.2.1 the clause headings do not affect its interpretation;
- 1.2.2 unless otherwise indicated, references to Clauses and Schedules are to Clauses of and Schedules to this Agreement and references in a Schedule to a Part or Paragraph are to a Part or Paragraph of that Schedule;
- 1.2.3 references to any statute or statutory provision include references to:
  - 1.2.3.1 all Acts of Parliament and all other legislation having legal effect in the United Kingdom as enacted at the date of this Agreement; and
  - 1.2.3.2 any orders, regulations, instruments or other subordinate legislation made under that statute or statutory provision;
- 1.2.4 unless otherwise expressly stated references to the Land includes any part or parts of it;
- 1.2.5 references to any party in this Agreement include the successors in title of that party. In addition, references to the Council include any successor local planning authority exercising planning powers under the 1990 Act;
- 1.2.6 "include" means "include, without limitation";
- 1.2.7 any covenant by the Developer not to do any act or thing includes a covenant not to permit or allow the doing of that act or thing;
- 1.2.8 if any provision is held to be illegal, unlawful, invalid or unenforceable, the legality, validity and enforceability of the remainder of the Agreement is to be unaffected.

- 1.3 The parties to this Agreement do not intend that any of its terms will be enforceable by virtue of the Contracts (Rights of Third Parties) Act 1999 by any person not a party to it

**2. EFFECT OF THIS AGREEMENT**

- 2.1 The Council is the freehold owner of the Land and it intends that in the event it parts with its interest in the Land it will ensure that the Land is bound by the terms of this Agreement.
- 2.2 The Developer holds the long leasehold interest in the Land which is shown on the Plan at Schedule 1 and described at Schedule 2 to this Agreement and it is intended that its interest is bound by the terms of this Agreement.
- 2.3 This Agreement is made pursuant to section 106 of the 1990 Act and binds the Land. The obligations contained in this Agreement are planning obligations for the purposes of section 106 of the 1990 Act and are enforceable by the Council.

- 2.4 Nothing in this Agreement restricts or is intended to restrict the proper exercise at any time by the Council of any of its statutory powers, functions or discretions in relation to the Land.
- 2.5 This Agreement will be registered as a local land charge in respect of the Land by the Council.
- 2.6 The obligations in this Agreement will not be enforceable against a statutory undertaker after the transfer of the statutory apparatus and any land upon or in which the statutory apparatus is situated by the Developer to that statutory undertaker.
- 2.7 Unless otherwise expressly stated nothing in this Agreement prohibits or limits the right to develop any part of the Site in accordance with a planning permission, other than one relating to the Development as specified in the Application, granted after the date of this Agreement, whether or not pursuant to an appeal.

### **3. COMING INTO EFFECT OF OBLIGATIONS**

- 3.1 Unless otherwise expressly stated the obligations contained in Schedule 3 do not come into effect until the date on which the Development Commences on the Land.

### **4. OBLIGATIONS OF THE PARTIES**

- 4.1 The Developer agrees with the Council to comply with the obligations set out in Schedule 3 in relation to the Development.
- 4.2 The Council agrees with the Developer to comply with its obligations set out in Schedule 4.
- 4.3 The Council agrees with the Developer to act reasonably, properly and diligently in exercising its discretion and discharging its functions under this Agreement. In particular, where any notice, consent, approval, authorisation, agreement or other similar affirmation is required under the terms of the Agreement, the Council will not unreasonably withhold or delay such notice, consent, approval, authorisation, agreement or other similar affirmation.
- 4.4 No person will be liable for any breach of the terms of this Agreement occurring after the date on which they part with their interest in the Land or the part or parts of the Land in respect of which such breach occurs, but they will remain liable for any breaches of this Agreement occurring before that date.
- 4.5 Any payment due from the Developer under this Agreement which is not paid on the due date shall be payable with Interest applied from the due date to the date of actual payment.
- 4.6 The sums stated in this Agreement are exclusive of VAT.

### **5. TERMINATION OF THIS AGREEMENT**

- 5.1 If the Planning Permission shall expire before the Commencement of the Development on the Land or shall at any time be revoked or for any reason cease to remain extant in respect of the Land this Agreement shall forthwith determine and cease to have effect in which case the registration of this Agreement on the Register of Local Land Charges will forthwith be cancelled by the Council.

5.2 Where the Agreement comes to an end under clause 5.1 any monies paid under this Agreement to the Council, with the exception of fees paid under clause 9, shall be returned to the party that made the payment within one month of the Agreement coming to an end together with Interest accrued on the monies from and including the date of payment to and including the date of repayment.

5.3 Where the Agreement is released in part by a future agreement, the Council shall within two (2) weeks of the date of such future agreement place a note against the entry made in the Local Land Charges Registers stating which obligations no longer have effect.

5.4 If the Owner makes a request in writing for the Council to place a note against the entry made in the Local Land Charges Register stating which obligations under this Agreement have been discharged and complied with, the Council shall within two (2) weeks of receipt of such request place such a note against the entry.

## **6. COMMENCEMENT OF DEVELOPMENT**

6.1 The Developer shall give to the Council no less than 5 Working Days' written notice of the intended Commencement Date for the Development on the Land.

## **7. COMMUNITY INFRASTRUCTURE LEVY**

7.1 If after the date of this Agreement there shall be enacted any "tax" related to the grant of planning permission (whether the community infrastructure levy or otherwise) and/or there shall be any changes to any such existing tax, and the terms of such tax or amended tax mean that any obligations under this Agreement or under any condition attached to the Planning Permission change or that the Developer must pay a sum to any person (whether HM Government or to the Council or otherwise) which would duplicate, add to or overlap with any obligation of a party under this Agreement then the parties agree that the terms of this Agreement may at the election of the party affected be modified to such extent (if any) as is necessary to ensure that the party affected shall not be required to contribute (whether by financial contribution or works in kind or both) more than once (in whole or in part) for any item of infrastructure or matter.

## **8. NOTICES**

8.1 Any notice, demand or any other communication served under this Agreement will be effective only if delivered by hand or sent by first class post, pre-paid or recorded delivery.

8.2 Any notice, demand or any other communication served is to be sent to the address of the relevant party set out at the beginning of this Agreement or to such other address as one party may notify in writing to the others at any time as its address for service.

8.3 Unless the time of actual receipt is proved, a notice, demand or communication sent by the following means is to be treated as having been served:

8.3.1 if delivered by hand, at the time of delivery;

8.3.2 if sent by post, on the second working day after posting; or

8.3.3 if sent by recorded delivery, at the time delivery was signed for.

8.4 If a notice, demand or any other communication is served after 4.00 pm on a working day, or on a day that is not a working day, it is to be treated as having been served on the next working day.

8.5 For the avoidance of doubt, where proceedings have been issued in the Courts of England and Wales, the provisions of the Civil Procedure Rules must be complied with in respect of the service of documents in connections with those proceedings.

## **9. COSTS OF THIS AGREEMENT**

9.1 Upon completion of this Agreement the Developer shall pay to the Council its reasonable and proper legal costs up to a maximum of £2250 in connection with the preparation, negotiation and completion of this Agreement.

## **10. DETERMINATION OF DISPUTES**

10.1 Subject to clause 10.7 if any dispute arises relating to or arising out of the terms of this Agreement, a party may give to the other written notice requiring the dispute to be determined under this clause 10. The notice is to propose an appropriate Specialist and specify the nature and substance of the dispute and the relief sought in relation to the dispute.

10.2 For the purposes of this clause 10 a "Specialist" is a person qualified to act as an expert in relation to the dispute and unless otherwise agreed in writing by the Developer and the Council having not less than ten years' professional experience in relation to developments in the nature of the Development.

10.3 Any dispute over the type of Specialist appropriate to resolve the dispute may be referred at the request of any party to the President or next most senior available officer of the Law Society from time to time who will have the power, with the right to take such further advice as he may require, to determine the appropriate type of Specialist and to arrange his nomination under clause 10.4.

10.4 Any dispute over the identity of the Specialist is to be referred at the request of any party to the President or other most senior available officer of the organisation generally recognised as being responsible for the relevant type of Specialist who will have the power, with the right to take such further advice as he may require, to determine and nominate the appropriate Specialist or to arrange his nomination. If no such organisation exists, or the parties cannot agree the identity of the organisation, then the Specialist is to be nominated by the President or next most senior available officer of the Law Society.

10.5 The Specialist is to act as an independent expert and:

10.5.1 each party may make written representations within ten working days of his appointment and will copy the written representations to the other party or parties;

10.5.2 each party is to have a further ten working days to make written comments on the other's representations and will copy the written comments to the other party or parties;

10.5.3 the Specialist is to be at liberty to call for such written evidence from the parties and to seek such legal or other expert assistance as he or she may reasonably require;

- 10.5.4 the Specialist is not to take oral representations from the parties without giving all relevant parties the opportunity to be present and to give evidence and to cross-examine each other;
- 10.5.5 the Specialist is to have regard to all representations and evidence before him when making his decision, which is to be in writing, and is to give reasons for his decision; and
- 10.5.6 the Specialist is to use all reasonable endeavours to publish his decision within 30 working days of his appointment.
- 10.6 Responsibility for the costs of referring a dispute to a Specialist under this clause 10, including costs connected with the appointment of the Specialist and the Specialist's own costs, but not the legal and other professional costs of any party in relation to a dispute, will be decided by the Specialist.
- 10.7 This clause 10 does not apply to disputes in relation to matters of law or the construction or interpretation of this Agreement which will be subject to the jurisdiction of the courts.

## **11. JURISDICTION**

- 11.1 This Agreement is to be governed by and interpreted in accordance with the law of England and Wales.
- 11.2 The courts of England and Wales are to have jurisdiction in relation to any disputes between the parties arising out of or related to this Agreement.

## **12. WAIVER**

- 12.1 No waiver (whether express or implied) by the Council of any breach or default in the performance or observance of any of the covenants terms or conditions of this Agreement shall constitute a continuing waiver and no such waiver shall prevent the Council from enforcing any of the relevant covenants terms or conditions or from acting upon any subsequent breach or default.

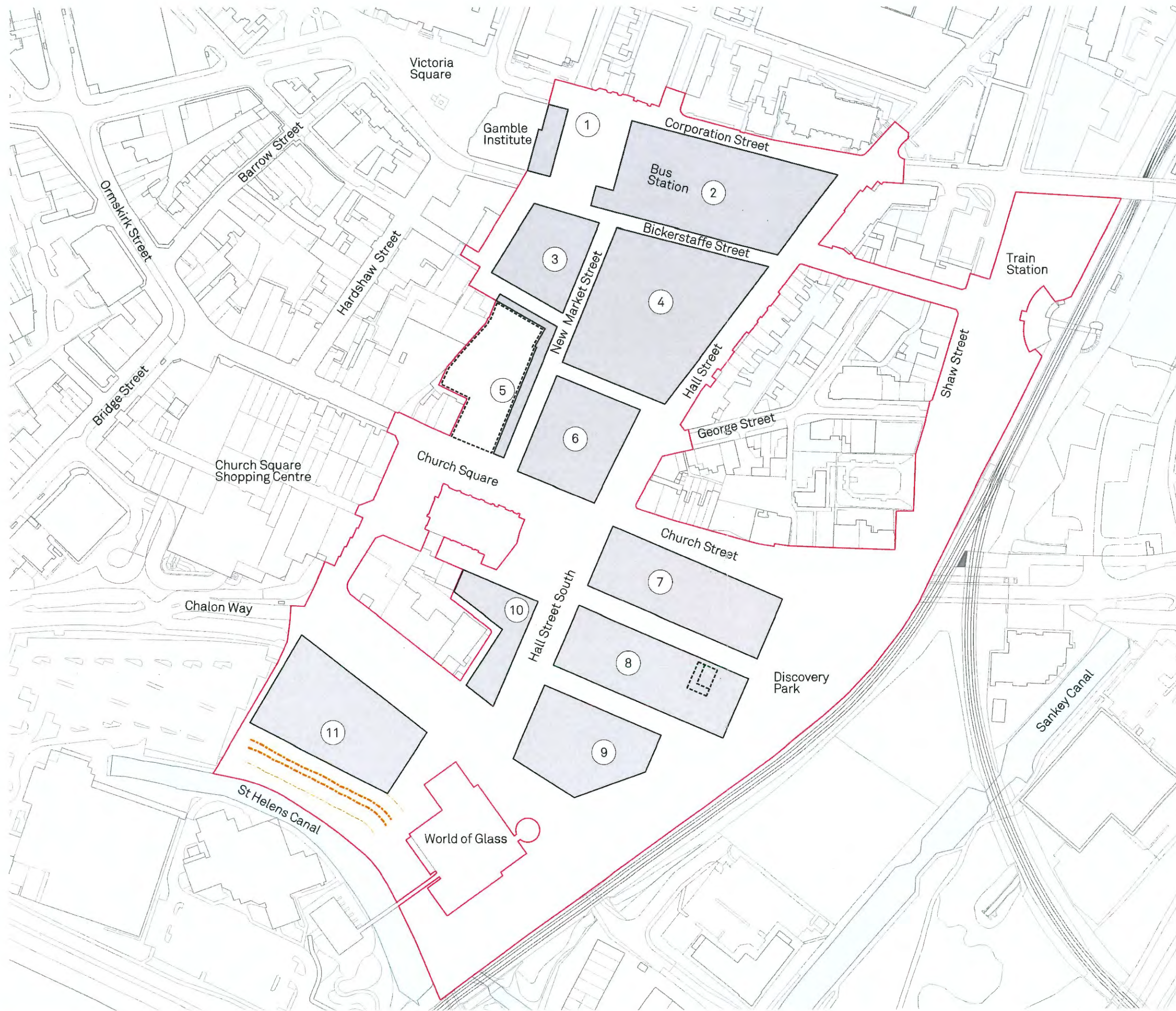
## **13. EXECUTION**

- 13.1 The parties have executed this Agreement as a deed and it is delivered on the date set out above.

## **SCHEDULE 1**

### **PLAN**





Copyright of this drawing is vested in Jon Matthews Architects and it must not be copied or reproduced without written consent. Figured dimensions only are to be taken from this drawing. Do not scale from this drawing.

All Contractors must visit the site and are responsible for taking and checking all dimensions relative to their work. Jon Matthews Architects are to be advised of any variation between drawings and site conditions.

Electronic data/drawings issued as 'read only' and should not be interrogated for measurement. All dimensions and levels should be 'read only' from these values stated in text, on the drawings.

**CONSTRUCTION** - It is considered that the proposed works are within the scope of a competent Contractor and as such no unusual hazards have been identified, refer to relevant Key/Schedule/Designers Risk Assessment

- Key Plan:
- 1
- Key
- Site boundary
  - Development plots
  - Buildings to be retained
  - Culvert and 8m easment
- 1 Gamble Extension
  - 2 Bus Station
  - 3 Office
  - 4 Hotel/Residential/Retail
  - 5 M&S Wrap
  - 6 Market
  - 7 Residential/Retail
  - 8 Residential/Retail
  - 9 Residential/Retail
  - 10 Retail/Office
  - 11 Residential/Office

Rev:	Date:	Details:
I	20.09.2022	Plot key added

**Jon Matthews Architects**  
22 City Tower, Picadilly Plaza, New York St, Manchester, M1 4BT

Project:  
St Helens Masterplan

Title:  
Parameters Plan  
Development Plots

Scale:	Origin Date:	Drawn:	Checked:
1:2000 @ A3	04.11.2021	HM	SA

Status:  
Information

Project:	Drawing No:	Rev:
139	MPS_04_2209	I



## **SCHEDULE 2**

### **TITLE DETAILS**

The Land is shown edged red on Plan and comprises Plots 4, 7, 8, 9 & 11

The freehold of the Land is owned by Council.

The long leasehold interest in the Land is held by the Developer.



## **SCHEDULE 3**

### **PLANNING OBLIGATIONS**

#### **EDUCATION CONTRIBUTIONS**

The Developer covenants with the Council as follows:

**1. Education Contributions Scheme**

- 1.1 Each application for Reserved Matters which includes Family Houses shall be accompanied by a calculation of the number of Primary Places and Secondary Place required for that Phase in accordance with paragraphs 2.1 and 2.4 of this Schedule ("Education Contributions Scheme").
- 1.2 Occupation of the Housing Units in any Phase shall not take place until the Council has approved in writing the Education Contributions Scheme for that Phase.

**2. Calculation of Education Contributions**

- 2.1 On submission of a Reserved Matters application for each Phase the number of Primary Places required shall be calculated for that Phase using the following formula:

Where the number of Primary Places = number of Family Houses/1000 x 37

- 2.2 Prior to Occupation of the first Housing Unit in the relevant Phase the Primary Education Contribution shall be calculated for that Phase using the following formula:

Cost of Primary Places x the number of Primary Places

- 2.3 It is agreed and acknowledged that no Primary School Contribution shall be payable for a Phase where at the time of the submission of the Reserved Matters for that Phase the primary schools in the Local Area are operating at a surplus of 7% (seven per cent) or more.

- 2.4 On submission of a Reserved Matters application for each Phase the number of Secondary Places required shall be calculated for that Phase using the following formula:

Where the number of Secondary Places = number of Family Houses/1000 x 16

- 2.5 Prior to Occupation of the first Housing Unit in the relevant Phase the Secondary School Contribution shall be calculated for that Phase using the following formula:

Cost of Secondary Places x the number of Secondary Places

- 2.6 It is agreed and acknowledged that no Secondary School Contribution shall be payable for a Phase where at the time of the submission of the Reserved Matters for that Phase the secondary schools in the Local Area are operating at a surplus of 7% (seven per cent) or more.

**3. Payment of Education Contributions**

- 3.1 No Housing Unit within a Phase shall be Occupied unless and until the Developer has paid to the Council the Education Contributions for that Phase in accordance with the approved Education Contributions Scheme for that Phase.

## **SCHEDULE 4**

### **THE COUNCIL'S COVENANTS**

1. The Council covenants with the Developer as follows:
  - 1.1 Where Education Contributions are paid pursuant to paragraph 3.1 of Schedule 3 the Council shall use the same solely for the purposes of primary and secondary education to be available for children residing in the Development and for no other purpose and in the event any Education Contribution (or any part of it) remains unspent or legally uncommitted on the expiry of 7 years from the date of payment, such unspent and/or uncommitted sum shall be repaid to the Developer together with Interest thereon.

The Common Seal of )  
**ST HELENS** )  
**BOROUGH COUNCIL** )  
was affixed in the )  
presence of: )

Authorised Signatory:

Name (in block capitals):

Signed as a deed by )  
**ENGLISH CITIES FUND (ACTING BY ITS** )  
**GENERAL PARTNER ECF (GENERAL** )  
**PARTNER) LIMITED)** acting by two )  
directors )

Signature of director:

Signature of director:

**Form of Undertaking**

Our ref:

Your ref: P2022/0212/HYBR

St Helens Council

[Add Address]

Dear Sirs

**SECTION 106 DATED**

**2022 ("the Section 106 Agreement")**

**PLANNING APPLICATION REFERENCE: P/2022/0212/HYBR**

We undertake following completion of the acquisition of the Obligation Land (as defined in the Section 106 Agreement) to submit to the Land Registry the application for registration of English Cities Fund (acting by its general partner ECF (General Partner) Limited) as the registered proprietor of the Obligation Land ("the Application") within the requisite timescales and thereafter diligently pursue the Application using our reasonable endeavours to address any requisitions that may arise in respect of the Application as soon as reasonably practicable.

Furthermore, we undertake to provide you with the Land Registry official copy entries indicating that English Cities Fund (acting by its general partner ECF (General Partner) Limited) has been so registered within 10 (ten) working days of completion of the registration.

Signed:

Name:

Date:

**SIGNED BY** or on behalf of the parties on the date stated at the beginning of this Agreement.

Signed as a deed by \_\_\_\_\_ )  
**ENGLISH CITIES FUND**

(acting by its general partner

**ECF (GENERAL PARTNER) LIMITED)**

acting by two directors

Signature of \_\_\_\_\_  
[ \_\_\_\_\_ ]

Signature of \_\_\_\_\_  
[ \_\_\_\_\_ ]

Authorised [signatory] [signatories]

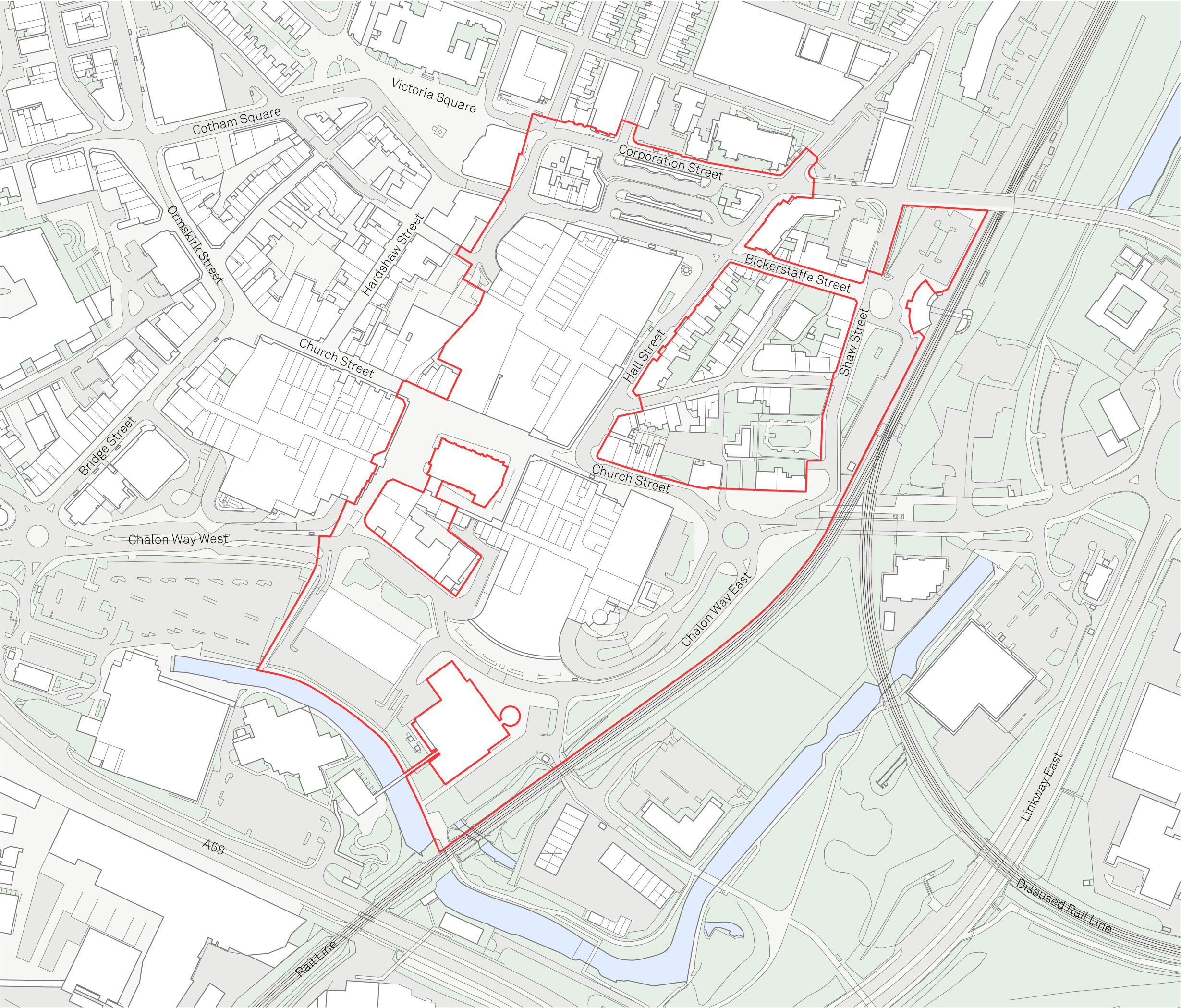
**EXECUTED AS A DEED** )  
by affixing the common seal of )  
**ST HELENS BOROUGH COUNCIL** )  
hereto affixed in the presence of: - )

*J. B. Keen*

Authorised signatory








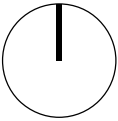
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Electronic data/ drawings issued as 'read only' and should not be interrogated for measurement. All dimensions and levels should be 'read only' from those values stated in text, on the drawings.

 CONSTRUCTION - It is considered that the proposed works are within the scope of a competent Contractor and as such no unusual hazards have been identified, refer to relevant Key/ Schedule/ Designers Risk Assessment

Key Plan:



Key  
 Planning Application Boundary

Rev:	Date:	Details:
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Jon Matthews Architects  
22 City Tower, Piccadilly Plaza, New York St, Manchester, M1 4BT

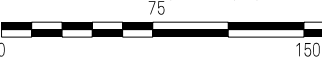
Project:  
St Helens Masterplan

Title:  
Site Location Plan

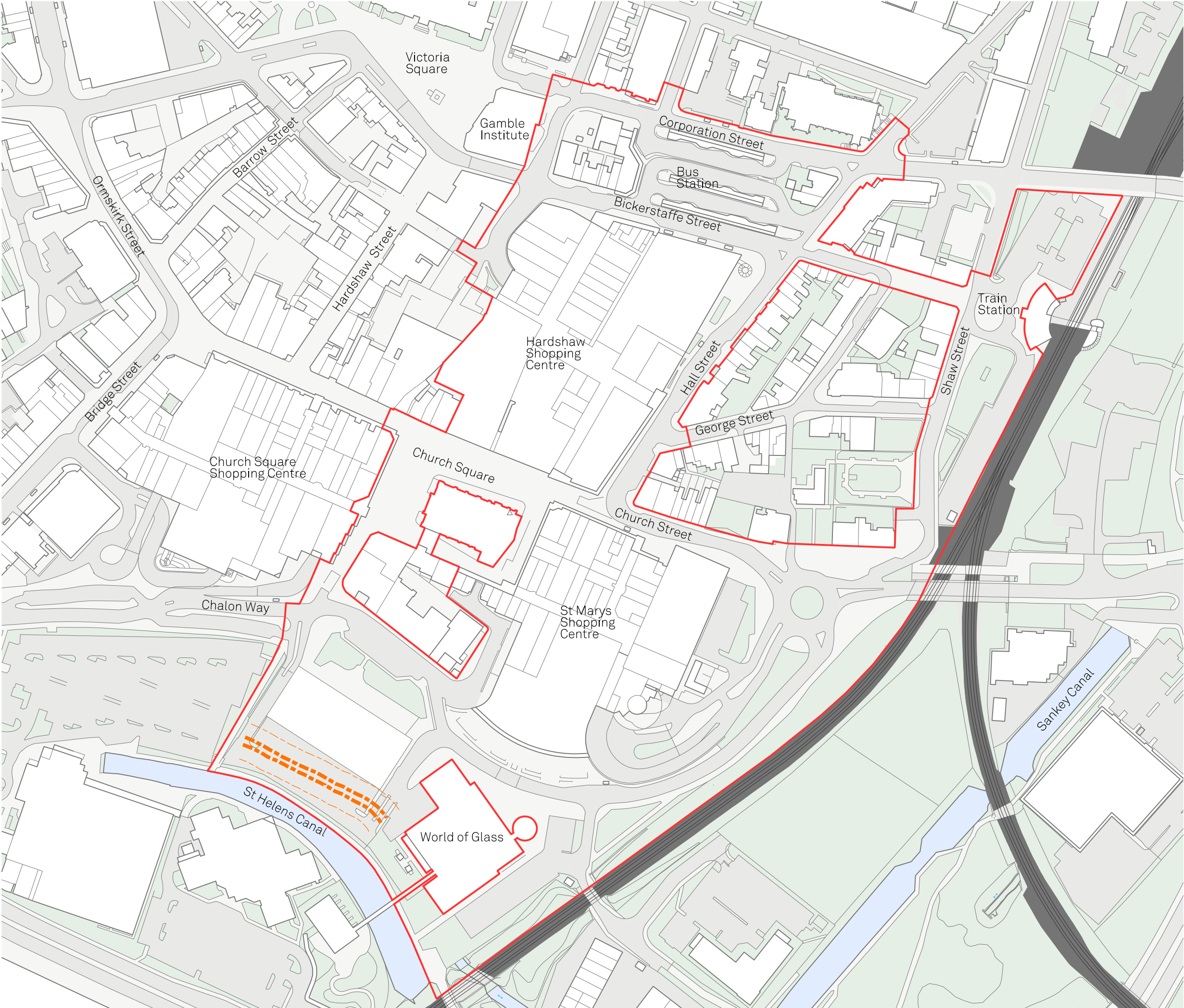
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Information

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139	MPS_04_1100	A








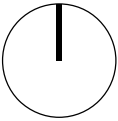
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 CONSTRUCTION - It is considered that the proposed works are within the scope of a competent Contractor and as such no unusual hazards have been identified, refer to relevant Key/ Schedule/ Designers Risk Assessment

Key Plan:



- Key
-  Planning Application Boundary
  -  Culvert and 8m easment

Rev:	Date:	Details:
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Jon Matthews Architects  
22 City Tower, Piccadilly Plaza, New York St, Manchester, M1 4BT

Project:  
St Helens Masterplan

Title:  
Existing Site Plan

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1:2000 @ A3	01.07.2021	SA	SA

Status:  
Information

Project:	Drawing No:	Rev:
139	MPS_04_1210	B





**ST HELENS**  
BOROUGH COUNCIL

**Town Hall, St. Helens, Merseyside, WA10 1HP**

**Telephone: 01744 676106 (Mrs KM Howard)**

## **Agenda**

### **PLANNING COMMITTEE**

**PUBLIC MEETINGS ARE WEBCAST (LIVE STREAMED)**

Date: Tuesday, 27 September 2022 Time: 5.30 pm Venue: Room 10

#### **Membership**

<b>Lab</b>	<b>9 Councillors</b>	<b>Banks, Bowden, Gomez-Aspron MBE, Hodkinson, Laird, D Long (Chair), Maloney MBE, McCauley and McCormack</b>
<b>Grn</b>	<b>2 Councillors</b>	<b>Hooton and Makin</b>
<b>LibDem</b>	<b>1 Councillor</b>	<b>Pearl</b>
<b>IND</b>	<b>1 Councillor</b>	<b>Tasker</b>
<b>NLWI</b>	<b>1 Councillor</b>	<b>Collier</b>
<b>Con</b>	<b>1 Councillor</b>	<b>Case</b>

<u>Item</u>	<u>Title</u>	<u>Page</u>
4(a)	P-2022-0212 - HYBR: Land Bound by Corporation Street to the North, St Helens Central and Rail Lines to the East, St Helens Canal to the South and the Town Centre, Broadly Defined by Bickerstaffe Street and Market Street, To the West.	3

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<b>APPLICATION NUMBER: PROPOSAL:</b>	<b>P/2022/0212/HYBR</b> <b>Hybrid planning application seeking:</b> <b>- Full planning permission and permission for relevant demolition in a conservation area for proposed demolition works; and</b> <b>- Outline planning permission for development of a mix of uses, comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community &amp; learning uses (Use Class F1(b-e) and F2(b)); and Sui Generis uses, with associated access, servicing, parking, public realm, and landscaping, with all matters (Access, Appearance, Landscaping, Layout and Scale) reserved for future determination.</b>
<b>LOCATION:</b>	<b>Land Bound by Corporation Street to the North, St Helens Central and Rail Lines to the East, St Helens Canal to the South and the Town Centre, Broadly Defined by Bickerstaffe Street and Market Street, To the West.</b>
<b>WARD:</b>	<b>Town Centre Ward</b>
<b>APPLICANT:</b>	<b>English Cities Fund (General Partner) Limited</b>
<b>CASE OFFICER:</b>	<b>Jill Nixon</b>
<b>RECOMMENDATION:</b>	<b>Be minded to Grant Planning Permission Subject to Conditions and the completion of a satisfactory Section 106 legal agreement to be approved by the Head of Planning under delegated powers</b>

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## **1. APPLICATION SITE**

- 1.1 The application site comprises 9.87 hectares in St Helens town centre. The site is bound by Corporation Street to the north, St Helens Central and rail lines to the east, St Helens Canal to the south and the town centre to the west (broadly defined by Bickerstaffe Street and Market Street). The application site includes 39-41 Hall Street, which are located within the George Street Conservation Area
- 1.2 The uses of the adjacent areas to the site are mixed. The predominant use is retail and with other town centre uses including commercial and community. Residential dwellings also exist adjacent to the site.

## **2. PROPOSAL**

- 2.1 The application has been submitted as a Hybrid application, meaning that it seeks Full

Planning permission for some elements and Outline Planning permission for others. An application for Outline Planning permission allows for a decision on the general principles of how a site can be developed. Outline Planning permission can be granted subject to conditions requiring the approval of one or more 'Reserved Matters'. Reserved Matters are those aspects of a proposed development that an applicant can choose not to submit details of with an Outline Planning application but can submit for approval at a later date. Planning legislation defines Reserved Matters as being access; appearance; landscaping; layout, and scale.

2.2 Full Planning permission is sought for the demolition of the following:

- The Hardshaw Centre;
- St Mary's Shopping Arcade, Market and Multi Storey Car Park (MSCP);
- 39-41 Hall Street (Swan Hotel and Town Fryer Fish & Chips shop) to the immediate east of the bus station and located within the George Street Conservation Area; and
- All buildings in the block of retail units bounded by the bus station to the east, Bickerstaffe Street to the south, Library Street to the west, and Corporation Street to the north.

51 Church Street, the former M&S building in the west of the application site, and a substation in the east of the site will not be demolished.

2.3 The Outline Planning aspect of the application seeks planning permission for the construction of a series of new buildings of varying heights, up to a maximum of 6 storeys in height (ground plus 5 storeys). The following scale of the range of uses are presented as two options (A and B):

- Up to 7,854 sqm Gross Internal Area (GIA) of retail / leisure / food & drink floorspace, including a market, kiosks, and other retail units (Option A) (Option B: up to 8,134 sqm);
- Up to 10,950 sqm GIA of office floorspace (Option A) (Option B: up to 24,678 sqm. GIA);
- Up to 340 sqm GIA of arts/leisure/community/retail floorspace;
- Up to 423 residential units (Option A) (Option B: up to 374 units);
- A hotel of up to 155 beds (Option A only); and
- Landscaping and public realm improvement works.

The Outline element of the application has been assessed based on the maximum floor spaces within the two options.

2.4 All matters have been reserved so should Outline Planning permission be granted, Reserved Matters applications would be required for the approval of layout, scale, appearance, access and landscaping. None of these details have been submitted with the application. However, a substantial amount of supporting information has been submitted with the application to evidence that once the buildings have been demolished, the scale of new development is achievable in principle and would be assessed at Reserved Matters stage.



Environmental Impact Assessment

- 2.5 In accordance with the requirements of Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (as amended), an Environmental Statement (ES) may be required in support of applications which exceed certain thresholds. On the basis the proposed development comprises more than 150 dwellings and the overall area of the development exceeds 5 hectares, an ES was considered necessary in accordance with Category 10(b) of Schedule 2.
- 2.6 The ES submitted with the application provides an examination of the potential effects of the development upon a number of key receptors. No formal Scoping Opinion was submitted by the applicant, but they did consult informally with the Local Planning Authority to identify the receptors.
- 2.7 The ES and addendum report the findings of the Environmental Impact Assessment (EIA) undertaken in respect of the proposed development and includes the following sections:
- Air Quality
  - Noise and Vibration
  - Built Heritage
  - Town Scape and Visual Amenity
  - Socio Economic
- 2.8 With regard to Air Quality, the ES concludes that subject to the implementation of demolition and construction management plans that the demolition and construction works can be carried out with minimal impact on air quality. The application seeks permission for up to 423 additional residential properties which would be additional sensitive receptors within the application site. The ES recognises that the residential properties could be exposed to elevated air pollution where elevations are adjacent to main roads. It is considered that, subject to the introduction of green travel plans and practical measures regarding design, an acceptable level of amenity can be achieved. The submission has been assessed by Council Officers and they are satisfied with the methodology and advised mitigation measures to be secured. A condition requiring the submission of an updated Air Quality Statement with mitigation measures at Reserved Matters stage is proposed.
- 2.9 With regard to noise and vibration, the ES concludes that subject to the implementation of demolition and construction management plans, the noise and vibration effects during these phases will range from negligible to minor. It is also noted that these impacts will be temporary. With regard to the impact of noise on the proposed residential caused by the operational phase, the ES concluded that subject to the incorporation of mechanical ventilation and acoustic glazing a satisfactory level of amenity can be achieved. The submission has been assessed by the Council's technical consultees who are satisfied with the methodology, but they advised conditions to secure additional noise surveys at the reserved matters stage when the relationship of uses and hours of operation can be considered together with the design

of the buildings. It is also proposed to limit permitted development rights for change of uses of buildings in the future which will allow the potential impact of noise on residential amenity to be assessed.

- 2.10 With respect to the impact on the Built Heritage, the ES has assessed the potential effects of the proposed buildings using the anticipated maximum building heights; therefore, impacts have been assessed on a worst-case scenario basis. The proposed new built form is in general higher than the existing townscape, however, the active frontages and activity at street level has been assessed as a positive change to the setting of the George Street Conservation Area, and the proposed New Market Street will improve views to the designated Church of St Helens placing it back in the heart of the town centre. The Council's Heritage Consultant has no objections to the demolition of any of the proposed buildings, including those located within the George Street Conservation Area at 39-41 Hall Street. The Heritage Consultant is satisfied with the proposed methodology subject to securing revised heritage impact assessments at reserved matters stage when the detail of layout, scale and appearance of the proposed development will be assessed.
- 2.11 Regarding Townscape and Visual Amenity, the ES concludes that the proposed development will replace a substantial amount of the existing built form within the existing town centre with high quality buildings and improved public realm, resulting in beneficial effects on the local townscape and visual amenity. It is considered that if the detail of all the Reserved Matters are carefully assessed to secure a cohesive design that will enhance the retained aspects of the town centre, then the proposal will improve the townscape and visual appearance.
- 2.12 With respect to the Socio Economic impact, the ES concludes that both during the construction and operational phase that the development will have a beneficial impact. Within the demolition and construction phase, the proposed development would generate the equivalent to 20 net construction jobs (taking account of leakages) over the 12-month construction duration; deliver 374–423 new residential homes which would represent 5.1% - 5.8% of the total target for the plan period according to the St Helens Borough Local Plan up to 2037; deliver 169-551 operational jobs (taking account of leakages and displacement); deliver 0.9 hectares of publicly accessible open space and sufficient play space which meets, or potentially exceeds, the prescribed requirements. The submitted ES states that there is currently a surplus of GP and school provision but that the applicants will provide contributions, if required, and deliver a safe and secure development implementing Secured by Design. The ES concludes that overall, there are significant socio-economic benefits.

### **3. CONSULTATIONS**

- 3.1 Coal Authority: No objection subject to conditions.
- 3.2 Contaminated Land: No objection.
- 3.3 Noise: No objection in principle.

- 3.4 Air Quality: No objection.
- 3.5 Highways: No objection subject to conditions.
- 3.6 Local Lead Flood Authority: No objection in principle subject to conditions.
- 3.7 Environment Agency: No objection in principle.
- 3.8 United Utilities: Additional information required at reserved matters stage.
- 3.9 Fire and Rescue: No objection.
- 3.10 Merseyside Police: No objection.
- 3.11 Lighting: No comments received.
- 3.12 Conservation: No objection in principle.
- 3.13 Trees and Woodland: Additional information required at reserved matters stage.
- 3.14 MEAS: No objection in principle.
- 3.15 Education: No objection subject to securing appropriate financial contributions.
- 3.16 Liverpool City Region Combined Authority: No objection.

#### **4. REPRESENTATIONS**

- 4.1 The application was advertised by press and site notices and 1,849 individual letters. As the application was accompanied by an ES the consultation period was extended to 30 days and paper copies of all the application documents were made available to view in St Helens Town Hall.
- 4.2 A single letter was initially received from the occupier of one of the properties to be demolished but the queries / issues raised related to the legal acquisition process, which is being dealt with separately and is not a planning related matter.
- 4.3 The applicants submitted amended plans and documents, and the application was readvertised. No representations have been received as a result of this additional publicity.

#### **5. SITE HISTORY**

- 5.1 There is extensive planning history for the application site as it encompasses a large town centre location. The below applications are significant recent applications in the area:
- 5.2 P/2022/0379/FUL - Land Fronting Chalon Way Car Park Foundry Street St Helens - Installation of containers & outdoor cinema screens for up to seven years to create a

mixed-use leisure hub with stage & outside seating areas to provide business start-up, retail, & leisure units. (Use Classes E(a), E(b), F1(b) and sui generis drinking establishments). Granted – 10.08.2022.

- 5.3 P/2020/0156/FUL - Unit 2 The Hardshaw Centre, St Helens, Merseyside, WA10 1EB. - Change of use from Class A1 Retail to a mixed use comprising Offices (Class B1), Coffee Shop (Class A1/A3), Launderette (Sui Generis), Centre Management Office (Class B1), enlargement of disabled WC and baby changing facilities and new shopfront on Hall Street. Granted - 22.04.2022.
- 5.4 P/2020/0763/FUL - Demolition of multi-storey car park, new street lighting and external works to the retained site. Granted - 15.12.2020.
- 5.5 P/2020/0913/FUL - 59 - 69 Church Street, St Helens. Partial change in use of existing building from Class E (commercial uses) to Class C3 (dwelling houses) to create 9 flats with associated partial demolition and material alterations to the external appearance of the building, together with a new shop frontage to Church Street. Granted - 10.03.2021.

## 6. POLICY

### National Planning Policy Framework

- 6.1 Paragraph 9 of the National Planning Policy Framework (NPPF) states that planning decisions should play an active role in guiding development towards sustainable development, but in doing so should take local circumstances into account to reflect the character, needs and opportunities of each area. Paragraph 11 states that planning decisions should apply a presumption in favor of sustainable development. This means approving development proposals that accord with an up-to-date development plan without delay; or where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting planning permission unless the application of policies in the NPPF that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the NPPF taken as a whole.
- 6.2 Paragraph 12 of the NPPF clarifies that the presumption in favor of sustainable development does not change the statutory status of the development plan as the starting point for decision making. Where a planning application conflicts with an up-to-date development plan, permission should not normally be granted. Local Planning Authorities may take decisions that depart from an up-to-date development plan, but only if material considerations in a particular case indicate that the plan should not be followed.

### Development Plan

- 6.3 The adopted development plan for relevant to this area of St Helens Borough is the St Helens Borough Local Plan up to 2037; and the Joint Merseyside and Halton Waste Local Plan (adopted 2013).

The policies of relevance are as follows:

Policy LPA01: Spatial Strategy  
Policy LPA02: Development Principles  
Policy LPA03: A Strong and Sustainable Economy  
Policy LPA04: Meeting St Helens Borough's Housing Needs  
Policy LPA06: Transport and Travel  
Policy LPA07: Infrastructure Delivery and Funding  
Policy LPA08: Green Infrastructure  
Policy LPA12: Health and Wellbeing  
Policy LPB01: St Helens Town Centre and Central Spatial Area  
Policy LPC01: Housing Mix  
Policy LPC02: Affordable Housing  
Policy LPC04: Retail and Town Centres  
Policy LPC05: Open Space  
Policy LPC06: Biodiversity and Geological Conservation  
Policy LPC09: Landscape Protection and Enhancement  
Policy LPC10: Trees and Woodland  
Policy LPC11: Historic Environment  
Policy LPC12: Flood Risk and Water Management  
Policy LPC13: Renewable and Low Carbon Energy Development  
Policy LPD01: Ensuring Quality Development  
Policy LPD03: Open Space and Residential Development

#### Other Considerations

##### *Environmental Statement*

- 6.4 An ES was submitted with the application. In accordance with The Environmental Impact Assessment Regulations 2017 Schedule 4 sets out the general requirements for the content of an ES. These comprise information on the nature of the development; consideration of alternatives; relevant aspects of the environment; likely environmental impacts arising; proposed mitigation measures; and an indication of any difficulties in compiling the information needed. A non-technical summary of the contents of the ES is also required. The submitted ES with addendums satisfies these requirements.
- 6.5 The application has been considered having regard to Article 1 of the First Protocol of the Human Rights Act 1998, which sets out a person's rights to the peaceful enjoyment of property and Article 8 of the Convention of the same Act which sets out his/her rights in respect for private and family life and for the home. Officers consider that the proposed development would not be contrary to the provisions of the above Articles in respect of the human rights of surrounding residents/occupiers.
- 6.6 This application has been considered in relation to Section 17 of The Crime and Disorder Act. The Police Crime Prevention Officer has provided comment and suggestions for consideration by the applicant at the detailed design stage. The applicants have committed to ensure that Secure by Design is an integral part of the detailed design of the proposals.
- 6.7 The application has been considered in accordance with the St Helens Council Comprehensive Equality Policy, which seeks to prevent unlawful discrimination, promote equality of opportunity and good relations between people in a diverse community. In this case the proposed development is not anticipated to have any potential adverse impact from an equality perspective, and it is expected that the future detailed design proposals to be considered at Reserved Matters stage will have a positive impact.

*St Helens Town Centre Masterplan Development Framework*

- 6.8 The St Helens Town Centre Masterplan Development Framework sets out a clear vision, objectives and development principles that will catalyse the delivery of transformational change. In doing so, it will help to secure a range of long-lasting economic, social and environmental benefits across the town centre and positively impact the wider borough.
- 6.9 The Masterplan Development Framework has been prepared in the context of, and informed by, wider regeneration ambitions for St Helens, including those set out within adopted Local Plan and related strategies. The Masterplan Development Framework guides development across the Framework area over the next 20 years and forms a material consideration in the determination of all future planning applications across the area. The Development Framework will not form part of the statutory Development Plan; however, it has been prepared having had full regard to relevant legislation.
- 6.10 The St Helens Town Centre Masterplan Development Framework has been developed with the Council's development partner the English Cities Fund (ECF). It sets out the shared vision for St Helens town centre. The Masterplan Development Framework was endorsed by the Council's Cabinet in February 2022 to guide and support the positive transformation of the town centre. It will ensure that development coming forward is of high quality, is sustainable and has the people of St Helens at its heart

*St Helens Council Housing Strategy*

- 6.11 The St Helens Borough Housing Strategy 2022 – 2027 adopted in June 2022 sets out the vision and priorities for housing in St Helens for the next five-year period. St Helens Borough Council has a duty to meet the housing needs of the borough residents and the strategy outlines the actions that will be taken to meet that local need. The Strategy has the overarching vision of 'Quality and Accessible Homes for All' and is a core supporting document of the St Helens Borough Strategy, which sets the vision for the Borough until 2030.

**7. ASSESSMENT**

- 7.1 The main considerations for this application are:

- Principle of the development
- Housing delivery
- Design and Layout
- Heritage
- Residential Amenity
- Transport and Travel
- Renewable and low carbon energy development
- Section 106 Legal Agreement
- Other matters

Principle of the development

- 7.2 The application is for the regeneration of St Helens town centre. St Helens Borough Local Plan up to 2037 (SHBLP (St Helens Borough Local Plan)) Policy LPA01: Spatial Strategy, sets out how development will be distributed across the Borough, including the comprehensive regeneration of the Town Centre (and wider Borough), that will be delivered by the English Cities Fund Regeneration Partnership, through the provision



of quality housing, new commercial activity, and upgraded infrastructure.

- 7.3 LPA01 states that new development will be directed to sustainable locations that are appropriate to its scale and nature and that will enable movements between homes, jobs and key services and facilities to be made by sustainable non-car modes of transport. The proposal is in accordance with the Spatial Strategy as it provides a range of appropriate town centre uses in the town centre, which are appropriate in scale and nature. The provision of housing in the town centre will also support the use of sustainable modes of transport.
- 7.4 Part 3 of LPA01 states that the re-use of suitable previously developed land in Key Settlements will remain a key priority. The proposal is for the redevelopment of a previously developed site, and as such accords with policy LPA01 of the Development Plan.
- 7.5 Policy LPB01 – St Helens Town Centre and Central Spatial Area, sets out that the English Cities Fund Regeneration Partnership will help deliver a comprehensive redevelopment of the Town Centre and Central Spatial Area, including new commercial activity, upgraded infrastructure, the provision of quality housing, and the overall improvement of the social and economic viability of the area. This application and the range of uses proposed accord with that area Policy.
- 7.6 The development principles and requirements for new development are set out in SHBLP Policy LPA02. These requirements include the creation of sustainable communities, through a mix of types and tenures of quality homes, whilst providing and contributing to infrastructure and service requirements and improving access opportunities for formal and informal recreation and improving cycling and walking routes. The proposal is for a mix of commercial retail, leisure, and residential uses all of which would be expected within a vibrant town centre. It is considered that the principles of the proposal to regenerate the town centre subject to the proposed conditions and S106 Agreement would therefore support the aims of the Local Plan and relevant sections of policies LPA01 and LPA02.

#### Housing Delivery

- 7.7 The application proposes between 374 and 423 new dwellings. The Local Plan, whilst allocating a range of housing sites across the Borough, includes a requirement within Policy LPA01: Spatial Strategy for development to be directed to sustainable locations that are appropriate to its scale and nature and that will enable movements between homes, jobs and key services and facilities to be made by sustainable non-car modes of transport. The provision of housing within the town centre accords with that requirement. Policy LPA01 also states that the re-use of suitable previously developed land within Key Settlements is a key priority, and that a substantial proportion of new housing throughout the Plan period will be on such sites. This application proposes a mix of uses, with the housing element one part of the development, which will assist the Council achieving the Spatial Strategy in the Development Plan.
- 7.8 Policy LPA02: Development Principles requires new development to support a series of development principles when relevant. These include creating sustainable communities with a strong sense of place and meeting the challenge of population retention and growth by; providing for a mix of types and tenures of quality homes to meet the needs and aspirations of all existing and future residents in sustainable locations. The proposal whilst made in outline will provide a range of different house types in a town centre location which will add to the existing mix of units currently located there. The NPPF encourages residential development in town centres,

recognising the role that such a use can play in ensuring the vitality of centres (para. 86).

- 7.9 Policy LPA04 – Meeting St Helens Borough’s Housing Needs states that in the period from 1 April 2016 to 31 March 2037 a minimum of 10,206 net additional dwellings should be provided in the Borough of St Helens, at an average of at least 486 dwellings per annum. The Council to meet that need has allocated housing sites totalling 6,948 dwellings. Over the plan period these allocated sites are anticipated to deliver 3,725 dwellings. The proposed development would therefore assist in meeting that need over the Plan Period, as well as making best use of previously developed land as required by Policy LPA01. This Policy states that the housing requirement will be met from various sources, including windfall development, sites already granted planning permission and those already over under construction. The Policy also states that within St Helens Town Centre a density of at least 40 dwellings per hectare (dph) would be considered appropriate. Therefore, the high density development proposed by this application accords with the Development Plan and is acceptable.
- 7.10 The St Helens Borough Housing Strategy (2022-27) states that the housing developed as part of the wider town centre Regeneration in St Helens and Earlestown should be transformative and an exemplar of town centre living. The Council through the Town Deal has secured £25 million towards various projects, including Town Centre Living and Regeneration. This application therefore accords with the aspirations of the Housing Strategy to develop new housing as part of the town centre regeneration through both new builds, repurposing of existing buildings and bringing forward housing development sites.

#### Design and Layout

- 7.11 The proposal is to demolish most buildings within the application site, except for the former M&S building at 51 Church Street, and a substation in the east of the site. Except for some 19<sup>th</sup> century properties, it is considered that the buildings being demolished do not provide a positive aesthetic value to the town centre and, therefore, the opportunity to redevelop the area with buildings that provide greater linkages and improved architectural integrity is welcomed. therefore, the opportunity to redevelop the area with buildings that provide greater linkages and improved architectural integrity is welcomed.
- 7.12 All matters relating to design including layout and scale have been reserved and will be assessed when the reserved matters application(s) are submitted. Drawings dividing the regeneration area into 11 development plots, consistent with the adopted St Helens Town Centre Masterplan Development Framework, have been presented with the application. Parameter drawings showing the maximum floor areas within the various plots (set out in Section 2 of this report) have been submitted to enable the assessment of principle.
- 7.13 Drawings defining both maximum and minimum heights to evidence that the scale of the proposals will befit the character of a major town centre such as St Helens have also been submitted in support of the application.
- 7.14 While the maximum floor areas of proposed uses have been defined, the number of proposed units would be defined at Reserved Matters stage although there is an upper limit of 423 residential units. The number of units and associated active frontages, will be an integral part of creating character areas and will be assessed at reserved matters stage.

- 7.15 As all matters are reserved, all aspects regarding the appearance of all the buildings and structures and landscaping will be dealt with in subsequent reserved matters applications. Conditions are proposed to secure details take account of the development as a whole and adjacent sites to enable a sense of place to be secured.
- 7.16 It is considered that, subject to the proposed conditions, a comprehensive high quality development that incorporates sustainable design features and integral landscaping can be secured. The proposal is therefore considered to comply with the principles of adopted policy LPD01.

#### Heritage

- 7.17 The application is supported by a Heritage Statement that assesses the potential impact based on the concepts within the supporting parameters drawings. The Council's Heritage Consultant has raised no objections to the principle of the development, however, as all matters are reserved the impact of design, scale, etc. will be fully assessed at Reserved Matters stage.
- 7.18 The majority of the demolition is confined to the modern shopping developments, comprising the Hardshaw Centre and St Mary's Shopping Arcade, neither of which have any heritage value. The supporting Historic Environment Assessment acknowledges a degree of harm resulting from the demolition of late C19 properties at 39-41 Hall Street (Swan Hotel and Town Fryer Fish & Chips shop). This small group of rendered (over brick) C19 buildings lie on a prominent corner of Hall Street and Corporation Street with the bus station located behind. They form an attractive group and, whilst altered, they have typical detailing which represents buildings of that period. The Council's Heritage Consultant does not consider these buildings of high value but that they nevertheless contribute in a positive manner to the character and appearance of George Street Conservation Area and the wider town. It is considered that their demolition will cause 'less than substantial' harm. The other group of existing late C19 to be demolished are located on the corner of Bickerstaffe Street and Library Street. This is a short row of much altered late C19 former small 2 storey terraces and their demolition is not considered to raise any negative heritage issues.
- 7.19 It is considered that in this instance it is clear that the wider proposals will provide substantial public benefits that will outweigh the less than substantial harm caused by the proposed demolition of the buildings. Subject to the detail design to be considered at reserved matters stage it is considered that the proposal complies with Local Plan Policy LPC11.
- 7.20 The Heritage Statement submitted in support of the application also advised the undertaking of an archaeological watching brief, to investigate any findings below ground, the Council's Archaeological Consultant endorses this recommendation which is secured by conditions.

#### Residential Amenities

- 7.21 The application site is within St Helens town centre. Within this area there are a number of existing residential properties, immediately adjacent and within the application site. Outline planning permission is sought for up to 423 new residential units to be constructed within the application site. Although siting is reserved, the Parameters Plan indicates that the residential units will be in the following development plots: plot 4 which is located on part of the existing Hardshaw Centre; plots 7, 8 and 9 which would occupy part of the current St Mary's Shopping Centre site; and plot 11 which is the former Chalon Way car park. The precise siting within the plots would be

determined at Reserved Matters stage.

- 7.22 The precise details, including siting and appearance of the new dwellings would be approved at reserved matters stage, but it is anticipated that there will be a mix of residential dwellings and apartments. The introduction of new dwellings would create additional receptors sensitive to noise within the town centre, which gave rise to this application being supported by an ES. Issues to be considered include the mix of commercial/ leisure uses and the resultant impact on residential amenity, including noise and air quality. Amenity needs to be considered at both operational, demolition and construction phases.
- 7.23 The detail of the relationship of uses will be considered at Reserved Matters stage and additional noise surveys and schedules for hours of operation would be secured by condition. Aspects relating to all aspects of the construction/demolition works and their management are also proposed to be secured by condition, as are details of phasing so account can be taken of the impact on proposed and existing residents. It is also proposed to remove the permitted development rights of all proposed buildings to prevent future changes of use without planning permission to allow consideration of the impact on residential amenity. It is considered that principle of the development mix is acceptable subject to the proposed conditions.

#### Transport and Travel

- 7.24 Local Plan Policy LPA06 states the Council's strategic priorities for the transport network are to facilitate economic growth, enable good levels of accessibility between homes, jobs, and services, improve air quality and minimise carbon emissions.
- 7.25 The application was supported by a Transport Assessment, which has been assessed by the Council's Highways Consultants. Whilst all matters, including access and layout relating to the construction phase have been reserved, the submitted application indicates the following possible works:
- St Helens Bus Station is to be upgraded as part of the development proposals. Layover buses, which currently park on the northern side of Corporation Street, will be provided with a dedicated space within the footprint of the bus station.
  - A north-south pedestrian prioritised route around the edge of the bus station will also be provided to connect the Theatre Royal, to the north, with the new retail core.
  - Bickerstaffe Street will become a new pedestrian/cycle link between the new bus station and the rail station to create a joined-up transport hub.
  - An enlarged public realm space will be created to the rear of the Gamble Building on the northern side of Corporation Street to better accommodate pedestrians and cyclists.
  - A new pedestrian link, known as New Market Street, will be provided between Bickerstaffe Street and Church Street to connect the new bus station with Church Square.
  - Introduction of two-way traffic on the existing one-way roads of Corporation Street and Shaw Street, and new north-south routes at New Market Street and Hall Street South.

- Closure and removal of Chalon Way East.
- Hall Street will become a centralised north-south walking and cycling route that will be extended to form a new access road, Hall Street South, between Church Street and the existing Chalon Way.
- A designated cycle lane will be provided along Hall Street and Hall Street South between Chalon Way in the south and Bickerstaffe Street in the north.
- An east-west designated cycle lane will also be provided along Church Street between Hall Street/Hall Street South and Shaw Street.
- Chalon Way East will be stopped-up for vehicle traffic between Shaw Street and Salisbury Street and will be replaced by a new outdoor green space, referred to in this application as 'Discovery Park.' A new pedestrian and cycle link will be provided within the park to connect Shaw Street with the World of Glass in the south, and onwards to the Totally Wicked Stadium via the Steve Prescott Bridge
- The park will also be accessed by a new east-west cycle and pedestrian link located off Hall Street South between development plots 7 and 8.
- The existing over-engineered give-way roundabout junction outside the rail station will be replaced by a more pedestrian-friendly "arrival gateway" space that will provide a crossing point east-west to Bickerstaffe Street.

This list is indicative, and conditions are proposed to secure detailed design submissions for the proposed highway works, car and cycle parking provision and management, details of taxi ranks, and the bus station at Reserved Matters stage. The proposed details of demolition/contractor parking and haulage routes, together with interim arrangements for keeping the town centre operational, will need to be submitted for approval.

- 7.26 It is considered, subject to the recommended conditions to allow full assessment at Reserved Matters stage, that the proposed demolition works, and principle of the Outline Planning element comply with adopted Policy LPA06 and the objectives of the 'Ensuring a choice of Travel SPD (Supplementary Planning Documents)'.

#### Renewable and Low Carbon Energy

- 7.27 Local Plan policy LPC13 - Renewable and Low Carbon Energy Development states that new developments for housing, employment or other uses will be required to meet high standards of sustainable design and construction and minimise carbon emissions equivalent to CSH level 4, i.e. 19% carbon reduction against Part L 2013 unless proven unviable. The Policy states that to this end they should use energy efficiently and where feasible incorporate decentralised energy systems that would use or generate renewable or other forms of low carbon energy. Large scale schemes that would generate a significant source or demand for heat should also be supported by evidence considering the feasibility of serving the development by means of a district heating scheme.
- 7.28 The application has been submitted with an Energy and Sustainability Report which outlines that an initial energy strategy optioneering exercise has been undertaken in order to establish what passive design, clean systems and renewable technology interventions are available to achieve energy and sustainability Key Performance Indicators (KPIs). It states that as the detailed design of the scheme is progressed,

further investigation will be carried out to finalise the Strategy. The Energy Strategy of the site has been assessed to be heat dominant, with the cooling loads assessed to be lower than 50% of the overall energy demand, which suggests that if a heat network is considered an ambient loop may be less efficient than a traditional low temperature heat network. If a heat network is considered to be technically possible and efficient, it may be possible to utilise the St Helens Canal to generate heat by a water-to-water heat pump technology which would deliver efficient and reliable heat to the development.

- 7.29 Given the requirements of the Local Plan and the lack of clarity in the application as to how the development will meet the standards required, it is considered that a condition requiring those details with each reserved matter phase of the scheme be included as part of any permission.

#### S106 Legal Agreement

- 7.30 Policy LPA07 – Infrastructure Delivery and Funding sets out how the Council will ensure satisfactory provision of all forms of infrastructure that are required to serve the needs of the community, and how Developers Contributions will be secured when appropriate. In relation to existing infrastructure, the Policy states that it will be protected when there is an identified need for it and supporting improvement of infrastructure when there is an identified need for it.
- 7.31 LPA07 states that subject to compliance with relevant legislation and national policy, development proposals will be expected to include or contribute to the provision, improvement, or replacement of infrastructure that is required to meet needs arising from the development proposal. Whilst part of the application is for community uses and public realm improvement works, which will provide new infrastructure, and the development site is sustainably located so as to not require contributions for improving public transport connections, the application also includes up to 423 dwellings. This number of dwellings will create a need arising from the development proposed.
- 7.32 The application seeks outline planning permission for up to 423 residential dwellings, with the precise number of dwellings and the mix in terms of numbers of bedrooms being determined at reserved matters stage. It is anticipated that there will be some family dwellings and as such school places will be required. When calculating the number of school places required, one-bedroom units are not included. If 423 dwellings with two or more bedrooms were constructed, then based on current calculations a total of 16 primary and 7 secondary school places would be required. While there is currently some surplus, there is unlikely to be sufficient surplus to absorb the required additional need. It is therefore proposed to secure appropriate financial contributions towards the provision of the required school places. The actual contributions will be dictated by the housing mix, potentially offset of any surplus capacity, and the added rate of inflation given some of the dwellings may not be completed until the final phase of the development. It is proposed, subject to Planning Committee approval, that the S106 legal agreement securing the contributions is completed under delegated powers by the Head of Planning prior to the granting of planning permission. The S106 would secure the obligation, with the final amount to be determined by the Reserved Matters applications.



- 7.33 Policy LPC02 takes a zonal approach to affordable housing requirements informed by the St Helens Local Plan Economic Viability Report (2018), which demonstrates there are geographical disparities in viability across the Borough. The application site falls within Affordable Housing Zone 1 (Town Centre), and as such there is no requirement for the application to provide affordable housing due to viability constraints.
- 7.34 Policy LPD03 requires new residential development of 40 dwellings or more to provide new open space. As presented, the proposals include the creation of a substantial 0.9 hectare 'Discovery Park' within the application site; however, this is indicative as all matters have been reserved. A planning condition is proposed to secure details of the proposed open spaces and the triggers for its implementation. The onsite provision of the Public Open Space means that no off-site contribution is required within the proposed legal S106 Agreement.
- 7.35 The proposal for up to 423 dwellings will create a demand for additional health services. The applicants Environmental Statement notes that there are 12 GP surgeries within one mile of the application site, with an existing surplus in patient spaces. But that the addition of between 830 and 1,139 new residents could add pressure to existing GP services for which a financial contribution may be necessary. They state that alternatively, a health centre could be developed on site as part of the proposed development. The Integrated Care Boards (ICB) that are responsible for GPs in St. Helens have not made any request for contributions towards providing GP services as part of this application. It is understood that they are currently undergoing an assessment of their current and future demand/capacity, such that future requests can be made on evidence, and appropriate, justified, contributions that are necessary to make development acceptable, can be secured.

#### Other Matters

- 7.36 The applicants have submitted a shadow Habitat Regulation Assessment (HRA), which has been assessed by MEAS (Merseyside Environmental Advisory Service) and who have advised the LPA that it can be adopted by the Council, thus satisfying obligations under the Habitat Regulations.
- 7.37 The application was supported by an Ecological Report and Bat Survey. The bat survey confirmed that there are no bats present within the buildings to be demolished. A condition is proposed to secure bat and bird boxes being incorporated in the designed scheme as a habitat enhancement.
- 7.38 The application was supported by a Drainage Strategy. United Utilities have expressed concern that the illustrative siting of buildings may need amending. As all matters are reserved, a detailed drainage scheme is required at Reserved Matters stage. A condition securing drainage information prior to the demolition is also proposed. The applicants have been made aware of the issues raised by United Utilities to allow them to address the concerns when designing the detail.

## **8. CONCLUSION**

- 8.1 This application seeks permission for the proposed demolition works and principle of a

comprehensive regeneration programme that enables delivery of the adopted St Helens Town Centre Masterplan Development Framework.

- 8.2 The application was supported by several documents that establish the principal approach to the proposed development, which is based on impacts arising from the submitted parameters reports. It is considered that the information submitted has evidenced, subject to a S106 Agreement and additional information (via Reserved Matters and submissions pursuant to planning conditions), that the proposal can be implemented to create a high-quality mixed-use development.
- 8.3 The proposal complies with the aspirations of the Development Plan, including the Spatial Strategy and Development Principles. The requirement of policy LPB01 -St Helens Town Centre and Central Spatial Area to deliver a comprehensive redevelopment of the Town Centre and Central Spatial Area, including new commercial activity, upgraded infrastructure, the provision of quality housing, and the overall improvement of the social and economic viability of the area are accorded with.
- 8.4 The proposal is also in accordance with the Councils Housing Strategy, in delivering a range of high-quality accommodation to the town centre, and the vision for the St Helens Town Centre Masterplan Development Framework to achieve development that is of high quality, is sustainable and has the people of St Helens at its heart.
- 8.5 The proposal can be considered to be sustainable development. It is therefore recommended that planning permission be granted.

## 9. RECOMMENDATION

- 9.1 That the Shadow HRA submitted with the application be adopted as the Council's own HRA and that the decision to Grant Planning Permission be delegated to the Head of Planning, with that decision to be subject to the completion of a Section 106 agreement and a schedule of appropriate conditions (and any other conditions or amendment to these condition they consider necessary to make otherwise unacceptable development acceptable)

The Section 106 Agreement is to secure a financial contribution (and the phasing of the payment of this contribution to be prior to occupation of the development) towards education in accordance with Policy LPA07.

### **Grant Full planning permission for relevant demolition in a conservation area subject to the following conditions:**

- 1 The works hereby permitted must be begun within 3 years of the date of this decision notice.
- 2 The demolition works hereby approved shall be carried out in accordance with the following plans and documents:
  - Site Location Plan MPS\_04\_1100 Rev. A

- Demolition Plan MPS\_04\_1211 Rev. C
  - Existing Elevations to be Demolished (Sheet 1) MP\_07\_1100 Rev. B
  - Existing Elevations to be Demolished (Sheet 2) MP\_07\_1101 Rev. B
  - Existing Elevations to be Demolished (Sheet 3) MP\_07\_1102 Rev. B
- 3 No part of the demolition works hereby approved shall commence until a drawing identifying the phases of demolition / development for the entire site has been submitted to and approved in writing by the Local Planning Authority.

The demolition works shall be carried out in accordance with the approved phasing drawing unless otherwise agreed in writing.

- 4 No phase of the demolition works hereby approved shall commence until a detailed scheme for that phase (as defined by Condition 3) indicating procedures, measures and mitigation for the management of flood risk assets and flood water routing during and after construction has been submitted to and approved in writing by the Local Planning Authority.
- 5 No phase of the demolition works hereby approved (as defined by Condition 3), shall commence until a Tree Protection Plan for the retained trees within the relevant demolition phase (as shown in Arboricultural Impact Assessment in Support of Hybrid Planning Application July 2022, Document Ref 9228.001. Version 5) has been submitted to and approved in writing by the Local Planning Authority. The tree protection shall be installed in accordance with timescales submitted within the Tree Protection Plan.
- 6 Prior to the commencement of each phase of demolition hereby approved (as defined by Condition 3) a Demolition Management Plan comprehensively detailing the phasing and logistics of demolition for that phase shall be submitted to and approved in writing by the Local Planning Authority. The Demolition Management Plan shall include, but not be limited to:
- Contractors and associated works vehicles traffic routes, including provision for access to the site;
  - Entrance / exit from the site for visitors / contractors / deliveries;
  - Siting of temporary containers;
  - Parking for contractors;
  - Identification of working space and extent of areas to be temporarily enclosed and secured during each phase of demolition;
  - Temporary road / areas of hard standing;
  - Schedule and temporal restrictions for large vehicles delivering / exporting materials to and from site;
  - Waste management;
  - Details of street sweeping / street cleansing / wheel wash;
  - Details of measures to minimise the spread of airborne dust;
  - Details of any temporary generators;
  - Hours of working;
  - Phasing of works;

- Details of including any interim measures for any displaced car, cycle parking and taxi ranks;
- Details of including any interim measures for public access and service areas for adjacent buildings and sites during the times of the demolition;
- Details of turning facilities for use by construction / demolition vehicles; and
- The plan shall also include interim measures, where necessary, for the continued operation of traders during the demolition.

The demolition shall be carried out in full accordance with the approved Demolition Management Plan, unless otherwise agreed in writing with the Local Planning Authority.

- 7 No tree felling, or demolition works are to take place during the period 1 March to 31 August inclusive. If it is necessary to undertake works during the bird breeding season, then all buildings and trees are to be checked first by an appropriately experienced ecologist to ensure no breeding birds are present. If present, details of how they will be protected are required to be submitted for approval in advance of works taking place.
- 8 No part of the demolition of 39-41 Hall Street hereby approved shall take place until a Written Scheme of Investigation (WSI) for a programme of historic building investigation works of these properties has been submitted to and approved in writing by the Local Planning Authority. The WSI must include the following:
  - A programme and methodology of site investigation and recording.
  - A programme for post-investigation reporting to include production of a final report of the significance of the historic building.
  - Provision for appropriate publication and dissemination of the archaeology and history of the historic building.
  - Provision for archive deposition of the report and records of the site investigation.
  - Nomination of a competent person or persons / organisation to undertake the works set out within the approved WSI.
- 9 Buildings with bat roost potential shall be demolished during the winter months November to February inclusive. If this is not possible a licensed bat ecologist is required to directly supervise the roof slates and other potential roost features as set out within the submitted Ecological Assessment version 1.4, TEP July 2022.

**Grant outline planning permission for development of a mix of uses, comprising hotel use (Use Class C1); residential units (Use Class C3); commercial, business and service uses (Use Class E(a-g)); local community & learning uses (Use Class F1(b-e) and F2(b)); and Sui Generis uses, with associated access, servicing, parking, public realm, and landscaping, with all matters (Access, Appearance, Landscaping, Layout and Scale) reserved for future determination. Subject to the following conditions and the completion of a satisfactory Section 106 legal agreement to be approved by the Head of Planning under delegated powers.**

- 10 Application for approval of the first reserved matters must be made not later than the expiration of three years beginning with the date of this permission. Application for the last approval of reserved matters must be made within seven years of the date of this

permission. The development relevant to each reserved matter must be begun not later than the expiration of two years from the approval of that reserved matter.

- 11 Before any phase of the development hereby granted outline permission is commenced, approval shall be obtained from the Local Planning Authority with respect to the reserved matters for that phase, namely (the layout, scale, appearance, access, and landscaping including details of landscape management and maintenance (hereinafter called “the reserved matters”)). For clarification phases are those as defined by Condition 15.
- 12 The development hereby approved in outline shall be carried out in accordance with the following plans and documents:
  - Site Location Plan MPS\_04\_1100 Rev. A
  - Parameters Plan Maximum Development Heights MPS\_04\_2212 Rev. L
  - Parameters Plan Minimum Development Heights MPS\_04\_2213 Rev. E
  - Parameters Plan Development Plots MPS\_04\_2209 Rev. H
  - Parameters Plan Ground Floor Uses MPS\_04\_2210 Rev. J
  - Parameters Plan Upper Floor Uses MPS\_04\_2211 Rev. J
  - Development Summary Schedule 08.07.22
- 13 This planning permission is for a maximum of 423 residential units.
- 14 Notwithstanding the provisions of the Town and Country Planning (Use Classes) Order 2021 (as amended) or any subsequent Orders or statutory provision re-enacting the provisions of these Orders, no units shall be permitted to change use from its first use, except with the prior grant of a further planning permission.
- 15 The first reserved matter(s) application(s) shall include a drawing identifying the phases of development / demolition for the entire site.

All reserved matter application(s) (including the first) should identify which phase(s) of the development/demolition they are within and where appropriate an updated plan which includes details of which reserved matters applications for each phase have been approved.

All phasing within supporting reports and plans submitted with all reserved matters applications should be in accordance with the submitted drawing.

All reserved matter application(s) should relate to entire phases of development, and not parts of phases of development. To clarify more than one entire phase of development can be included within the same reserved matter application(s).

- 16 No above ground construction of any phase (as defined by Condition 15) shall commence until a detailed materials schedule for that phase has been submitted to and approved in writing by the Local Planning Authority. The schedule shall include a strategy for the entire site including precise details of any materials previously approved as part of this application. Details of materials of adjacent sites shall also be



included for context and to allow a full assessment.

- 17 All reserved matters application(s) for layout, scale, access, and landscaping shall be accompanied by details of the proposed ground and floor levels for the relevant phase (as defined by Condition 15), including the finished floor levels of all buildings previously approved as part of this application. The submitted details shall include a number of sections across the site, which shall indicate existing and proposed ground levels, together with the finished floor levels of any proposed dwellings/buildings through which the sections run and shall extend beyond the site boundaries to include any surrounding adjacent properties. The development shall thereafter be implemented in accordance with the approved details.
- 18 All the reserved matters for layout, scale, access, and landscaping shall be accompanied by a scheme(s) identifying areas of motor vehicular parking, cycle parking, service areas and access routes, taxi facilities, drop-off and pick-up areas within the relevant phase (as defined by Condition 15).

The scheme(s) shall include but not be limited to the following

- a. A strategy for all parking provision across the entire site;
- b. Details of location;
- c. Details of access;
- d. Details of drainage, surfacing and markings; and
- e. A Management Strategy identifying phasing and triggers for the implementation of parking, designated spaces / areas for traders and the public, and measures to be implemented to enforce the strategy.

The areas and facilities so provided for the relevant phase (as defined by Condition 15) shall be installed, managed, and retained in accordance with the approved scheme.

- 19 All the reserved matter(s) applications for layout, scale, access, and landscaping shall be accompanied by a detailed highway scheme for that phase (as defined by Condition 15). The scheme shall include but not be limited to the following:
  - a. The proposed highway layout including the highway boundary
  - b. A timetable including triggers for the implementation of all the proposed works
  - c. Dimensions of all existing and proposed carriageway, cycleway, footway, and verges including details of any proposed alterations
  - d. Details of existing and proposed visibility splays
  - e. The proposed buildings and site layout, including levels and vehicular and pedestrian access
  - f. Cycle infrastructure (lanes, crossings)
  - g. Pedestrian crossings
  - h. All types of existing and proposed surfacing (including tactile paving), kerbing and edging
  - i. Full working drawings for any structures which affect or form part of the highway network

- j. Details of any carriageway reallocation
- k. Highway arrangements and raised tables
- l. Delivery and servicing arrangements
- m. Speed reductions (and any necessary locations for new Traffic Regulation Orders)
- n. An updated Transport Statement relevant to that phase

- 20 Prior to the first use or occupation of any building, an Interim Travel Plan shall be submitted to and approved in writing by the Local Planning Authority. Following approval of the Interim Plan each individual occupier that employs 10 or more persons shall, prior to occupation, appoint a Travel Plan Coordinator. The Travel Plan Coordinator shall be responsible for the implementation, delivery, monitoring and promotion of the Travel Plan, including the day-to-day management of the steps identified to secure the sustainable transport initiatives. The details (name, address, telephone number and email address) of the Travel Plan Coordinator shall be notified to the Council as Local Planning Authority upon appointment and immediately upon any change.

Within six months of occupation, each Travel Plan Coordinator shall submit their own Travel Plan to the Local Planning Authority for approval and the approved plans shall thereafter be implemented within one month of approval by the Local Planning Authority.

The Travel Plan(s) shall include immediate, continuing, and long-term measures to promote and encourage alternative modes of transport to the single-occupancy car. For the avoidance of doubt, the travel plan shall include but not be limited to:

- a. Production of SMART targets to reduce single-occupancy car traffic movements to and from the respective site/planning unit;
- b. Updating of timetables and fare information on a regular basis; and,
- c. Involvement of employees;
- d. Information on existing transport policies, services and facilities, travel behaviour and attitudes;
- e. Access by all modes of transport;
- f. Resource allocation including Travel Plan Coordinator appointment and confirmation of the name holder and budget;
- g. A marketing and communications strategy;
- h. Appropriate measures and actions to reduce car dependence and encourage sustainable travel;
- i. An action plan including a timetable for the implementation of each such element of the above; and
- j. Mechanisms for monitoring, reviewing, and implementing the travel plan.

The approved Travel Plan(s) shall be implemented in accordance with the timetable contained therein and shall continue to be implemented as long as any part of the development is occupied and in use.

An Annual Report shall be submitted to the Council no later than 1 month following the anniversary of the first occupation of the development for a period of 5 years. The Annual

Report shall include a review of the travel plan measures, monitoring data and an updated action plan.

- 21 All the reserved matter application(s) for layout, scale, access, and landscaping shall be accompanied by a detailed drainage scheme for the relevant phase (as defined by Condition 15). No development (excluding demolition) shall take place until a scheme for the disposal of foul and surface water from the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The approved development shall be constructed in accordance with the approved scheme. The scheme shall be based on the hierarchy of drainage options contained in the Planning Practice Guidance and St Helens Council adopted SuDS Guidance, where relevant, shall demonstrate compliance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national and local standards. The scheme shall include but not be restricted to the following:
- a. A drainage strategy referencing and using the Councils SuDS Guidance and Checklist as a base which takes account of adjacent phases.
  - b. Assessment of possible improvement opportunities to the drainage run-off and storm water storage for the Town Centre and downstream water catchment.
  - c. Assessment of the SuDS Drainage Hierarchy
  - d. Evidence of an assessment of the site conditions to include site investigation (including contamination) and test results to confirm drainage outfall methods and infiltrations rates if directed to ground or other permeable features.
  - e. Information about the lifetime of the development design storm period and intensity (1 in 30 and 1 in 100 year, plus allowance for climate change and urban creep), discharge rates and volumes (both pre and post development), temporary storage facilities, the methods employed to delay and control surface water discharged from the site, and the measures taken to prevent flooding and pollution of the receiving groundwater and/or surface waters, including watercourses.
  - f. Any works required off-site to ensure adequate discharge of surface water without causing flooding or pollution (which should include refurbishment of existing culverts and headwalls or removal of unused culverts where relevant).
  - g. Flood water exceedance routes, both on and off site.
  - h. Detailed design drawings including; details of inlets, outlets, and flow controls / long and cross section drawings of proposed drainage system(s), including design levels / details of appropriate water quality treatments; Designs to be accompanied with relevant storm water simulations assessments (both report form and electronic software files used).
  - i. A timetable for implementation, including phasing as applicable.
  - j. Detailed construction plan scheme must be provided indicating procedures and measures for the management of flood risk assets and surface water flood outting and storm exceedance routes during construction.
  - k. Details of a management and maintenance plan for the drainage system after completion, including any arrangements for adoption by an appropriate public body or statutory undertaker
- 22 No part of the development hereby permitted in outline shall commence until a Phase 2 site investigation and risk assessment for the relevant phase (as defined by Condition

15) has been submitted to and approved in writing by the Local Planning Authority.

The assessment shall include a strategy for addressing piling or any other foundation designs using penetrative methods, if proposed.

Should the site investigation and risk assessment identify any remediation requirements then a remedial strategy including a validation methodology, should be included.

If any remediation works are required, then prior to the first use/ occupation of any part of the building sited within the defined area of remediation a validation/closure report shall be submitted and approved in writing by the Local Planning Authority.

All such reports shall be completed by a competent person in accordance with government and Environment Agency guidance, namely "Land Contamination: Risk Management" (<https://www.gov.uk/guidance/land-contamination-how-to-manage-the-risks>).

- 23 All the reserved matter application(s) for layout and access shall be accompanied by a coal mining report for the relevant phase (as defined by Condition 15), if applicable. The report shall include but not be limited to the following:
- a. A scheme of intrusive site investigations, designed by a competent person and adequate to properly assess the ground conditions on the site and establish the risks posed to the development by past coal mining activity (shallow mining and mine entries);
  - b. A report of findings arising from the intrusive site investigations and any remedial and / or measures necessary, including the submission of the proposed layout plan which identifies the location of any on-site mine entries (if found present) including appropriate zones of influence for all mine entries, and the definition of suitable 'no-build' zones.
  - c. A timetable for the implementation of the remedial works.
- 24 All reserved matter(s) applications for scale, layout and appearance shall be accompanied by an Energy Saving Statement for the relevant phase (as defined by Condition 15), setting out details of all proposed measures to be incorporated within the proposal.
- 25 All reserved matter(s) applications for scale, layout and appearance and access shall be accompanied by updated noise assessments for the relevant phase (as defined by Condition 15).
- 26 All reserved matter application(s) for appearance for any building that may include a commercial kitchen shall evidence how the design of the building can include provision for external flues/ ventilation.
- 27 All reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by a schedule for the hours of operation, servicing,

and deliveries of the relevant building/phase (as defined by Condition 15). The proposal shall be operated in full accordance with the approved scheme.

- 28 Prior to any above ground works hereby approved a scheme for the signage strategy for all buildings within the relevant phase (as defined by Condition 15) shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be fully implemented and retained at all times unless otherwise approved in writing by the Local Planning Authority.

The scheme may need to be accompanied by an application for advertisement consent.

- 29 All the reserved matters application(s) for layout, scale, access, appearance, and landscaping shall be accompanied by details of any proposed lighting within the relevant phase (as defined by Condition 15). Details submitted should relate to existing and proposed lighting within the phase and should include but not be limited to the following:

- a. A strategy for the entire application site
- b. Detailed lighting designs;
- c. Luminaire technical details;
- d. Luminaire locations;
- e. Illuminance levels;
- f. Lighting standard achieved;
- g. Control methods; and
- h. Proposed times of operation of lighting
- i. Details of timescales/triggers for installation

The scheme shall be installed and retained in accordance with the approved scheme.

- 30 Details of any external CCTV to be installed shall be submitted with the reserved matter application(s) for layout, scale, access, appearance, and landscaping of the relevant phase (as defined by Condition 15). The scheme shall include details of location and appearance and should be plotted on drawings that show existing and proposed streetlighting and trees.

The approved scheme shall be fully implemented at all times the development is in operation.

- 31 All the reserved matter application(s) for layout, access and appearance shall be accompanied by a scheme for waste management and storage for the relevant phase (as defined by Condition 15). The scheme shall relate to public and commercial waste and shall include but not be limited to the following:

- a. A strategy for the entire site
- b. Location of proposed and existing refuse bins/ storage areas
- c. Design of bins/ means of enclosure of storage areas
- d. Details of collection points



e. Strategy for collection of waste

The approved scheme shall be fully implemented at all times the development is in operation.

- 32 All reserved matter application(s) for landscaping for each phase of the development hereby permitted (as defined by Condition 15) and any reserved matter application(s) for layout and scale that includes residential units within the phase shall be accompanied by a Public Open Space Strategy. The Strategy shall include but not be restricted to details of management and maintenance, phasing, and triggers for implementation.
- 33 All the reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by a tree survey and Arboricultural Impact and Method Statement, relevant to that phase (as defined by Condition 15).
- 34 All the reserved matters application(s) for layout and landscaping for each phase of the development hereby permitted (as defined by Condition 15) must include an updated Biodiversity Net Gain Assessment, including up to date DEFRA Biodiversity Net Gain Metric, which demonstrates that there is no net loss of biodiversity across the site as a result of the proposals within any reserved matters application, relevant to that phase (as defined by Condition 15).
- 35 All the reserved matters application(s) for layout and landscaping must include a Precautionary Working Method Statement (PWMS) which incorporates prevention and mitigation measures for protected species and retained habitats.
- 36 All the reserved matters applications for siting and landscaping shall be accompanied by a Method Statement for the removal/control of any invasive plant species (as defined within the Wildlife and Countryside Act 1981, as amended) which fall within the application site. Development shall be in accordance with the Method Statement approved. The Method Statement shall include:
  - a. A plan showing the location of the invasive species, the plan should include the boundaries of the phases (as defined by Condition 15).
  - b. The methods that will be used to prevent the plant/s spreading further, including demarcation.
  - c. The methods of control that will be used, including details of post-control monitoring and
  - d. How the plants will be disposed of after treatment/ removal.

All subsequent reserved matter application(s) for siting and landscaping which contain invasive species shall be accompanied by an updated Method Statement.

- 37 Prior to occupation, details of the design, number and location of proposed bat and bird boxes relevant to that phase (as defined by Condition 15) and timescales/triggers for installation shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved

details.

- 38 All the reserved matters application(s) for layout, scale, appearance, access, and landscaping shall be accompanied by an updated Heritage Assessment for the relevant phase (as defined by Condition 15). The updated study shall take account of approved parameters and precise details of all buildings approved as part of this application.
- 39 No part of the development hereby approved in outline shall commence until a Construction Management Plan comprehensively detailing the phasing and logistics of construction for the relevant phase (as defined by Condition 15) has been submitted to and approved in writing by the Local Planning Authority.

The Construction Management Plan shall include, but not be limited to:

- Contractors and associated works vehicles traffic routes, including provision for access to the site;
- Entrance / exit from the site for visitors / contractors / deliveries;
- Siting of temporary containers;
- Parking for contractors;
- Identification of working space and extent of areas to be temporarily enclosed and secured during each phase of construction or associated works;
- Schedule for large vehicles delivering / exporting materials to and from site;
- Waste management;
- Details of street sweeping / street cleansing / wheel wash;
- Details of measures to minimise the spread of airborne dust;
- Details of any piling works and measures to mitigate associated noise;
- Details of any temporary generators;
- Hours of working;
- Phasing of works;
- Details of including any interim measures for any displaced car, cycle parking and taxi ranks;
- Details of including any Interim measures for public access and service areas for adjacent buildings and sites during the times of the construction or associated works operation;
- Details of turning facilities for use by construction vehicles;
- The plan shall also include interim measures, where necessary, for the continued operation of traders during the construction or associated works operation;
- Interim measures for traders parking service areas during times of the construction or associated works operation

The construction and associated works operation shall be carried out in full accordance with the approved Construction Management Plan, unless otherwise agreed in writing with the Local Planning Authority.

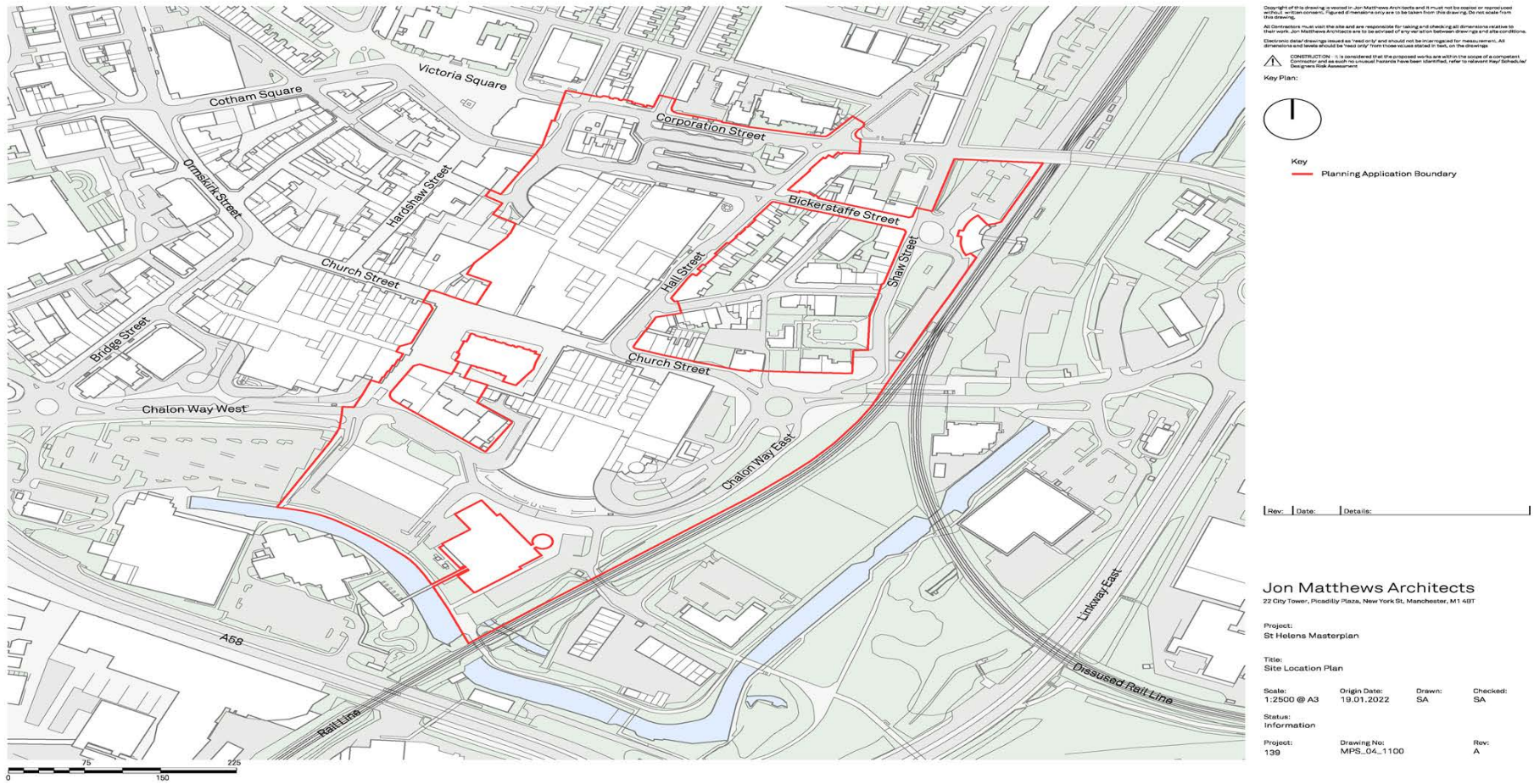
- 40 No phase of construction works, excluding demolition, shall take place until a Written Scheme of Investigation (WSI) for a programme of archaeological works for the

relevant phase (as defined by Condition 15) has been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

- 41 All reserved matters application(s) for layout, scale and landscaping shall be accompanied by an updated Wind Assessment
- 42 All reserved matters application(s) for layout, scale landscaping and access shall include an updated Air Quality Report for the relevant phase (as defined by Condition 15) setting out any necessary mitigation measures. Any mitigation identified as necessary shall be implemented as part of the development hereby approved.
- 43 All reserved matters application(s) for layout or scale shall include a Renewable and Low Carbon Energy Statement which details how the proposal will minimise carbon emissions equivalent to CSH level 4, i.e. 19% carbon reduction against Part L 2013 for the relevant phase (as defined by Condition 15). The development thereafter shall be carried out in accordance with the approved details.

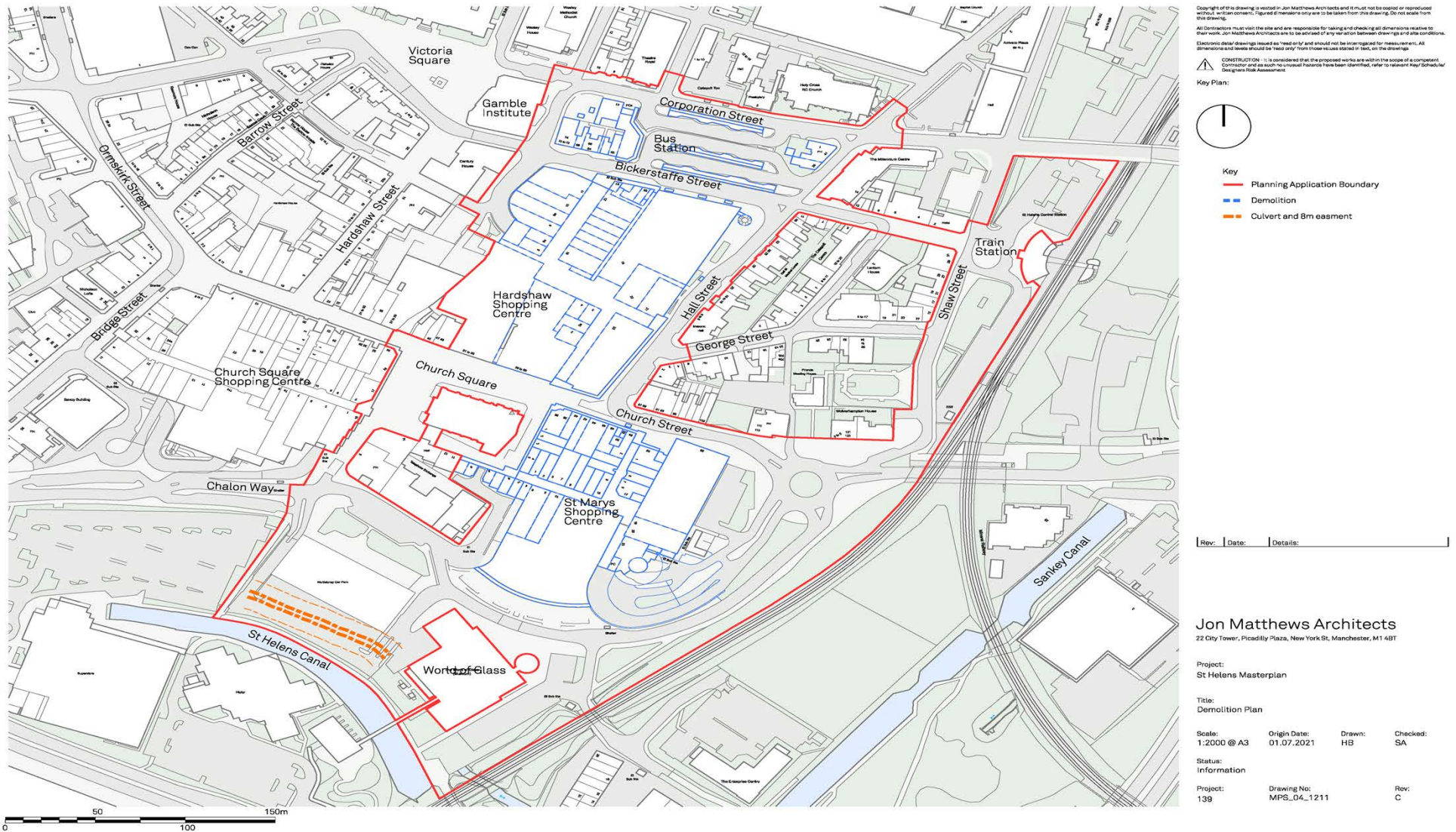
10. IMAGES

Location Plan



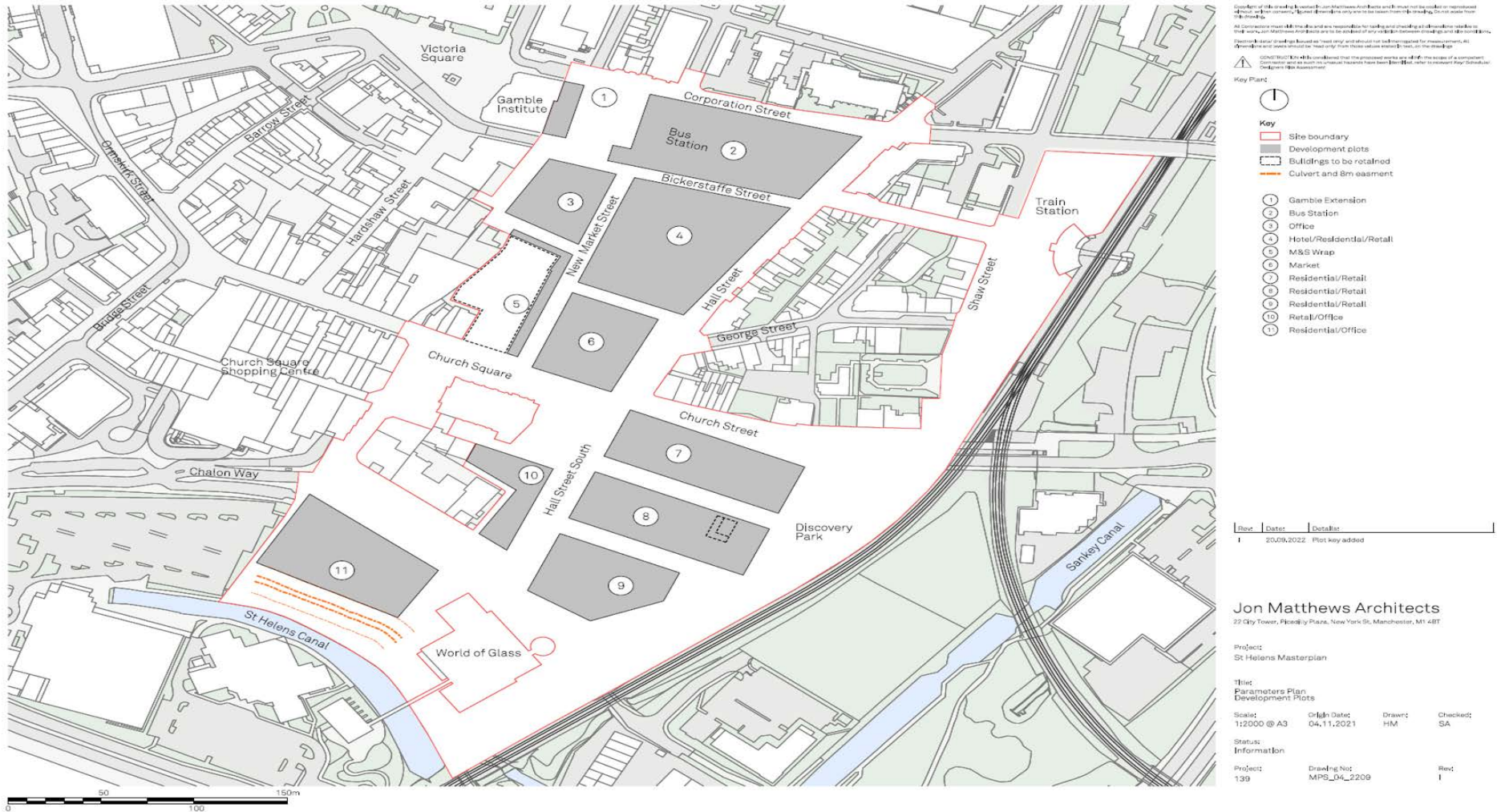


Demolition Plan



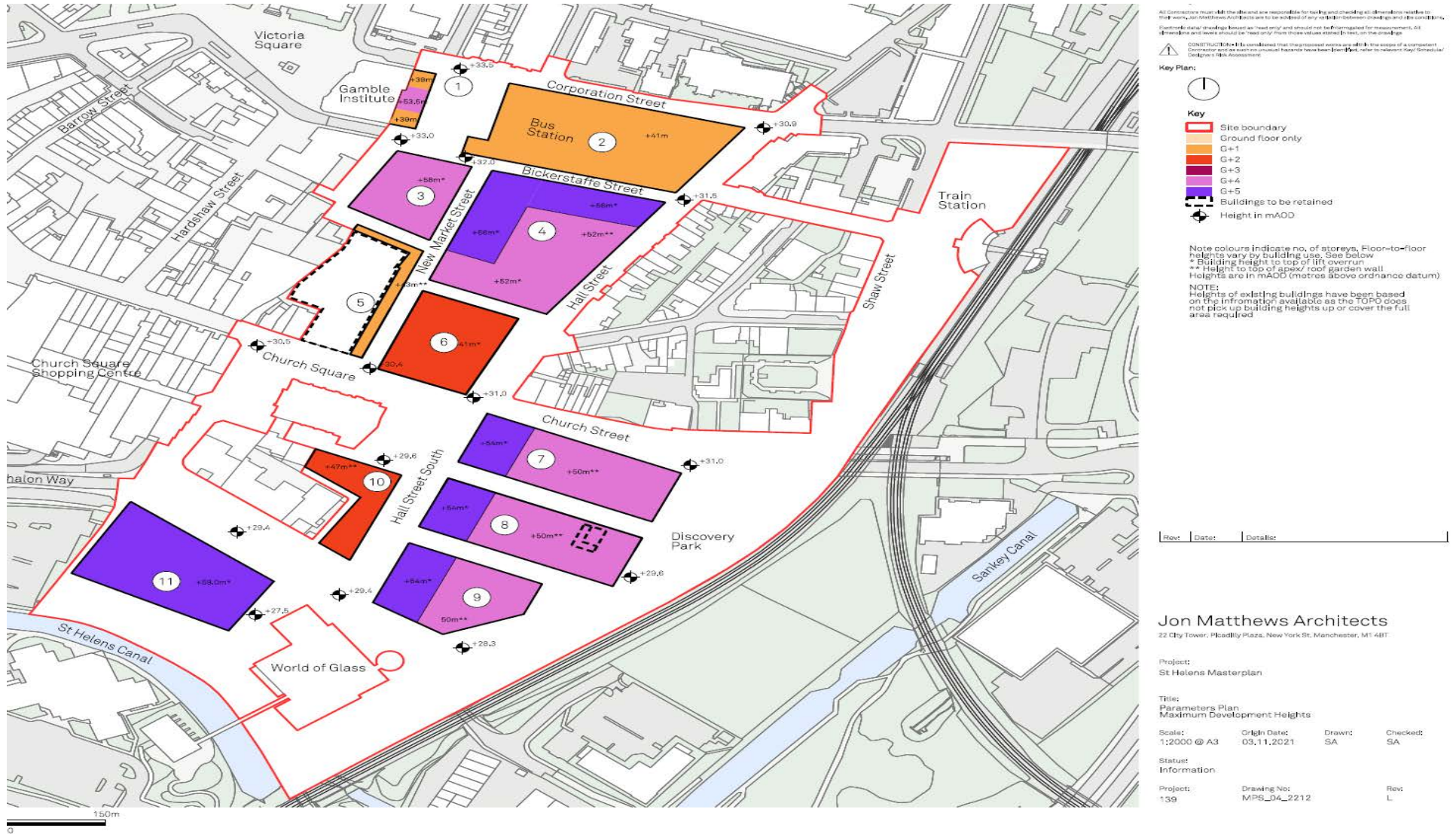


Parameters Plan – Development Plots





# Parameters Plan Maximum Heights







## St Helens Borough Council

### Statement of Community Involvement – Addendum

#### Temporary measures in response to the Covid-19 pandemic

May 2021

## Introduction

The Covid-19 pandemic has had an impact on day to day life over the past months, changing the way in which people behave and undertake daily tasks to reduce the risks associated with contracting and / or spreading the virus. Changes in behaviour have not just been voluntarily made by an individual or organisation's own choice, but have also been driven by the need to ensure compliance with Government rules and restrictions in response to curbing the spread of the virus, to protect the NHS.

Some of the restrictions having the biggest impact include:

- Stay at home orders
- Social distancing
- Closure of services / buildings
- Limitations on public gatherings / multiple households meeting

Whilst the previous months have seen restrictions fluctuating between periods of tightening and loosening, the impacts continue to be felt daily, and will likely continue to be felt to some extent for more time to come. For example, there could be further lockdowns and restrictions introduced (either nationally or locally) should any further waves of Covid-19 be experienced.

Consequently, this has an impact on the Council's plan making processes, and most notably in relation to public consultation in the plan making process. For example, Covid-19 has resulted in some of the libraries in St Helens being closed for prolonged periods. Even when open, restrictions may be in place in terms of capacity. On this basis, it is not possible to guarantee that hard copies of documents will be available in the libraries for viewing during periods of public consultation.

The current St Helens Council Statement of Community Involvement (SCI), entitled 'Revised Statement of Community Involvement' sets out how the community is to be engaged in the planning processes within St Helens Borough (in respect of both plan making and decision taking). It was adopted in November 2013 and has been complied with in the preparation of planning policy documents to date. This SCI addendum must be read in conjunction with the 2013 SCI. The provisions of this 2021 SCI addendum take precedence over the provisions of the 2013 SCI during the temporary period that the SCI addendum is in effect.

## Government Guidance

The Government's National Planning Practice Guidance advises the following:

***"How can a Statement of Community Involvement be reviewed and updated to comply with COVID-19 guidance?"***

*Local planning authorities will need to assess their Statements of Community Involvement to identify which policies are inconsistent with current guidance on staying at home and away*



*from others or any superseding guidance. This could include, for example, holding face-to-face community consultation events or providing physical documents for inspection.*

*The local planning authority should then make any temporary amendments that are necessary to allow plan-making to progress, and that continue to promote effective community engagement by means which are reasonably practicable ....”*

(Source: National Planning Practice Guidance, <https://www.gov.uk/guidance/plan-making# covid19>, paragraph 078)

Further to this, and in reflection of the difficulty of doing so in a Covid-19 compliant way, the Government has amended The Town and Country Planning (Local Planning) (England) Regulations 2012 to remove the requirement to make certain documents available in hard copy until 31 December 2021, as currently set out.

It is therefore clearly the case that SCIs can and should be updated to reflect updated plan making processes, which comply with Government restrictions in response to the Covid-19 pandemic.

### Overarching consultation principles

Whilst the impacts of Covid-19 mean that there need to be some changes over the following months to how the Council is able to consult as part of the plan making process, the Council remains committed to engaging with all interested individuals and organisations, as a priority. This will include providing access to consultation documents, and opportunities to make comments through public consultation, providing the opportunity to inform, influence and shape the plan making process.

It is acknowledged that digital exclusion remains an issue for some in the Borough. Where people may struggle to access information or engage in the process digitally, they are encouraged to contact the Planning Policy Team for assistance.

This addendum seeks to provide clarity around the process, and limits proposed changes to only those needed as an absolute necessity to ensure consultation can be undertaken in a Covid-19 compliant way, in accordance with Government rules, and for only as long as such measures are necessary. This is done to protect our communities and staff, and to reduce the spread of the virus.

### Temporary Measures

In response to the pandemic and Government restrictions, this addendum clarifies how the Council will consult with the community throughout this challenging period. This means:

- 1) The Council cannot commit to making hard copies of documents available from public buildings (including the St Helens Town Hall and public libraries) during public consultation periods, or beyond. Likewise, documents in hard copy cannot be

provided to other organisations (such as Parish Councils). Instead, all documents will be made available on the Council's website, specifically the Local Plan pages, and with regard to the Local Plan Examination, all relevant documents will be available on the main Local Plan Examination and Examination Library pages;

- 2) Local Plan public consultations that need to be undertaken whilst restrictions remain in place will be done without consultation events / public meetings taking place in a physical setting. This is to ensure the safety of all potential attendees and staff, ensuring Government rules around public gatherings and social distancing can be respected. Instead, consultation will be undertaken digitally as appropriate, and be made as accessible as possible to all;
- 3) The distribution of consultation posters around supermarkets, Parish halls, and elsewhere cannot be guaranteed; and
- 4) Any other changes of approach required to ensure the safety of the public and Council staff will be allowed, whilst the pandemic has ongoing effects on daily life.

The Council website will continue to be used to provide information, updates on plan making processes as appropriate, and access to documents. The Council's social media channels will also be used to provide information to as wide an audience as possible. The Council will remain open to exploring different and innovative ways in which to engage people in the plan making process through this time.

For those that have difficulty accessing consultation documents digitally, help can be sought by contacting the Development Plans Team using the following details:

Contact Centre  
Wesley House  
Corporation Street  
St Helens  
WA10 1HF

Email: [planningpolicy@sthelens.gov.uk](mailto:planningpolicy@sthelens.gov.uk) or telephone: 01744 676190

Also, where people may have preferred to attend public consultation events to ask questions and gain a better understanding of proposals, they are encouraged to contact the Development Plans Team separately (as above), who will assist in answering questions and providing information where possible.

#### Timescale, and next steps

To reflect similar updates to the legislation as previously referenced, the primary access to documents, both during consultation periods, and beyond, will be in a digital format, and this will remain the case until 31 December 2021. This does not mean that hard copies might not be available to some degree during this period, only that it cannot be committed to. The other measures referred to will remain in place until Government advice changes and restrictions are lifted to such an extent that the Council considers it safe to hold public events in a physical setting or practical to distribute consultation posters.

NOTE: There are 2 options provided that affect Plot 4 and 11 only

08.07.22

ST HELENS MASTER PLAN PHASE 1				
Development Summary Schedule - Option A				
Plot Number	Use Classes	Plot Total GIA (m²)	Notes	Plot Total GIA (m²)
1	E (a-f) Retail/leisure/food & drink/commercial and Sui Generis (drinking establishments)	340	Use to be combined with accessible access to the Gamble Note the plot total is based on the extension footprint only 340m² and potential 130m² space to be located for ancillary bus station facilities	470
	F1(b-e) Learning and non-residential institutions			
	F2(b) Community	130		
	Sui Generis (Ancillary bus station facilities)			
2	Bus Station (Sui Generis)	1,000	Bus station accommodation, does not include the canopies	1,200
	E(a-f) Retail/leisure/food & drink/commercial	150	Approx one unit ground floor	
3	E(g)(i-ii) Office/ Research	7,500		7,700
	E(b) Restaurant/café	200	Approx one unit ground floor	
4	C1 Hotel	5,510	155 Beds	12,967
	C3 Resi	5,574	65 Units, 40 Parking Spaces	
	E(a-f) Retail/leisure/food & drink/commercial	1,753	Retail with some F&B offer at ground floor (excludes hall street) Note the plot total includes potential 130m² space to be located for ancillary bus station facilities	
	Sui Generis (Drinking establishment)			
	Sui Generis (Ancillary bus station facilities)	130		
5	E(a-f) Retail/leisure/food & drink/commercial	650	Ground floor	650
	Drinking establishment (Sui generis)			
6	E(a) Market	2,000	Market hall	2,000
	E(b) F&B Sales		The market will have provision for a food court/ street food offer	
	Drinking establishment (Sui generis)			
7	C3 Resi	9,037	90 Units, 34 parking Spaces	9,590
	E(a-f) Retail/leisure/food & drink/commercial	553	Retail/ commercial overlooking Hall Street South at ground floor	
	Drinking establishment (Sui generis)			
8	C3 Resi	9,037	90 Units, 30 Parking Spaces	9,590
	E(a-f) Retail/leisure/food & drink/commercial	553	Retail/ Commercial overlooking Hall Street South	
	Drinking establishment (Sui generis)			
9	C3 Resi	9,905	88 Units, 26 Parking Spaces	10,650
	E(a-f) Retail/leisure/food & drink/commercial	745	Retail/Commercial overlooking Hall Street South	
	Drinking establishment (Sui generis)			
10	E(a-f) Retail/leisure/food & drink/commercial	750	Ground floor	4,200
	Drinking establishment (Sui generis)			
	E(g)(i-ii) Office Research	3,450	Upper floors	
11	C3 Resi	7,700	90 Units, 45 Parking spaces	8,200
	E(a-f) Retail/leisure/food & drink/commercial	500	Ground floor	
	E(g)(i-ii) Office Research	Option B Only		

NOTE: There are 2 options provided that affect Plot 4 and 11 only

08.07.22

ST HELENS MASTER PLAN PHASE 1				
Development Summary Schedule - Option B				
Plot Number	Use Classes	Plot Total GIA (m²)	Notes	Plot Total GIA (m²)
1	E (a-f) Retail/leisure/food & drink/commercial and Sui Generis (drinking establishments)	340	Use to be combined with accessible access to the Gamble Note: This includes potential 130m² space to be located for ancillary bus station facilities	470
	F1(b-e) Learning and non-residential institutions			
	F2(b) Community			
	Sui Generis (Ancillary bus station facilities)	130		
2	Bus Station (Sui Generis)	1,000	Bus station accommodation, does not include the canopies	1,220
	E(a-f) Retail/leisure/food & drink/commercial	150	Approx one unit ground floor	
3	E(g)(i-ii) Office/ Research	7,500	Approx one unit ground floor	7,700
	E(b) Restaurant/café	200		
4	C1 Hotel	Option A only		11,412
	C3 Resi	9,529	106 Units, 40 Parking Spaces	
	E(a-f) Retail/leisure/food & drink/commercial	1,753	Retail with some F&B offer at ground floor (excludes hall street) Note: This includes potential 130m² space to be located for ancillary bus station facilities	
	Sui Generis (Drinking establishment)			
	Sui Generis (Ancillary bus station facilities)			
	5	E(a-f) Retail/leisure/food & drink/commercial	650	
Drinking establishment (Sui generis)				
6	E(a) Market	2,000	The market will have provision for a food court/ street food.	2,000
	E(b) F&B Sales			
	Drinking establishment (Sui generis)			
7	C3 Resi	9,037	90 Units, 34 parking Spaces	9,590
	E(a-f) Retail/leisure/food & drink/commercial	553	Retail/ restaurant overlooking Hall Street South	
	Drinking establishment (Sui generis)			
8	C3 Resi	9,037	90 Units, 30 Parking Spaces	9,590
	E(a-f) Retail/leisure/food & drink/commercial	553	Retail/ restaurant overlooking Hall Street South	
	Drinking establishment (Sui generis)			
9	C3 Resi	9,905	88 Units, 26 Parking Spaces	10,650
	E(a-f) Retail/leisure/food & drink/commercial	745	Retail/ restaurant overlooking Hall Street South	
	Drinking establishment (Sui generis)			
10	E(a-f) Retail/leisure/food & drink/commercial	750	Ground Floor	4,200
	Drinking establishment (Sui generis)			
	E(g)(i-ii) Office Research	3,450	First Floor	
11	C3 Resi	Option A only		14,508
	E(a-f) Retail/leisure/food & drink/commercial	780	45 Parking spaces, Use E(a-f) ground only	
	E(g)(i-ii) Office Research	13,728	Ground and upper floors	

# The English Cities Fund St Helens Town Centre Development Design & Access Statement

28.07.2022



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Key

 Application Site

 Masterplan Development Framework Boundary



# 1.0 Introduction

Purpose of the Document

This document has been prepared on behalf of the English Cities Fund (ECF) for the hybrid planning application for the proposed St Helens town centre development.

It has been prepared by Jon Matthews Architects (JMA) with input from ECF and the technical team.

We have sought to thoroughly investigate the site and local context, in order to carefully develop our proposal and build upon the unique historic character that St Helens offers.

This document is intended to tell the story of our analysis, our response to the brief and ultimately how we arrived at our development proposal.

Since the submission of planning application P/2022/0212/HYBR, the following changes have been made to the proposed plans:

- On the Area Schedule, the changes which have been made are highlighted in green for ease;
- Plot 11 has been reduced in order to avoid a no-build zone associated with a culvert in this area;
- Changes to the shape/size of parameter plots 1, 2, 3, 4, 5 and 6;
- Upper Floor for Plot 5 has been removed as this is now proposed as a wrap around the existing, former M&S unit to allow access into the former M&S unit from the east, if this building were to be sub-divided in the future;
- Minimum height of Plot 5 has been increased from 33.5m to 37.1m;
- Gamble extension (Plot 1):
  - Minimum height reduced from 38m to 37m;
  - Maximum height amended with a taller middle section;
- Shaw Street has been downgraded from a Primary vehicle street to a Secondary vehicle street;
- Crossing point south of Gamble increased in size and new crossing added to the north east of plot 6; and
- Note re. minimum street widths added to Access and Movement Plan and Public Realm Plan.

Summary of Masterplan

JMA worked on behalf of ECF and St Helens to produce a Masterplan Development Framework for St Helens town centre. The Masterplan Development Framework has been endorsed by St Helens Metropolitan Borough Council (SHMBC), in early 2022, following an extensive public consultation. This Design & Access Statement draws upon information from the St Helens Masterplan Development Framework (MDF).

This section summarises the key points of the Masterplan Development Framework which are relevant to the planning application and which have shaped the brief of this development. The purpose of the St Helens MDF was to produce an innovative new plan to re-position and shrink and link the town centre to be and attractive destination with a varied retail, leisure and cultural offer for all.

The principles set out in this document highlight that this area of the town centre can provide a deliverable, sustainable, mixed-use community, providing new homes and employment space along with associated leisure and amenity to serve this emerging destination and create a sustainable thriving town centre.

Vision

The vision statement below was developed as part of the public consultation undertaken to date and forms part of the endorsed Masterplan Development Framework for St Helens.

The vision for St Helens town centre has been informed by stakeholder engagement together with the ‘#StHelensTogether: Our Borough Strategy 2021-2030’ which was subject to extensive engagement prior to its adoption. It also considers the strategic context and the opportunities and constraints within the town centre which are detailed in the following sections.

The preferred approach, which is consistent with the ‘#StHelensTogether: Our Borough Strategy 2021-2030’, envisages radical transformation of the town centre through development that will nurture, celebrate, and host culture, building upon the town’s creative and innovative gene and positive legacy of its’ industrial heritage.

*‘St Helens...a culturally centred vibrant town centre that matches the borough’s potential with people at the heart’*

“By 2030 St Helens will be culturally centred, will nurture and celebrate its cultural and industrial heritage, will build upon its creative and innovative gene in glass making and foundation-industries to innovate and do things differently, and create new opportunities, including those from the City Region. Vibrancy will be created for the town centre; the town will be a more accessible and enjoyable place to come together and will be a place where the community will learn from one another, improve their physical

and mental well-being and where people want to live, work and visit. Businesses and residents will be attracted to the new opportunities available, will have quality spaces to dwell and will benefit from improved transport and digital connections borough-wide”.

The following strategic objectives will help to achieve the delivery of a culturally centred and vibrant town centre, which is at the heart of the vision for St Helens:

**1. Delivering a diverse, vibrant and animated town centre.**  
To create a town centre that responds to the changing nature of the high-street; consolidating the proportion of retail and concentrating retail provision.

Encouraging new town centre uses, increasing active frontages and offering spaces that attract the local and independent traders that make St Helens truly unique.

Introducing a diverse mix of town centre uses, to include both retail, leisure and other traditional town centre uses, as well as a complementary residential offer.

Promoting the existing cultural offer and improving opportunities for linked visits as well as encouraging the night-time economy.

Introducing temporary or ‘meanwhile’ uses including events and pop ups to create a vibrant, lively ever-changing place that people want to visit.

Delivering new public art work within key streets and spaces in the town centre.

**2. Establishing a foundation for future growth.**  
Providing the foundations to attract high-quality employment into the town centre, building on the catalytical impact of Glass Futures and the opportunity to harness innovation and skills for the benefit of the local community.

To create the infrastructure to support the emerging global employment in the borough whilst also creating places for people to collaborate and innovate.

**3. Promoting high-quality town centre living.**  
Delivering a unique, high-quality, residential offer within the town centre that meets housing need and aspiration. To diversify the borough’s housing stock across a mix of typologies and providing the opportunity for a new residential population to thrive as part of the wider placemaking ambitions.

**4. Creating a sustainable, accessible and connected town centre.**  
To create a more sustainably accessible town centre to make it easier for people to enjoy the services, spaces and amenities that the town centre has to offer.

To improve the transport and digital infrastructure laying the foundations for communities, businesses and visitors to digitally connect, and for businesses and investment to be attracted into the borough.

To ensure the town centre deals with the effects of climate change and maximises the opportunity for net zero carbon development, with sustainability and climate change at the forefront of its transformation.

**5. Positively changing perceptions of the town.**  
Positively changing the perceptions of the town and the borough through promoting our heritage, protecting and enhancing our historical assets, the canal and our cultural offer to provide a distinctive experience for visitors, residents and businesses.

**6. To provide a healthy and community-focused town centre.**  
Creating high-quality spaces and places for independent businesses and community functions to thrive, places to dwell and socialise within, providing opportunities for active travel and leisure, and creating a town centre which the people of St Helens are proud of.

Ensuring that connectivity and accessibility for sustainable modes of transport are accommodated within the town centre to increase health and wellbeing.

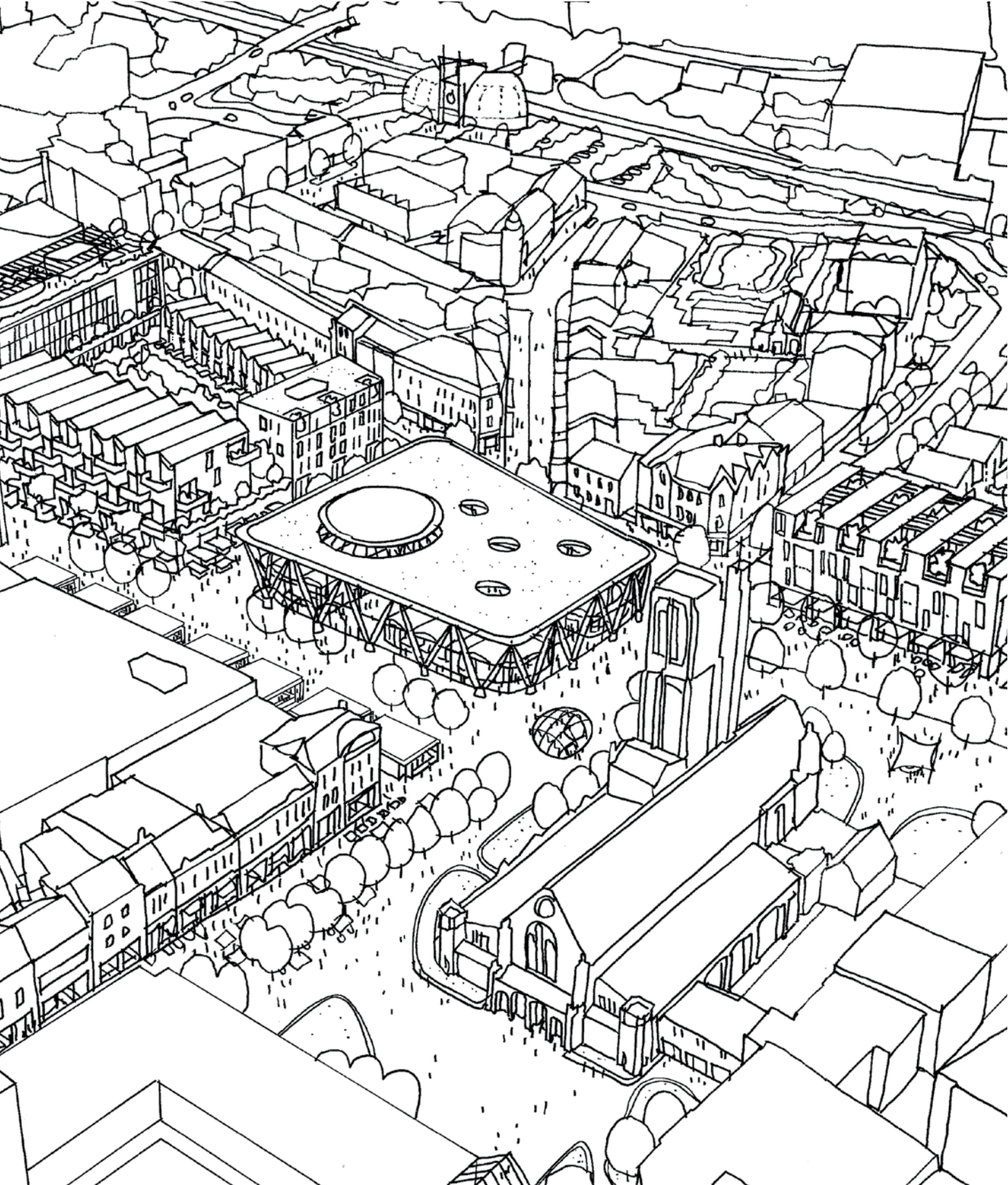
Greening and improving the public realm to contribute towards net zero carbon, biodiversity and sustainability goals, to the benefit of the existing community and future generations.

The Brief

This area presents a unique opportunity to revitalise and redevelop the town centre, encouraging growth through new businesses as well as future proofing existing companies through the provision of modern workspaces.

Our approach embodies the three themes of the vision to shrink and link, be culturally centred and create an animated experience.





Artists impression, aerial view looking north east across Church Square

### Spatial Context

The Masterplan Development Framework outlines the ambition for the whole town centre. The first phase will focus on the heart of the town centre and the immediate deliverable place-making opportunities in and around Church Square, which forms the basis of this hybrid planning application. As illustrated, there are important wider links to consider which go beyond the proposed red line or area of intervention; these will be dealt with in complementary masterplans which will come forward in the future.

The below provides a summary of the town centre wide spatial context, as detailed in the SHMBC endorsed St Helens Masterplan Development Framework. It is important to consider the proposed development in this context, in order to ensure that potential linkages and interactions are taken into account and that the proposed development is not progressed in isolation of the wider town centre.

To the south/ south east a key change area is the site of Glass Futures, and the broader opportunity to create a potential Foundation Industries Campus, bringing together innovation in glass and other industries to drive forward the green economic growth of the town and the borough. It is important to emphasise the key links between these opportunities to ensure employment-generating development around Glass Futures is not seen as separate to the activities and change taking place in the town centre; those new employees and businesses are attracted into the town centre for retail, leisure and amenity use during and after the working day.

Likewise, the existing link between the town centre, railway station and rugby stadium are important to enhance the visitor experience and offer a reason for extended visits and return complementary visits.

To the north, there are future redevelopment opportunities for the College Street area and the potential to consolidate public sector uses to make more of this key gateway into the town centre. The spatial link between the town centre, through College Street and up to the future housing-led development at Cowley Hill will be important to create, so this future residential community can easily access the offer within the town centre via walking and cycling opportunities.

To the west, drawing in the existing educational uses and offer provided by St Helens College is key, and making sure that there are spaces, places and a town centre offer that is attractive for students and young people. The areas in and around Westfield Street and Duke Street need consideration too – creating the opportunity for a more diverse and town centre wide evening economy.

### Design Principles

All development coming forward within the application boundary will adhere to adopted planning policy and consider the following key design principles.

- Create positive active frontages throughout the town centre, such as leisure, retail and residential at ground level.
- Open up the desire lines improving natural wayfinding and maximising existing landmarks and focal points.
- Create opportunities to provide experiences through a range of placemaking activities and events which animate the spaces and create interest.
- Improve and link public space, historic, cultural buildings, the canal and existing positive streets.
- Increase the footfall throughout the town centre.
- Create new and improved dwell spaces.
- Create a density that is appropriate to a town centre location.
- Draw upon the heritage analysis of the site and street patterns, seeking opportunities to restore the historic street grain pattern with a network of interconnecting shared surface streetscapes.
- Consolidate car parking supply to make car parks more efficient, reduce circulating vehicles in the town centre and promote sustainable travel.
- Adhere to latest guidance on 'Building Better, Building Beautiful'.

## 2.0 The Site



The aerial photograph to the right shows the area we have focused our efforts on within this document. The red line indicates the application site, the blue line indicates the Development Framework zone and the purple line the area of influence.

The focus of the town centre is Church Square dominated by St Helens Parish Church. Despite this the square feels hard and uninviting, a number of the large retail units are vacant having seen the likes of M&S move to out of town retail style parks. The square itself has the opportunity to be a positive dwell space on route through the town, a key focal point.

The centre is dominated by 3 large indoor shopping centres, Church Square, St Mary's and Hardshaw. Split over 3 sites they contribute to the poor way-finding and inactive frontages. The upper levels of Church Square and Hardshaw shopping centres are occupied by offices with car parks above, accessed to the latter by large ramps. There is a distinct lack of food and beverage outlets in the retail quarter.

To the back of St Mary's Shopping Centre is a purpose built market hall. This is under occupied and suffers from its location, its entrance on Chalon Way turning its back on the town centre. Chalon Way, is overwhelmed by service entrances and feels dedicated to vehicular movements rather than an inviting place for pedestrians. The inactive frontage contribute to the poorly used public realm around The World of Glass, making it feel like its located on the outer fringes of the town.

To the east round the St Helens College are more traditional retail streets with historic frontages, have character and intimacy. Given the adjacency of the College there is the potential for this to become a thriving area. The current offer of vertical drinking establishments doesn't cater for the modern day shopper.

The town hall, Victoria Square and the Transport Museum to the north of the centre are key assets. They are poorly linked which reinforces their location on the periphery of the town centre.

Beyond this key permeability routes extend beyond the site to the wider St Helens context, the buildings and places along these routes act as way markers helping you navigate within this wider context.



Key

Application Site

Masterplan Development Framework Area

Masterplan Area of Influence

Local Landmarks

Transport Hub

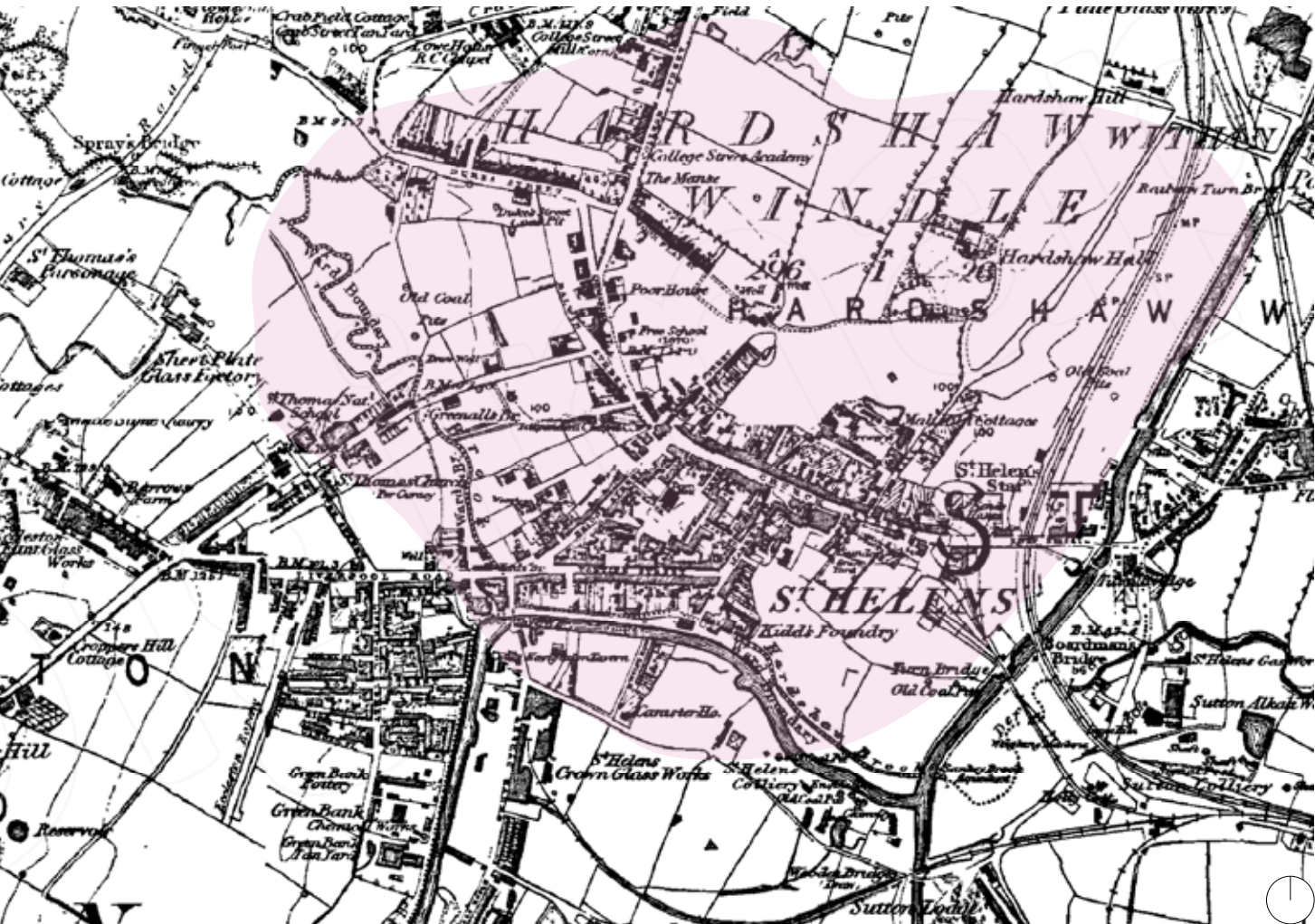
Travel Distance by foot



2.2 Historical Maps

1850

The current day street pattern along Church Street is visible as are the railway lines and canal. The Town Hall is shown in a different location.



1890

The historic street pattern is clearly visible and remains to the current day. The town hall has now become the market and the Town Hall has been constructed in its current location along with the library and train station. The growth of the industries around the canal and railways can be clearly seen, notably: glass, copper, lead and the gas works.





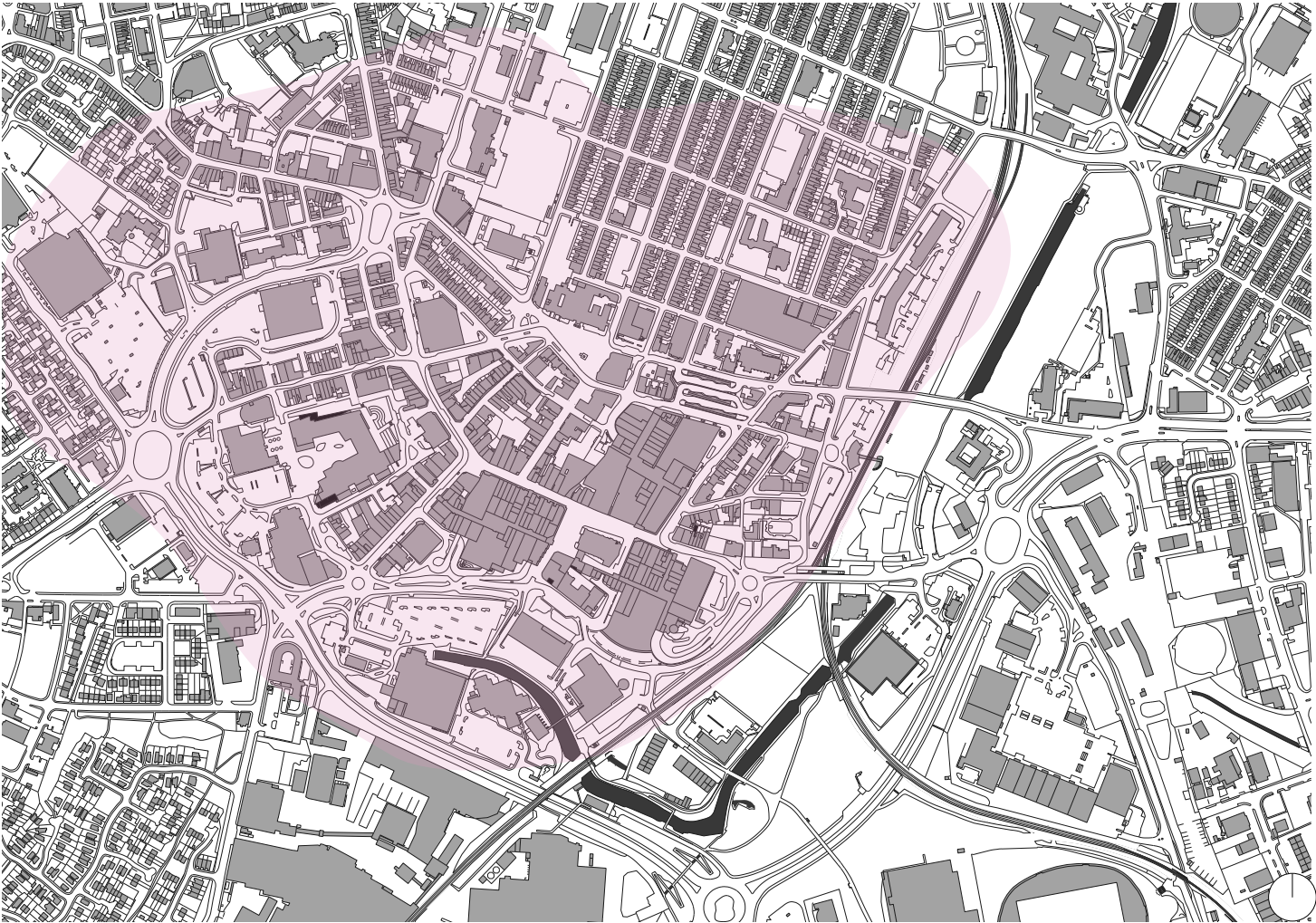
2.2 Historical Maps

1980

The block containing the original market hall and square has been demolished as has the block to the east of St Helens Church. The Church Square shopping centre built. The site to the north of St Helens Parish Church has been cleared ready to construct The Hardshaw Centre. By the 1980s significant lengths of the canal have been filled in.

Current

The most significant changes over the past 40 years is the removal of industry and associated rail lines.





Town hall  
Circa 1900

The Town Hall as it still exists today  
forming the northern boundary to Victoria  
Square.



Church Street  
Circa 1910

Church street one of the main transport  
routes, showing a thriving high street.





Aerial view of the industry  
centred round the railway circa  
1969



View showing the scale of the industrial  
warehouse buildings, chimneys and how  
these buildings formed large blocks within  
the town.

The Market Hall  
Circa 1969



The steel frame market hall with open  
sides to the street would have been a real  
focus and draw to the centre of St Helens.



2.4 Existing Site

The 3 main points of arrival into the town centre by bus, train or car do not feel like key gateways that present St Helens as a modern and vibrant town.

The bus station is located at the back of the Hardshaw Centre with poor and very limited public realm. There is no space to pause, orientate yourself or to wait for a friend. This contributes to the lack of intuitive way-finding that should lead pedestrians into the centre to explore its four quarters.

Similarly, by train the arrival space is immediately confronted by a roundabout, rather than a public space to dwell and meet. The roundabout cutting pedestrians off from the direct route down Bickerstaffe Street to the bus station and civic quarter.

By car, the likely arrival points are located at the surface or multi-storey car parks. Here the town centre presents inactive façades and backs of buildings. The town centre feels like it is turning its back on you rather than welcome you in.

South of Chalon Way feel outside of the town centre, a sea of surface parking and the now demolished Chalon Way multi-storey car park conceal the St Helens Canal. A potential gem which The World of Glass looks on to and bridges over.



1



2



3



4



5



6



7



8



9



2.4 Existing Site



10



11



12



13



14



15



16



17



18

Images

1. View up on arriving at the train station looking up Bickerstaffe Street towards the bus station and Civic Quarter.
2. View of the main bus station with St Helens Catholic Church beyond.
3. View on arrival by car looking up Bridge Street and Chalon Way West, dominated by the Tontine multi-storey car park.
4. View of the market hall and The World of Glass on Chalon Way.
5. View east across Church Square with the empty M&S to the left.
6. View of St Helens Parish Church the focus of the town centre.
7. View looking north towards Church Square from Chalon Way, the church tower just visible on the right hand side.
8. View south across Chalon Way surface car park, with no evidence of the canal in front of the left side of the Range shopping centre.
9. View north east from Victoria Square with the Town Hall to the left and Gamble Building to the right.
10. View south from Victoria Square of the building bounding the southern edge of Victoria Square. The public realm broken by the road.
11. View north west form the end of Church Street up Ormskirk Street.
12. View from the top of Bridge Street looking east down Church Street towards Church Square.
13. View down Bridge Street towards the A58 Linkway.
14. View towards the bus station up Hall Street showing the inactive street frontages on the left hand side of Hall Street.
15. View at the corner of Cotham and Ormskirk Street looking south east towards Church Square.
16. View across Square towards St Helens Collage and the listed Beecham Building.
17. View across the square of St Helens College.
18. View of the arcade with a well used coffee shop within the Hardshaw Shopping Centre.



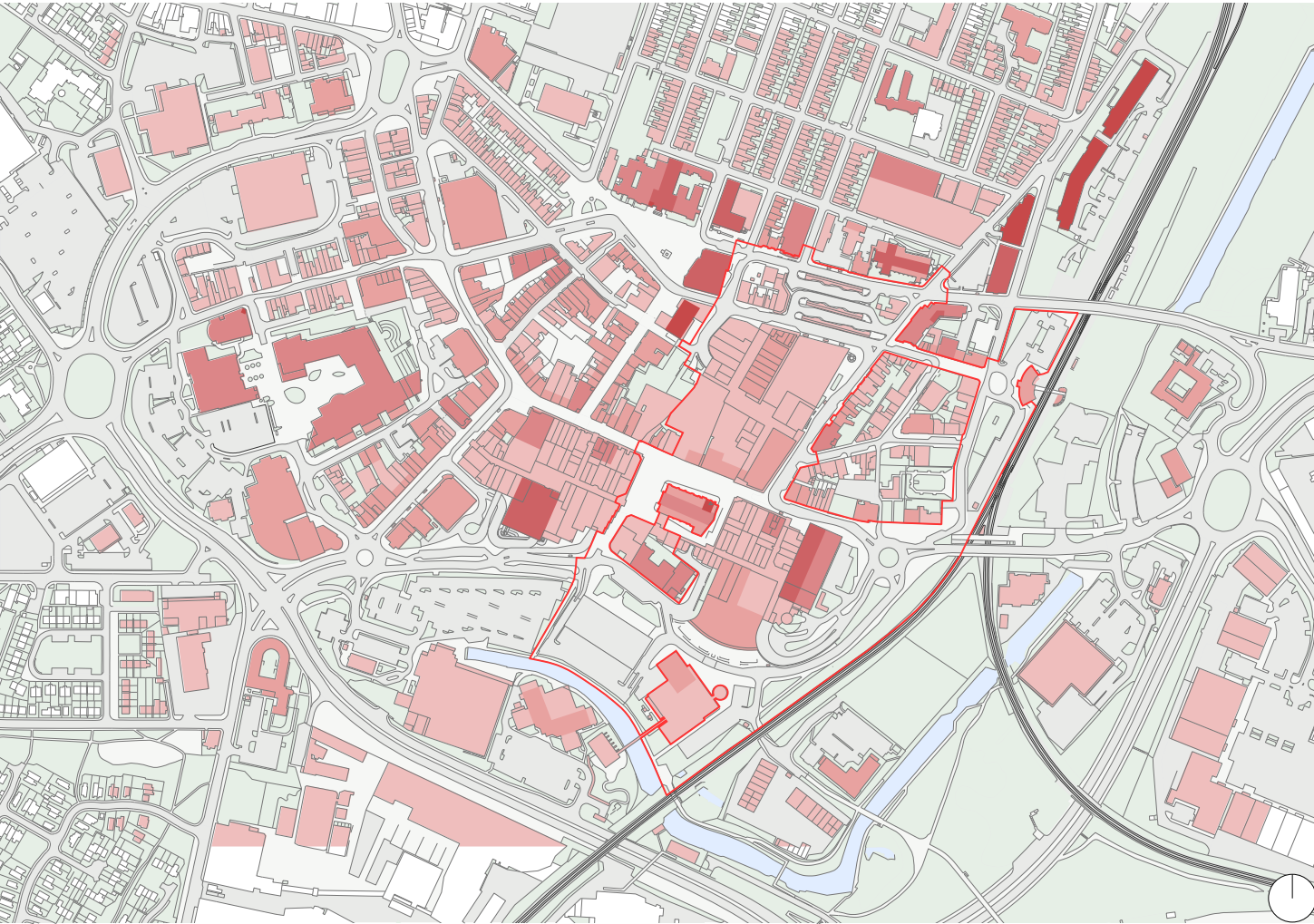
# 2.5 Site Analysis

## Environmental & Building Heights

### Building Heights

The town centre is relatively similar in scale from 2 to 4 storeys. The buildings that rise above this datum being the churches, town hall, multi-storey car parks and new housing. Century House stands out above all of them at 9 storeys high.

Historically the warehouse buildings and chimneys would have risen above this datum at around 6 storeys.

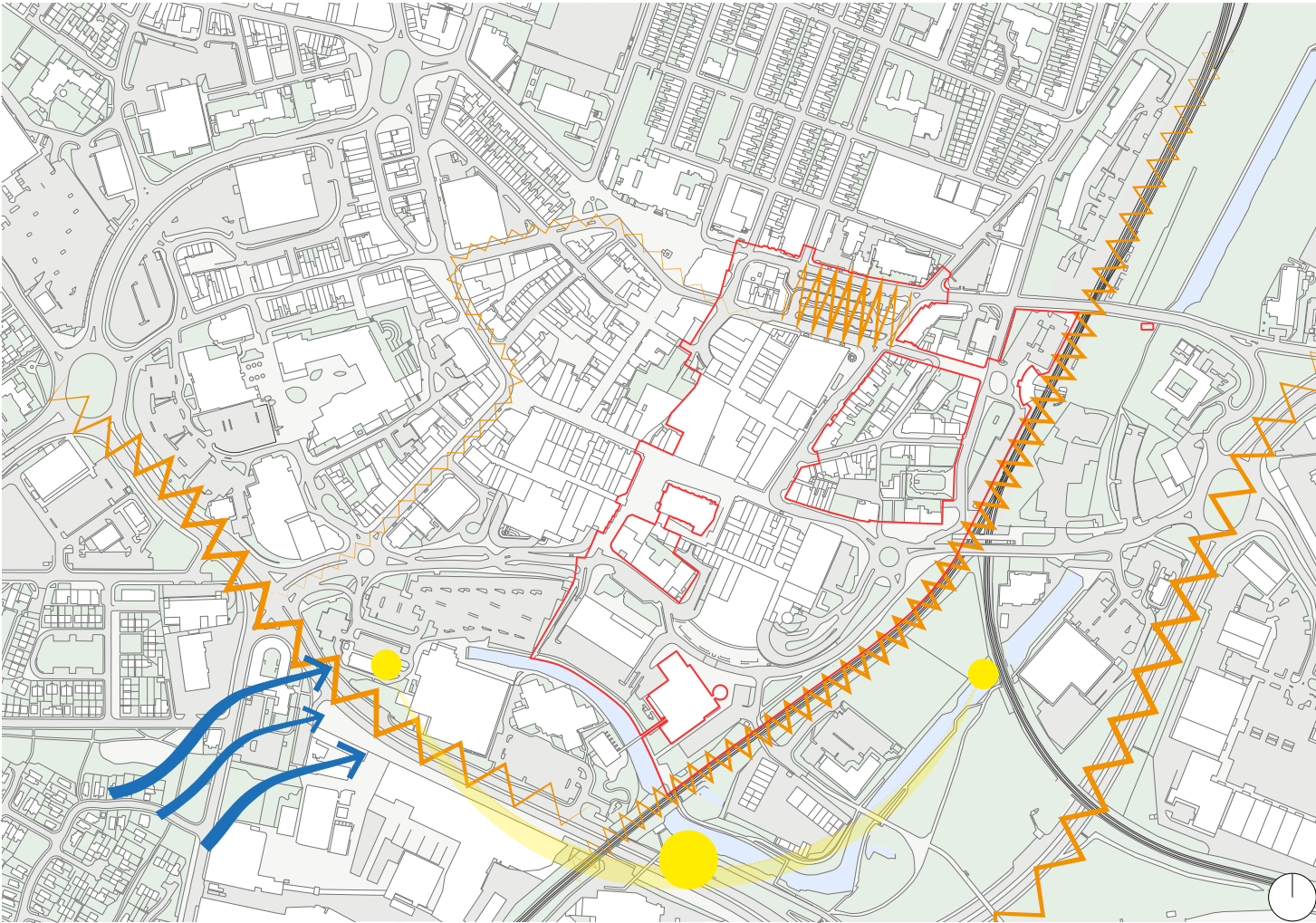
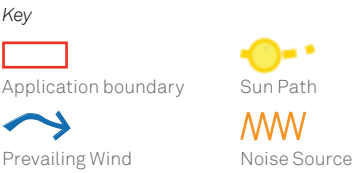


### Environmental

The town centre buildings are relatively low rise as such the centre benefits from high levels of natural light. It also feels sheltered from the prevailing wind.

The bus station and bus routes contribute to the noise levels within the centre. To the south the A58 is a source of noise being busy during the day and especially during rush hour.

The train line to the south and east also contribute to the noise levels.





2.5 Site Analysis  
Movement

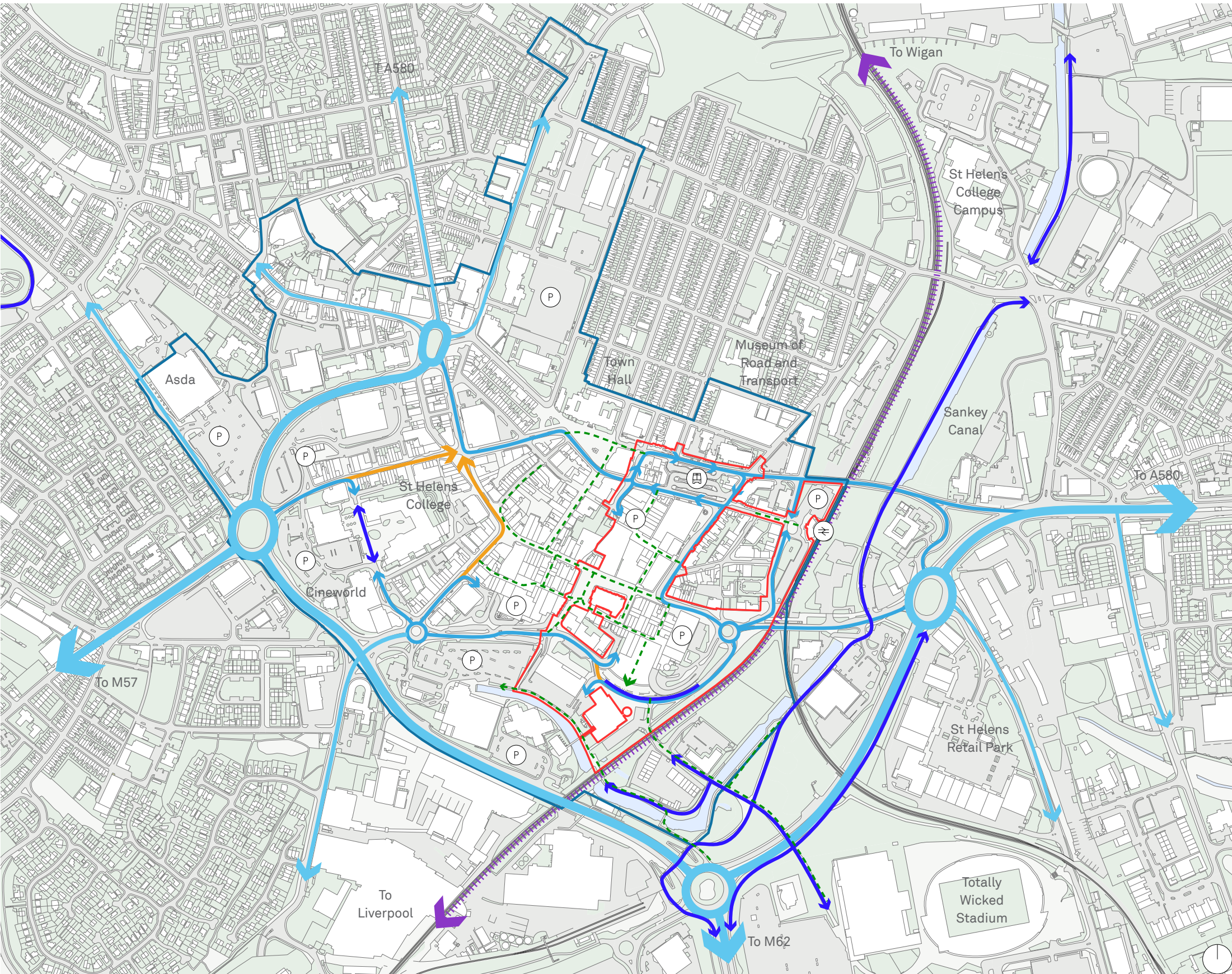
The town centre is well served by the main bus station to the north of Church Square and St Helens train station to the east.

The primary vehicle movement is along the A58 Linkway to the south of the centre. This was busy even outside of rush hour. The A58 quickly takes you to the main motorways serving the north west, the M62 and M57. The Linkway forms a barrier to the south of the town centre.

The main route to the bus station, A571, branches off from this and runs around the west and north of the town centre. This forms a second busy edge to the west of the centre.

Chalon Way runs to the north of the Linkway and to the south of the main shopping centre. It is not a through route for general traffic but services the market and gives access to surface parking. At the time of visiting on a weekday morning, this route felt underused and oversized.

The focus of the town centre is St Helens Parish Church (Grade II listed) and Square. The Church of the Holy Cross and St Helens Baptist Church (Grade II listed) also occupies a prominent location on the northern edge of the bus station. The World of Glass and its listed tank house which is also a listed monument lies to the south of Chalon Way, it feels disconnected from the town centre.





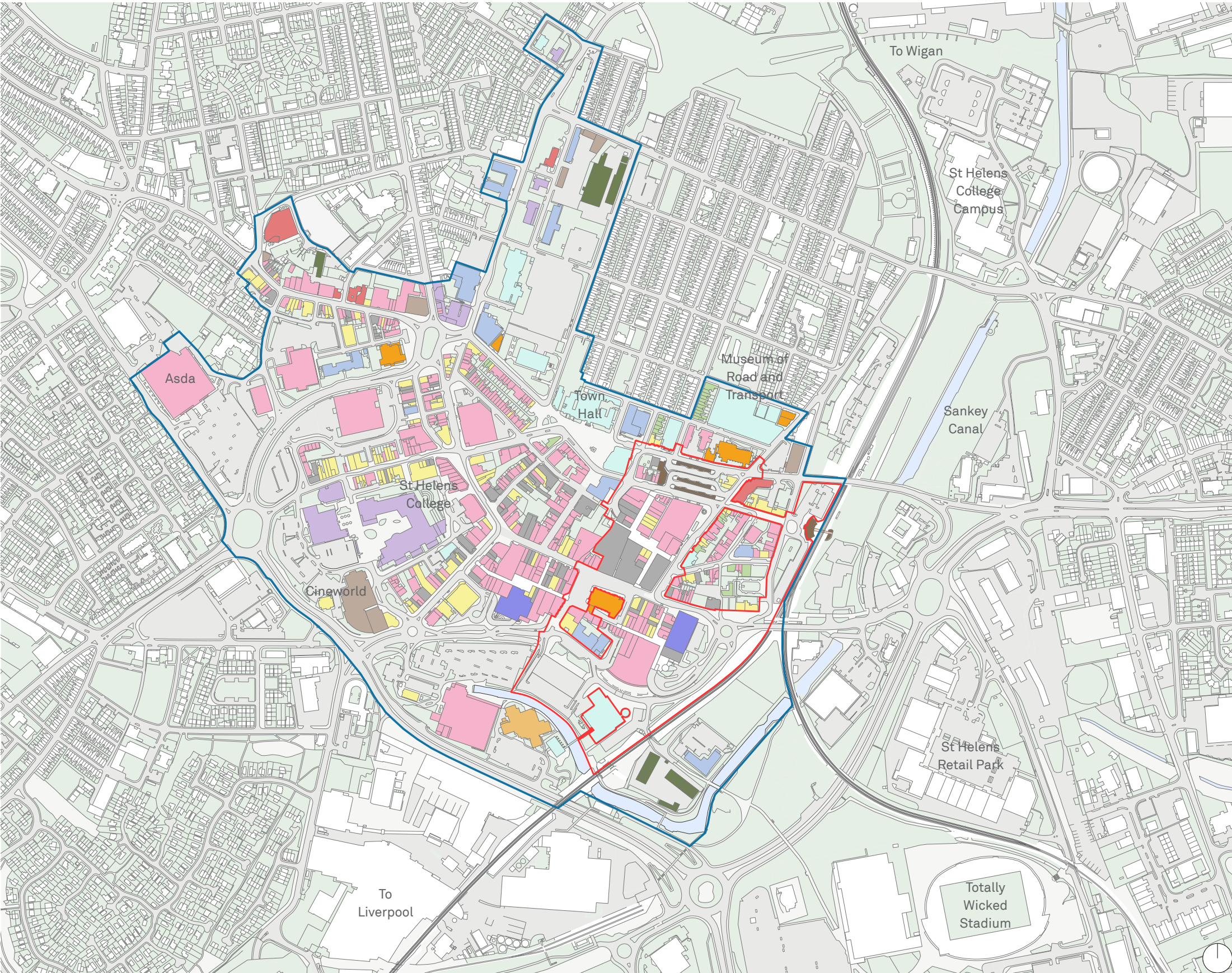
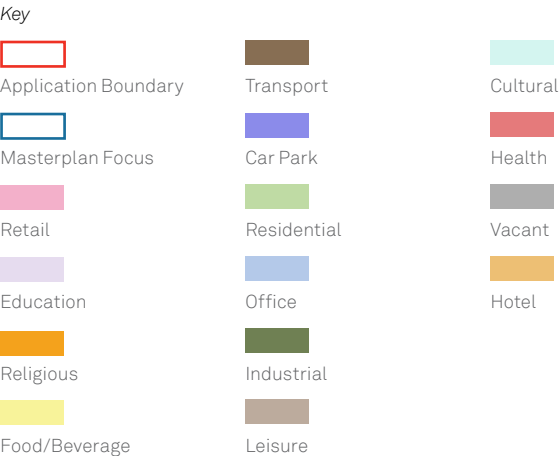
# 2.5 Site Analysis

## Building Uses

There are very few residential buildings within the town centre and area of focus boundary. Immediately outside of this to the west and north you are into residential streets. Similarly to the east, once over the Sankey Canal which is bounded by open space left over from the demolition of the industrial buildings, you are into residential streets.

The make up of the town centre correlates to the proposed quarters, Education and Entertainment to the east, Civic and Heritage to the north, Central Retail to the centre and a more open site to the south and east round the canal and rail line the Discovery Quarter.

The number of vacant units is clearly seen, since this analysis was undertaken St Mary's shopping centre is less than 50% occupied. This supports the councils vision to 'shrink and link'.





The constraints and opportunities analysis of the existing town is shown on the right and draws together all of the sites physical and environmental issues that define its character and the immediate context. Each constraint must be carefully considered, and opportunity capitalised upon for the masterplan design principles.

The constraints identified define the parameters that any future development must work within and where possible, seek to improve on. In doing so, the value of the site will be enhanced environmentally, socially and economically, leading to a truly sustainable mixed-use development.

The key constraints to the site are summarised below:

- Large town centre blocks occupied by the 3 shopping centres.
- Poor north/ south links.
- Significant amounts of inactive frontages and rear elevations dominate the town centre.
- Poor town edge elevations at key arrival points.
- Dominance and over provision of parking both surface and multi-storey.

Key

Application Boundary

Large Indoor Shopping Centre Plots

Surface and Multi-storey Parking

Noise Source

Inactive Elevations

Access Required

Poor Key Arrival Elevations

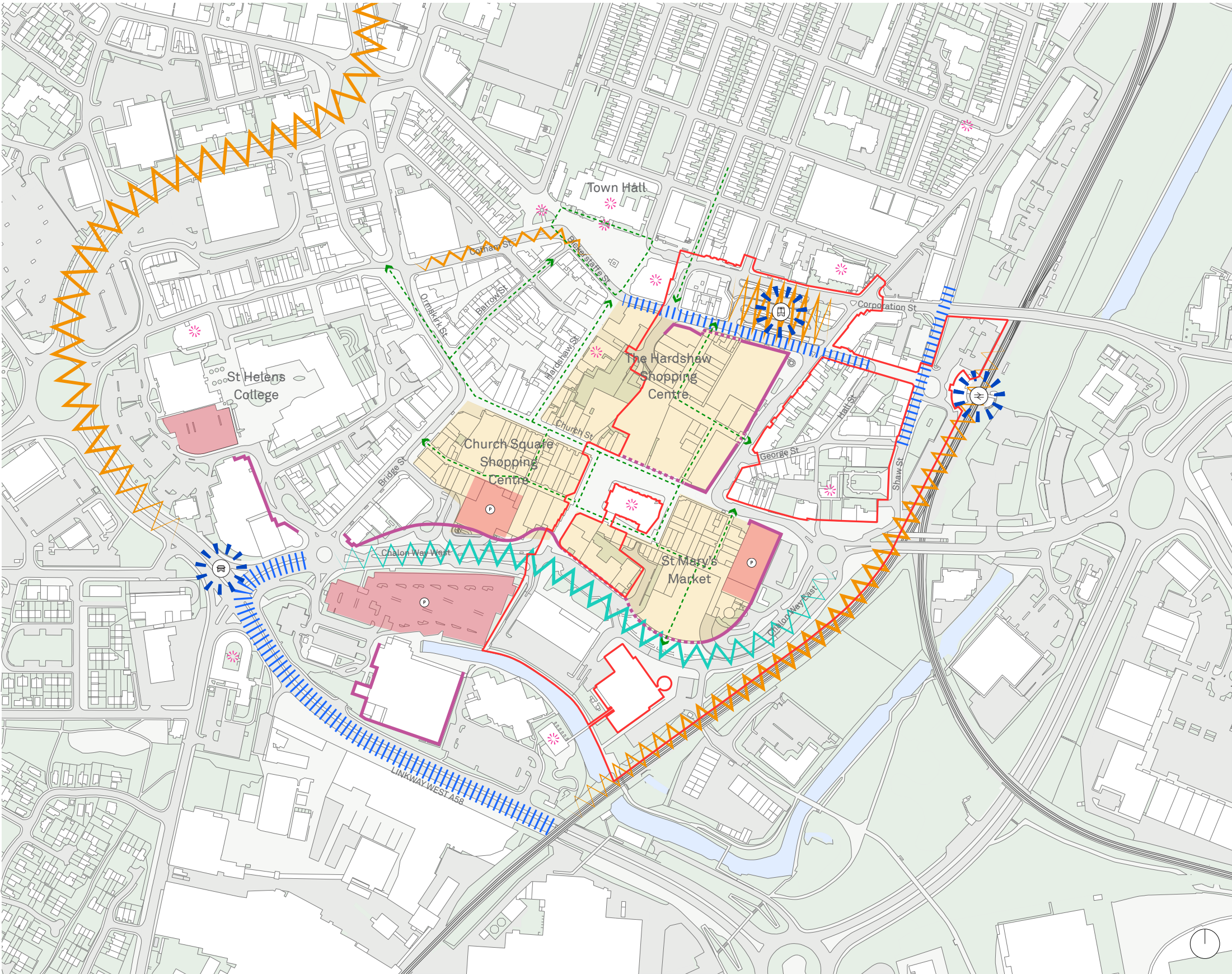
Wide over engineered highway.

\*

Building of merit/Listed Building/ Scheduled Monuments

\*

Transport Hub





# 2.5 Site Analysis Opportunities

A series of opportunities unique to the site must be realised for the delivery of any successful and contextual response.

The key opportunities are summarised below:

- Create active frontages throughout the town.
- Open up the desire lines improving natural way-finding. 'Animated Experience'.
- Improve and link public space, historic, cultural buildings, canal and existing positive streets.
- Increasing the footfall throughout the town centre - Transport hubs to destination buildings via retail.
- Create dwell spaces.
- Intensify the retail experience 'shrink and link'
- Develop the canal basin creating a new public space.
- Creating a density appropriate to a town centre location.
- The site is served by excellent transport connections.
- Draw upon the heritage analysis of the site and street patterns.
- The proposed development should learn from the existing negative environment.
- Consolidate car parking supply to make car parks more efficient, reduce circulating vehicles in the town centre and promote sustainable travel.
- Develop processional route to stadium for spectators on match days from the rail station and the town centre.
- Potential to soften the town centre through additional landscaping and creation of an area of open space.





1. Permeability

This section identifies the key moves which have informed the proposed development. These early design principles informed the Masterplan and have been used to shape the application proposals.

The key to any successful place is simple, intuitive permeability. A key move in our response is to restore the historic street grain. With a network of interconnecting shared surface streetscapes. The objective of ensuring pedestrians and cyclists can permeate through the town centre safely and with ease using the towns wayfinding points. Arrival points feed through the retail quarter increasing footfall providing places to dwell linking to all the 4 quarters and dispersing beyond.



Key

	
Application Boundary	Development Plots
	
Public Space	Existing Buildings
	
Permeability	






2. Giving everything back to the street

Positive active frontages are key to the success of any place. Streetscapes are activated through leisure, retail and residential use at ground level, providing an active 24/7 town centre.

This is further animated through the introduction of more leisure use and artisan food offerings, reinvigorating the market hall giving the new and existing St Helens residents a range of amenity and breathing life into the town centre.



Key

	
Application Boundary	Development Plots
	
Public Space	Existing Buildings
	
Active Frontages	

## 3.0 Planning Policy and Guidance



Planning Policy Context

The following section identifies the current adopted and emerging policies, which have been referred to in the design development of the proposed scheme.

Section 38(6) of the Planning and Compulsory Purchase Act (2004) and Section 70(2) of the Town & Country Planning Act (1990) require planning applications to be determined in accordance with the statutory development plan, unless material considerations indicate otherwise.

The statutory development plan for St Helens currently comprises:

- Joint Merseyside and Halton Waste Local Plan (adopted 2013); and
- Bold Forest Park Area Action Plan (adopted 2017).

The following are other relevant material considerations in decision making in St Helens:

- National Planning Policy Framework (NPPF) (2021);
- Planning Practice Guidance (PPG) (2014 and as amended);
- Supplementary Planning Documents (SPD) / Supplementary Planning Guidance (SPG); and
- St Helens Masterplan Development Framework (2022).

The following national and local design and access policies and guidance are relevant to the proposed development:

Statutory Development Plan

St Helens Local Plan 2021-2037

Policy LPA03: Development Principles

New development in St Helens Borough will be required to contribute to inclusive communities by addressing the requirements of an ageing population; children, young people, and families; people with special needs; and the specific identified needs of minority groups in the Borough. Developments will be required to:

- Contribute to a high quality built and natural environment by:
  - a. Securing high quality design in all development and a high standard of amenity;
  - b. Taking account of the Borough’s landscape character and townscape, and the distinctive roles and settings of different areas;
  - c. Protecting and enhancing the Borough’s natural, built, and historic environments;
  - d. Protecting and enhancing the Borough’s water, air, land, and biodiversity; and
  - e. Making effective use of land, buildings and existing infrastructure.
- Minimise the need to travel and maximise the use of sustainable transport through a range of measures; and
- Lower St Helens Borough’s carbon footprint and adapt to the effects of climate change.

Policy LPA09: Green Infrastructure

Development that would contribute to or provide opportunities to enhance the function of existing green infrastructure and its connectivity from residential areas, town, district and local centres, employment areas and other open spaces, will be encouraged.

Policy LPA11: Health and Wellbeing

The Council will work with its health and wellbeing partners to promote public health principles, maximise opportunities for people to lead healthy and active lifestyles, and reduce health inequalities for residents. Planning decisions will be used to:

- a. **Encourage access to an improved choice of homes;**
- b. **Ensure there is a provision of easy-to-maintain, safe and attractive public and green spaces to serve new development;**
- c. **encourage people to be physically active by providing opportunities for walking, cycling, outdoor recreation, and sport.**

Policy LPB01: St. Helens Town Centre and Central Spatial Area

- The Council will promote the Central Spatial Area as an accessible and high-quality built environment.
- Development proposals within the Primary and Secondary Frontages that would not result in an active ground floor use with a window display frontage will be refused.
- New development proposals where appropriate, will be required to facilitate linked trips between the Primary

- Shopping Area and other existing and proposed developments within the St. Helens Central Spatial Area.
- Proposals for housing or a mix of housing within or on the edge of the Town Centre will be supported where they would avoid prejudicing the retail and service role of the Town Centre.
- New development in the vicinity of St. Helens Canal will be required to improve the public realm by retaining and enhancing the existing waterway, integrating with the canal and securing improvements to Green Infrastructure.
- Pedestrian and vehicular accessibility within the Town Centre will be managed in line with the Liverpool City Region Transport Plan for Growth to:
  - a. maintain pedestrian priority within the Town Centre and extend pedestrian links;
  - b. make suitable provision for cyclists;
  - c. support the Town Centre as the hub of the public transport network; and
  - d. make appropriate provision for cars and service vehicles.

Policy LPC01: Housing Mix

New market and affordable housing must be well designed and include a range of types, tenures and sizes of homes as informed by relevant evidence in the latest Strategic Housing Market Assessment (SHMA). Exceptions may be made where the applicant has submitted an independent viability assessment, which clearly demonstrates that meeting the requirements would render the scheme unviable.

Material Considerations

National Planning Policy Framework (2021)

The NPPF sets out the Government’s planning policies for England and how these should be applied. It provides a framework within which locally-prepared plans for development can be produced. The NPPF is a material consideration in the determination of planning applications.

Section 2: Achieving sustainable development

Sustainable development is at the core of the Framework. Paragraph 8 of the NPPF establishes that there are three overarching objectives to sustainable development: economic, social and environmental. Within the social objective there is a requirement to foster well-designed, beautiful and safe places with accessible services and open spaces. Within the environmental objective the NPPF aims to protect and enhance the natural, built and historic environment.

Section 7: Ensuring the vitality of town centres

The NPPF supports the role that town centres play at the heart of local communities. Paragraph 86 recognises the importance of:

- promoting the long-term vitality and viability of town centre – by allowing them to grow and diversify in a way that can respond to rapid changes in the retail and leisure industries;
- recognising the role residential development often plays in ensuring the vitality of centres and encouraging residential development on appropriate sites.

Section 12: Achieving well-designed places

Paragraph 130 stipulates that planning policies and decisions should ensure that developments:

- a. will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development;
- b. are visually attractive as a result of good architecture, layout and appropriate and effective landscaping;

- c. are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d. establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit;
- e. optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks; and
- f. create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.



**LPD01: Ensuring Quality Development**

All proposals for development will be expected to meet or exceed the following requirements:

**Quality of the built environment:**

- a. Maintain or enhance the character and appearance of the local environment;
- b. Avoid causing unacceptable harm to the amenities of the local area;
- c. Ensure a high standard of amenity;
- d. Link in with surrounding movement patterns;
- e. Increase natural surveillance and to minimise opportunities for crime;
- f. Conserve, restore, enhance and/or minimise any adverse impacts on biodiversity;
- g. Provide landscaping as an integral part of the development;
- h. Encourage the inclusion of, or contribution to, public art in appropriate circumstances;
- i. Provide for the needs of special groups in the community; and
- j. Protect the setting, integrity and character of heritage assets.

**LPD02: Design and Layout of New Housing**

New residential developments will be required to:

1. be of a high-quality design;
2. enhance local distinctiveness;
3. provide appropriate landscaping;
4. provide a safe, secure, attractive, permeable, legible and useable environment for all users;
5. promote safe living environments that encourage natural surveillance and reduce the levels and fear of crime;
6. ensure heritage assets are treated in accordance with other policies to promote their conservation and enhancement;
7. consider the Borough's environmental assets in accordance with other policies;
8. provide a satisfactory level of privacy, outlook and natural lighting occupants and neighbours; and
9. be laid out and designed to ensure that the development is inclusive and accessible for all.

**LPD06: Prominent Gateway Corridors**

All proposals for new development that would be within or visible from one or more prominent gateway corridor(s) must, as appropriate, having regard to its scale and nature:

- a) be of high architectural quality, ensuring that the density, design, height and layout of any building(s) respond positively to the site and its setting; and
- b) provide appropriate landscaping as an integral part of their design and layout.



Artists impression looking north across Church Square and up Hall Street with plot 6, market, on the left



Design and Crime Supplementary Planning Document (SPD) (2011)

Access and Movement

The layout and permeability of development, the streets, buildings and spaces have an important effect on the levels of crime in an area. Layouts can provide clear, direct routes that are well overlooked, spaces that are interconnected, and places and buildings that link in with adjacent areas. It can also promote social interaction and a greater sense of place.

Activity and Natural Surveillance

New development proposals should, where appropriate, have a mix of uses that create levels of activity to help reduce the opportunity for crime and improve natural surveillance. A limited use or single user can lead to places feeling deserted and can result in crimes being unable to be detected.

Town Centres

Town centres which lack any activity in the evening after the shops have closed are often vulnerable to criminal and anti-social activity. The evening and late-night economy should try to provide for a mix of uses including restaurants, bars, leisure facilities, shops and galleries. Any external areas for gardens, terraces or smoking should be appropriately positioned, designed and segregated from the street.

Public realm and private spaces

All public spaces should be designed with clear, legible layouts that benefit from high levels of natural surveillance and incorporate boundary treatments, which do not obscure views in and out of the site. All routes and designated areas of activity, should be well secured with controlled access and appropriately laid out.

Car parking and servicing

New development should provide safe and convenient parking and servicing that is secure whilst also not having a negative impact on the street scene.

Design Guidance SPD (2007)

The SPD sets out a series of principles for good design and looks at ways in which general objectives can be applied to specific issues, these include:

1. A healthy, safe, attractive and rich environment, with a choice of good transport facilities for all.
2. Reduced crime and fear of crime.
3. Sustainable and stronger communities, narrowing inequalities with better opportunities for disadvantaged groups.

4. Securing urban regeneration.
5. Balancing the needs of new development and protection of the environment.
6. Improving the quality of the environment to make St. Helens a more attractive and safer place to live, work, play, invest and to visit.
7. Considering the needs of all sectors of the community in the provision of housing, employment, transport, recreation facilities and infrastructure.
8. Conserving resources and working towards the principles of sustainable development.

New Residential Development Supplementary Planning Document (2011)

The aim of the SPD is to set out the expectations of the Council in relation to new residential development, including:

- Providing clear and consistent guidance on new housing developments.
- Assisting in the determination of planning applications for new housing developments.
- Providing guidance on good design principles to allow high quality housing that is well designed and built to a high standard.
- Ensuring developments are sympathetic and appropriate to their context.
- Improving the quality of the built environment in the borough.
- Ensuring developments create an environment that it is safe for all users and in which people are encouraged to walk, cycle, use public transport and feel safe doing so.

Shop fronts Supplementary Planning Document (2010)

The aim of the SPD is to set out the expectations of the Council in relation to shop fronts. The key objectives are:

- To provide clear and consistent guidance on the design of shop fronts.
- To assist in the determination of planning applications for development.
- To encourage good practice in shop front design and redesign to ensure that it makes a positive contribution to the street scene.
- To ensure that shop fronts contribute positively to the daytime and evening economy.
- To ensure shop fronts are sympathetic and appropriate to the character of the Borough's conservation areas and listed buildings.
- To enable shop fronts to be accessible to all through inclusive design.
- To improve the quality of the built environment in the Borough.



Artists impression looking west down Bickerstaffe Street, Plot 4 on the right

St Helens Masterplan Development Framework (2022)

The Masterplan Development Framework (MDF) for St Helens town centre provides an aspirational vision with a focus on deliverable transformation, built on feedback from the community. The MDF identifies the opportunities and challenges which exist, and illustrates first thoughts and ideas around significant commercial and leisure investment that will completely transform the centre.

The investment will include new retail spaces, new homes, high quality offices, hotel accommodation, a new bus station, improved public realm and new green spaces.

The MDF sets out that the transformation of St Helens town centre will focus on its heritage, sporting and cultural assets, building on its uniqueness to do things differently and innovatively, including the global opportunity of Glass Futures and Foundation Industries. This focus will in turn help create vibrancy within the town centre for all to use, value and enjoy, making St Helens an attractive place in which to live, work, visit, and invest.

The key issues for the MDF to address are:

- Supporting existing retailers and traders, by consolidating and enhancing the retail offer and responding to existing challenges and future trends to ensure a vibrant town centre for the existing community and future generations to enjoy;
- Reducing the oversupply of shopping centres that over dominate the built form and creating smaller modern retail properties attractive to the independent sector and new entrepreneurs. This will build on the already strong and growing independent sector within the town.
- Introducing a mix of new uses and diversifying the traditional retail offer through a mix of complementary alternative uses including food and drink, commercial, residential, as well as range of amenities and services.
- In doing so, improving and enhancing the night-time economy and broadening the overall leisure, food & beverage opportunities within the town with a focus on families.
- Delivering a quality town centre living offer to diversify the housing stock and to provide an alternative for those wanting to experience the new amenities in the town centre. This will create a critical mass of new population who will provide footfall to support the night-time economy and invest in their local high street.
- The need to respond and be cognisant of the changing demographic of the borough, including the ageing population.

- The opportunity to utilise available development sites within the town centre and the potential of brownfield land to contribute towards growth targets including the delivery of new homes.
- Improving place-making and sense of place. Providing better amenity and experience will encourage people of all ages to visit and spend their leisure time in St Helens town centre.
- Ensuring that the town centre is accessible by all modes of transport. This must include sustainable modes of transport such as walking and cycling, and that appropriate infrastructure is in place to accommodate this.
- Positively changing the perceptions of the town centre, through high quality events and activities in the centre including temporary pop ups or ‘meanwhile’ uses to demonstrate change is happening.
- The opportunity to redevelop Council-owned assets to best effect. This includes making the most of the significant and extensive heritage and cultural assets that exist in the town centre.

The following strategic objectives will help to achieve the delivery of a culturally centred and vibrant town centre, which is at the heart of the vision for St Helens::

1. Delivering a diverse, vibrant and animated town centre
2. Establishing a foundation for future growth
3. Promoting high-quality town centre
4. Creating a sustainable, accessible and connected town centre
5. Positively changing perceptions of the town
6. To provide a health and community-focused town centre

The following key design principles are set out in the MDF:

- Create positive active frontages throughout the town centre, such as leisure, retail and residential at ground level.
- Open up the desire lines improving natural wayfinding and maximising existing landmarks and focal points.
- Create opportunities to provide experiences through a range of placemaking activities and events which animate the spaces and create interest.
- Improving and linking public space, historic, cultural buildings, the canal and existing positive streets.
- Increasing the footfall throughout the town centre.
- Creating new and improved dwell spaces.
- Creating a density that is appropriate to a town centre location.

- Draw upon the heritage analysis of the site and street patterns, seeking opportunities to restore the historic street grain pattern with a network of interconnecting shared surface streetscapes.
- Consolidate car parking supply to make car parks more efficient, reduce circulating vehicles in the town centre and promote sustainable travel.
- Adherence to latest guidance on ‘Building Better, Building Beautiful’.

The public realm strategy focuses on the following elements:

- Establish a strong hierarchy of streets and spaces that prioritise pedestrians and cyclists;
- Improve strategic connections between the key assets and destinations within the town centre and beyond;
- Provide a mix of public space opportunities that encourages people to engage with their environment and community;
- Create a unique and distinctive town centre including introducing public art within streets and spaces to create local landmarks and aid with legibility; and
- Support the development of key sites and encourage investment in the town centre.

The town centre area has been divided into four broad character zones, which have been delineated based upon key characteristics. Each zone has an important role to play in the wider transformation of St Helens town centre, and collectively, they will support the delivery of the overarching vision. The relevant zones to the application site have been summarised in this Design & Access Statement.

## 4.0 Design Development and Masterplan Response



Initial Scheme

The initial masterplan was based on the briefing information issued by St Helens Council and ECF without any prior to consultation with key stakeholders.

The masterplan response was to carve the town centre into a series of developable plots which respond to and mend the historic street grain. This is realised through creating new links between existing and new public space.

Expanding Church Square with a new market hall, food hall and cultural building along the northern edge. A new public square on the site of the old market, strengthening the link to the education quarter. Creating valuable town centre space for additional community and town square gatherings.

The proposal provides continuous active frontage to the heart and what were the fringes of the town centre, so it no longer turns its back on the wider town and arrival hubs.

Chalon Way is narrowed creating an intensity and identity. Bounded by a new business park and leisure facility with food and beverage outlets clustered round the extended canal basin. A new green lung at the heart of the city creates a backdrop for The World of Glass. The park creates an arrival elevation to the A58 announcing that the city is open for business.

New apartments and town houses are proposed. Hall Street becomes a series of flexible, incubator type workspaces, which respect the scale of the existing historic context complementing the offer in the George Street Conservation Area. Creating a thriving place to be for creative young professionals and families.

Design Development 1

The development round the St Helens Canal was paused and left as proposed, it was felt the delivery of this was a future phase for consideration at a later date. Similarly the proposal to demolish the Church Square Shopping Centre and replace it with a new open street with new civic square was not developed further as the current shopping centre is performing.

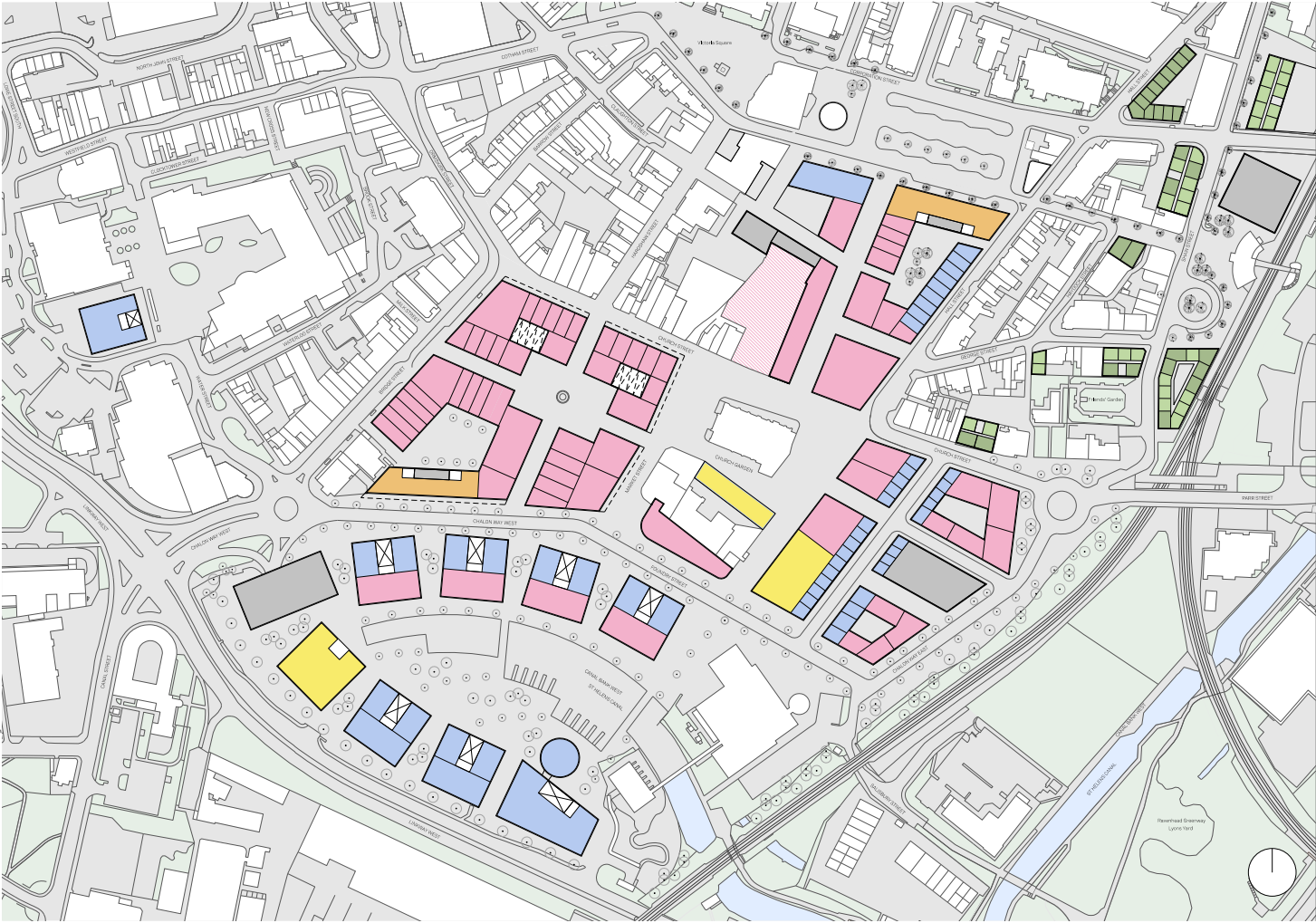
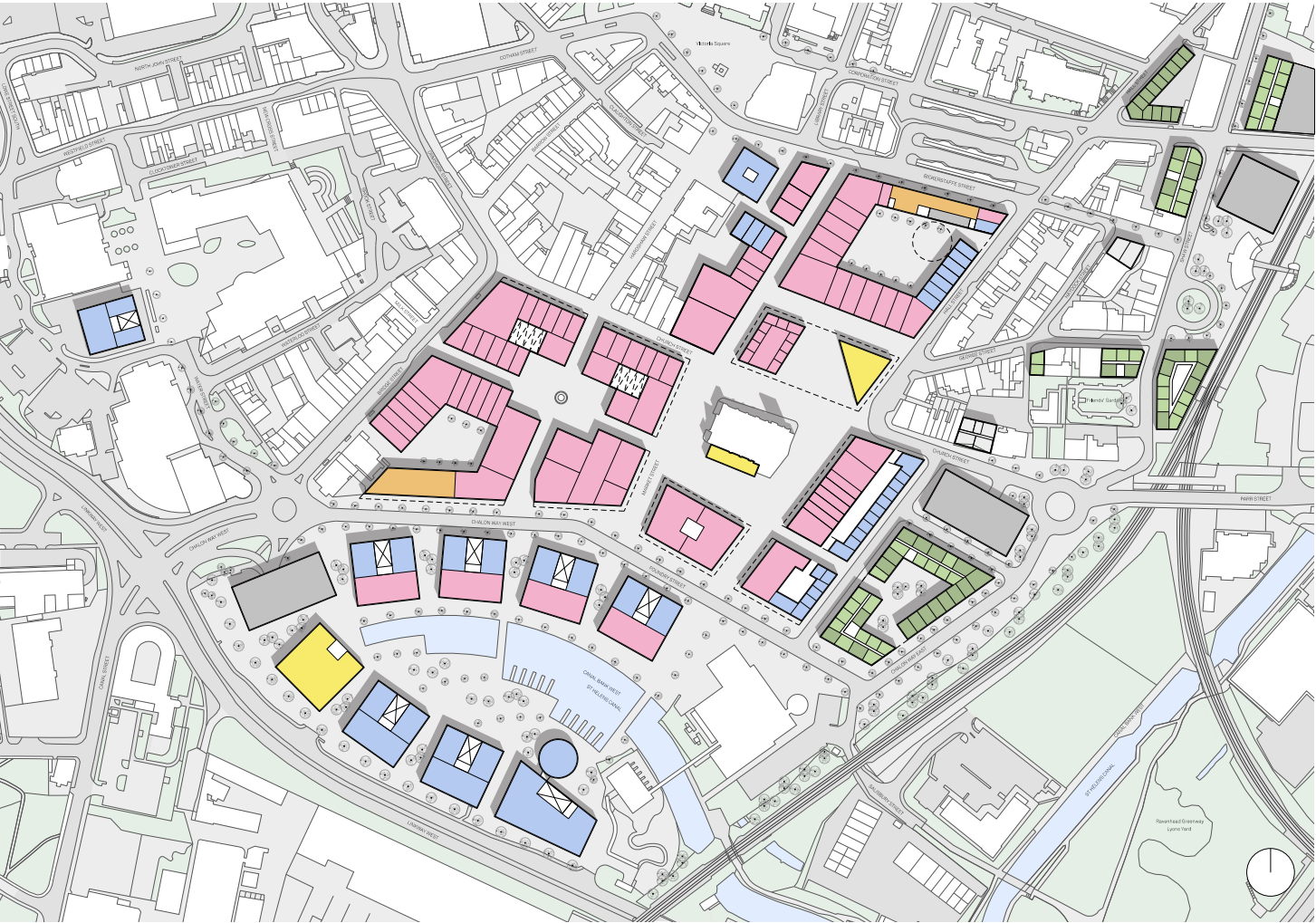
The council scheme to relocate St Mary's Market into the former M&S building was integrated into the scheme. Whilst this kept the market at the heart of the town it meant it didn't have a dedicated building that was recognisable as a market. This change allowed the new street linking the bus station with Church Square (New Market Street) to be put on axis with the Church Tower, creating a wayfinding route/ draw, pulling people into the centre.

The block to the south of Church Square containing Halifax Bank, Wetherspoons Pub and the BT building is retained as its not

within the council ownership. Its proposed to introduce some new blocks to screen the rear of this block to create a new backdrop to the Church.

Similarly Century House is retained as existing as its not within the council ownership and its retention does not negatively impact on the proposed plan.

Its proposed to demolish the terraced properties to the east of the Gamble to create a new public space in front of the Theatre Royal. It becomes Bickerstaffe Square which isn't dominated by a bus station.





Design Development 2

The key design development that transforms the masterplan is the de-engineering and re-routing of Chalon Way. This allows the introduction of a new urban park, Discovery Park. This not only creates a positive edge to the barrier of the rail line but provided a much needed green amenity space for the town centre and new setting for the historic Quaker Meeting House. The de-engineered Chalon Way still connects through to Church Street and Hall Street but for buses & taxis only.

Two new streets are created linking Church Square with the new park. The retail is reduced and the residential character of the George Street Quarter pulled south along the edge of the park.

Hall Street is extended to become Hall Street South on axis with the World of Glass bringing this key asset back into the centre of the town. This move also serves to tighten up Church Square from the previous iteration. The Discovery Park extends down to the St Helens Canal providing a new setting for the World of Glass.

A new extension to the rear of the Gamble Building is proposed, addressing the new Bickerstaffe Square. It forms its western edge with the Theatre Royal to the north and new office block to the south.

A separate study was undertaken to look at the proposed plot for the office to understand if it could deliver a net zero carbon office efficiently and within the height constraints of the masterplan.

Design Development 3

Phase 1 of the masterplan is honed down to the elements that have the maximum positive impact on the town centre and is deliverable within the agreed time frame. Its chosen to focus on the immediate areas to the north, east and south of Church Square with the red boundary line adjusted accordingly.

Church Square Shopping Centre to the west of Church Square remains, its well occupied and does not negatively impact on the key masterplan moves.

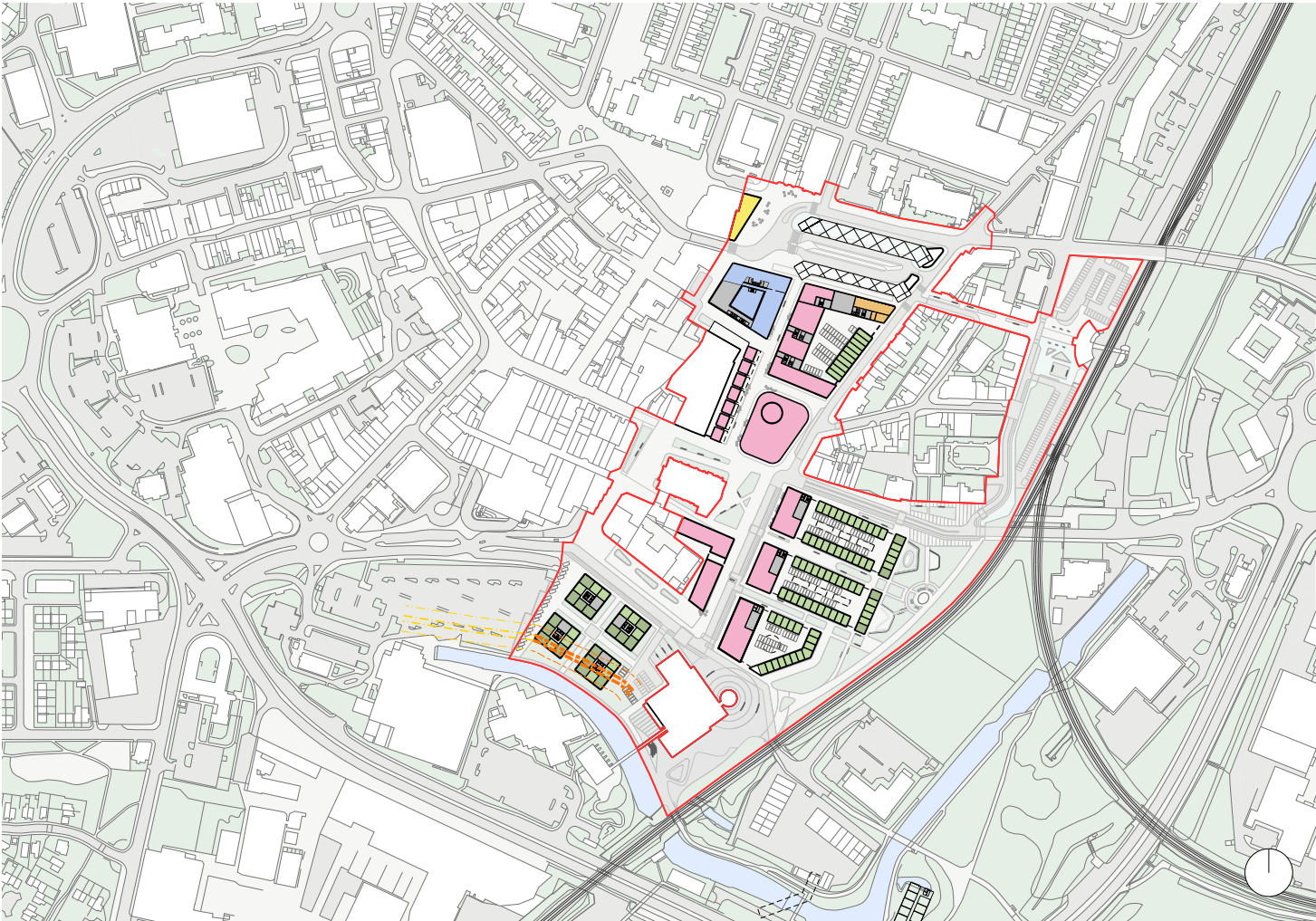
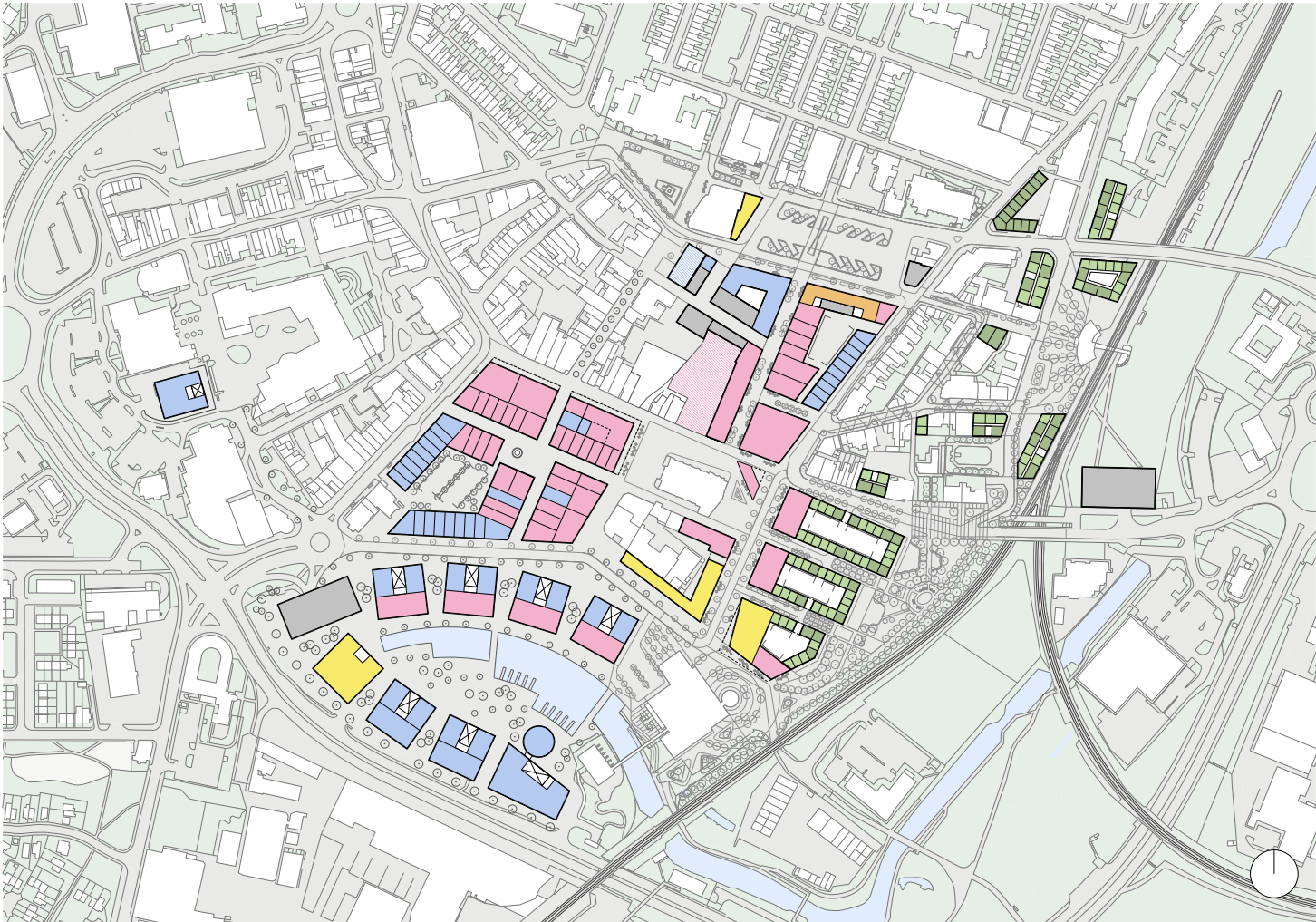
A dedicated market hall is reintroduced at the heart of the town in a dominant position on the edge of Church Square. A key gateway building that can celebrate the market put back at the hear of the town.

The council implements the demolition of Chalon Way multi-storey car park retaining its base as an event space. A residential/ office scheme is produced exploring the development of this site and linking up the town centre with St Helens Canal. Opening up the possibility of this site to be derived earlier in the programme.

The middle residential block to the west of Discovery Park is extended to consume and hide the existing substation.

The bus station design is developed with WSP to deliver an efficient bus station and meet masterplan moves to improve north south permeability. This requires the demolition of the Swan Hotel and chip shop on Hall Street.

The gap sites are removed from the proposal, it was always the view that these could be delivered by smaller independent developers, completing and intensifying the urban grain.






4.3 Illustrative Masterplan  
Roof Plan

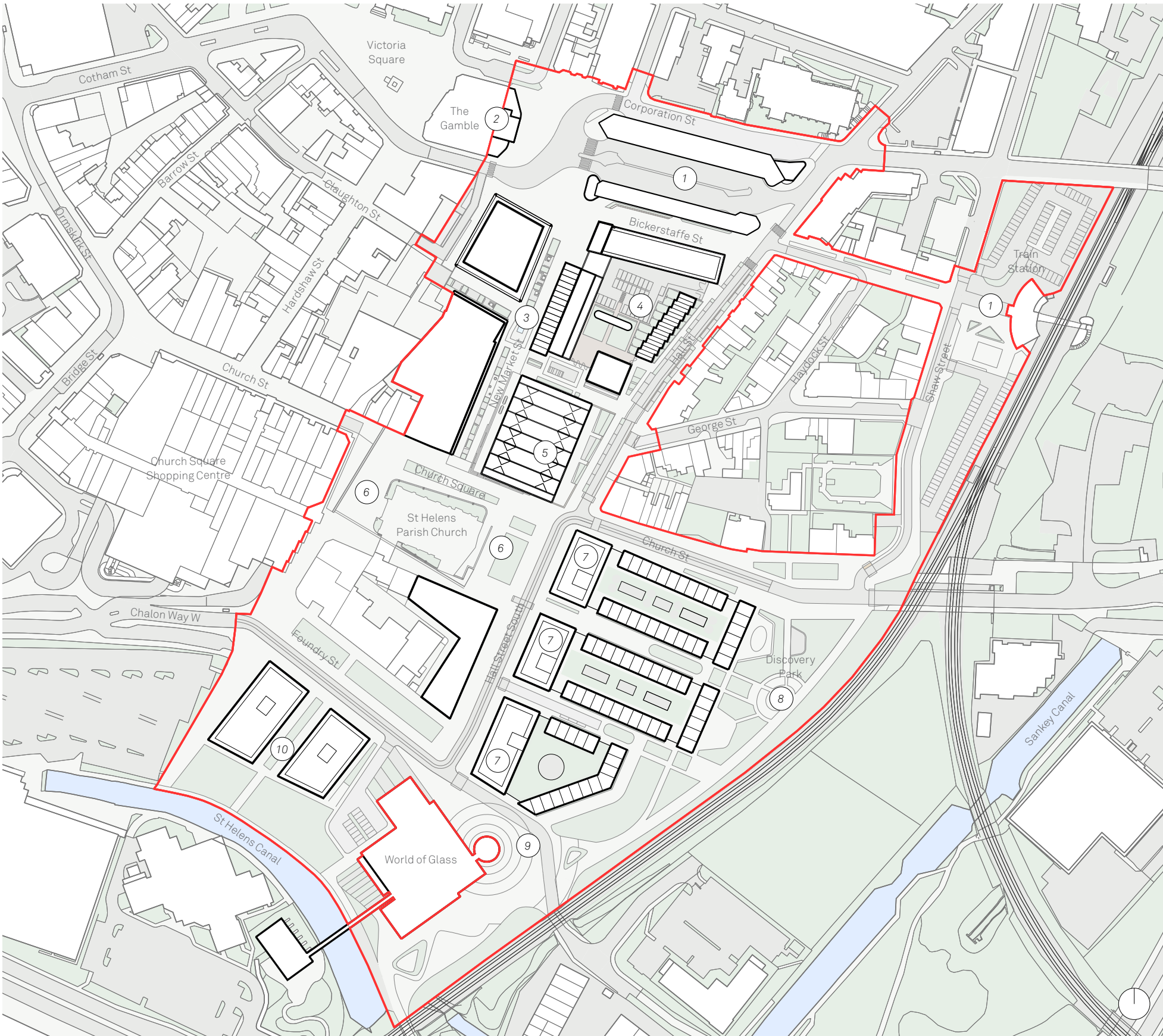
The town centre masterplan sets out a proposed spatial framework for the implementation of the town centre vision and strategic objectives. The masterplan has been developed around a number of zones or key character areas, opportunity sites, public realm and transport and movement considerations, as well as the consideration of specific urban design principles within each zone. The proposal respects the heritage of St Helens, indeed the masterplan seeks to repair post war damage to the town centre by respecting historic street patterns and re-activating streets and spaces. The regeneration of the town centre is based on 2 key masterplan moves.

Firstly, a rationalisation of north/south and east/west permeability. This creates a clear legible street pattern that links up the towns cultural and historic assets. The north south permeability will connect the civic and heritage area with new bus station through the central retail area and beyond to the St Helens Canal. A new arrival gateway for buses, trains and bikes comes together to form a transport hub. Creating a clear east/west link along Bickerstaffe Street through to Victoria Square.

Secondly the re-population of the town centre with high quality housing is seen as critical to establishing a sustainable community economically, socially and environmentally. New residential developments will offer a mix of tenure and size. Rationalisation of the oversupply and underutilised retail units will create space for modern exciting retail and leisure businesses. The new market will accommodate existing operators but also create a modern exciting food court. De-engineering Chalon Way into a new Discovery Park will resoundingly change the environment of the eastern edge of the town. The park will provide a new green amenity space within the town centre linking the historic Quaker Meeting House to The World of Glass Museum and the St Helens Canal. Finally creating the setting these great assets deserve.

1. Transport Hub welcoming you to St Helens and linking through to Victoria Square leading you into the town centre.
2. Extension to the Gamble Building addressing the newly named Bickerstaffe Square and bus station.
3. New Market Street on axis with the church tower, a new direct link with the transport hub and the town centre.
4. New mixed use development with a hotel overlooking the bus station, providing passive surveillance. Town houses to the east restore the residential scale and activity to Hall Street. Retail on New Market Street and overlooking the market hall with apartment above.
5. New Market with improved F&B offer places the market at the heart of the town centre with direct access from the transport hub.
6. Church Square the heart of town.
7. Retail/ F&B offer fronting Church Square with apartments above and town houses linking through to a new park.
8. Discovery Park overlooked by new town house development with children's play areas, pulling the residential character of George Street down and providing an amenity.
9. New setting for the World of Glass complemented by new cultural buildings bringing focus and intensity.
10. Residential/ commercial blocks fronting St Helens Canal and linking it back to the town centre.

Key  
  
Application Boundary





# 4.4 July 2022 Update

Since the submission of planning application P/2022/0212/HYBR, the following changes have been made to the proposed plans:

- On the Area Schedule, the changes which have been made are highlighted in green for ease;
- Plot 11 has been reduced in order to avoid a no-build zone associated with a culvert in this area;
- Changes to the shape/size of parameter plots 1, 2, 3, 4, 5 and 6;
- Upper Floor for Plot 5 has been removed as this is now proposed as a wrap around the existing, former M&S unit to allow access into the former M&S unit from the east, if this building were to be sub-divided in the future;
- Minimum height of Plot 5 has been increased from 33.5m to 37.1m;
- Gamble extension (Plot 1):
  - Minimum height reduced from 38m to 37m;
  - Maximum height amended with a taller middle section;
- Shaw Street has been downgraded from a Primary vehicle street to a Secondary vehicle street;
- Crossing point south of Gamble increased in size and new crossing added to the north east of plot 6; and
- Note re. minimum street widths added to Access and Movement Plan and Public Realm Plan.

These changes have been made to the parameters plans for the following reasons:

- Details designs are progressing for several blocks, which has resulted in minor amendments to the parameter boundaries, to ensure the buildings remain within the designated extents.
- Plot 3 has been re-orientated and a greater offset from Bickerstaffe Square created to enhance the pedestrian experience in this area.
- Plot 4 has been increased in depth, however this is to accommodate a larger internal parking and courtyard area for residents, rather than increasing the floorspace of this plot.
- Plot 5 has been reduced to a wrap around the former M&S unit to allow potential future subdivision of this unit and access from the east, whilst retaining a sufficient public realm area and streetscape along New Market Street.
- Plot 6 has been moved to the west to accommodate a servicing/delivery and drop-off area and landscaping, to allow a greater offset from the George Street Conservation Area.
- The Gamble extension (Plot 1) has been increased in height in the middle section, to allow lift access to be provided to all storeys of the Gamble from the extension, whilst still retaining appreciation of the eastern facade of this building.
- Plot 11 has been reduced in extent to accommodate an existing culvert and associated no-build zone.



Artists impression: aerial view looking north east



4.5 Character Zones  
Overview

The town centre area has been divided into four broad character zones, which have been designated based upon key characteristics. Each zone has an important role to play in the wider transformation of St Helens town centre, and collectively, they will support the delivery of the overarching vision.

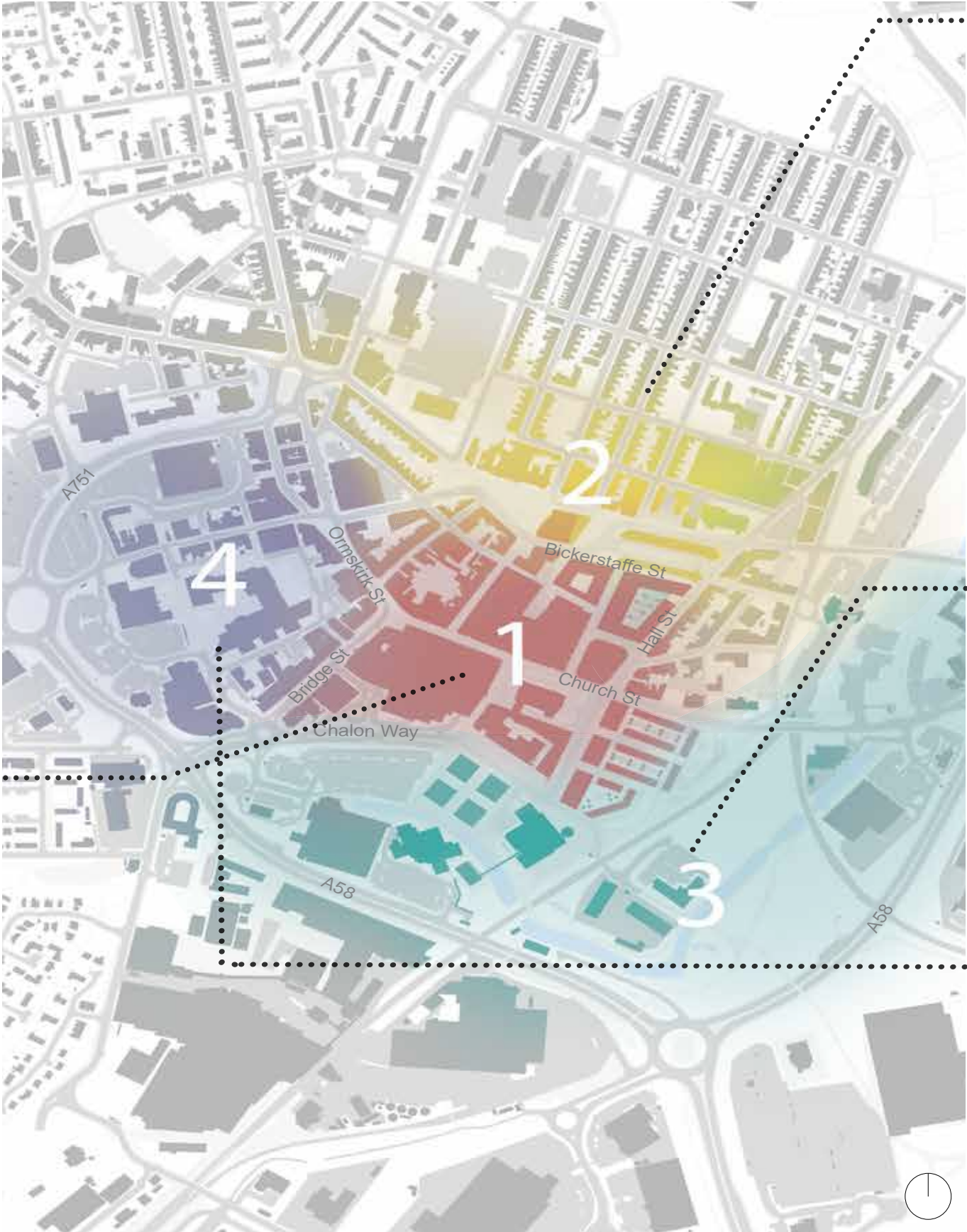
It is important to note that the zones do not represent phasing and delivery priorities. They have been identified based on the types of uses, scale and massing that will be considered in each area.

The zones are set out below, including a summary of the urban design principles and key components that have been considered in each area.

Character Zone 1: Central Retail.

Encompassing the heart of the town, occupied by traditional high street uses and centred around the Grade II listed St Helens Parish Church, which is a key asset for St Helens and the focus of Church Square. There are opportunities to improve the built environment, landscaping and reduce the dominance of the insular shopping centres which reduce the permeability within the town centre.

The Central Retail zone is bounded by the existing bus station and Cloughton Street to the north, St George's Quarter/conservation area to the east, Foundry Street/Chalon Way West to the south, and Bridge Street to the west.



Character Zone 2: Civic and Heritage

Comprising the area between St Helens Central (rail station), existing bus station around Bickerstaffe Street, George Street Quarter Conservation Area, Victoria Square Conservation Area, and the parcel of land between College Street and Birchley Street. This zone includes many of the town centre's heritage assets.

Development coming forward in this location needs to be sensitive to the unique character of the conservation areas – to preserve and/or enhance the heritage assets through bringing forward appropriate and sensitively-considered development in terms of scale, massing and use of materials.

There is a real opportunity in this zone to enhance the quality of the conservation areas.

Character Zone 3: Discovery

Presents an arc of opportunity for change and future transformation for the town centre. It includes the area in and around the Sankey (St Helens) Canal, important listed structures including the highly significant Tank House, which is both Grade II\* listed and a Scheduled Ancient Monument (SAM), being the best surviving example of a late 19th century glassmaking tank furnace building. These important buildings and structures represent the cultural heritage and glass legacy of the borough. The zone is bounded by the Linkway (A58) to the south and east.

Character Zone 4: Education and Entertainment

As its name suggests, this part of the town centre includes the St Helens College Campus and is bounded by Linkway West. This area is characterised by education uses, surface car parking, large, big box retailers, leisure uses and provides the focus for the evening economy from Bridge Street and Barrow Street to Westfield Street and extending to Duke Street.



Zone 1: Central Retail

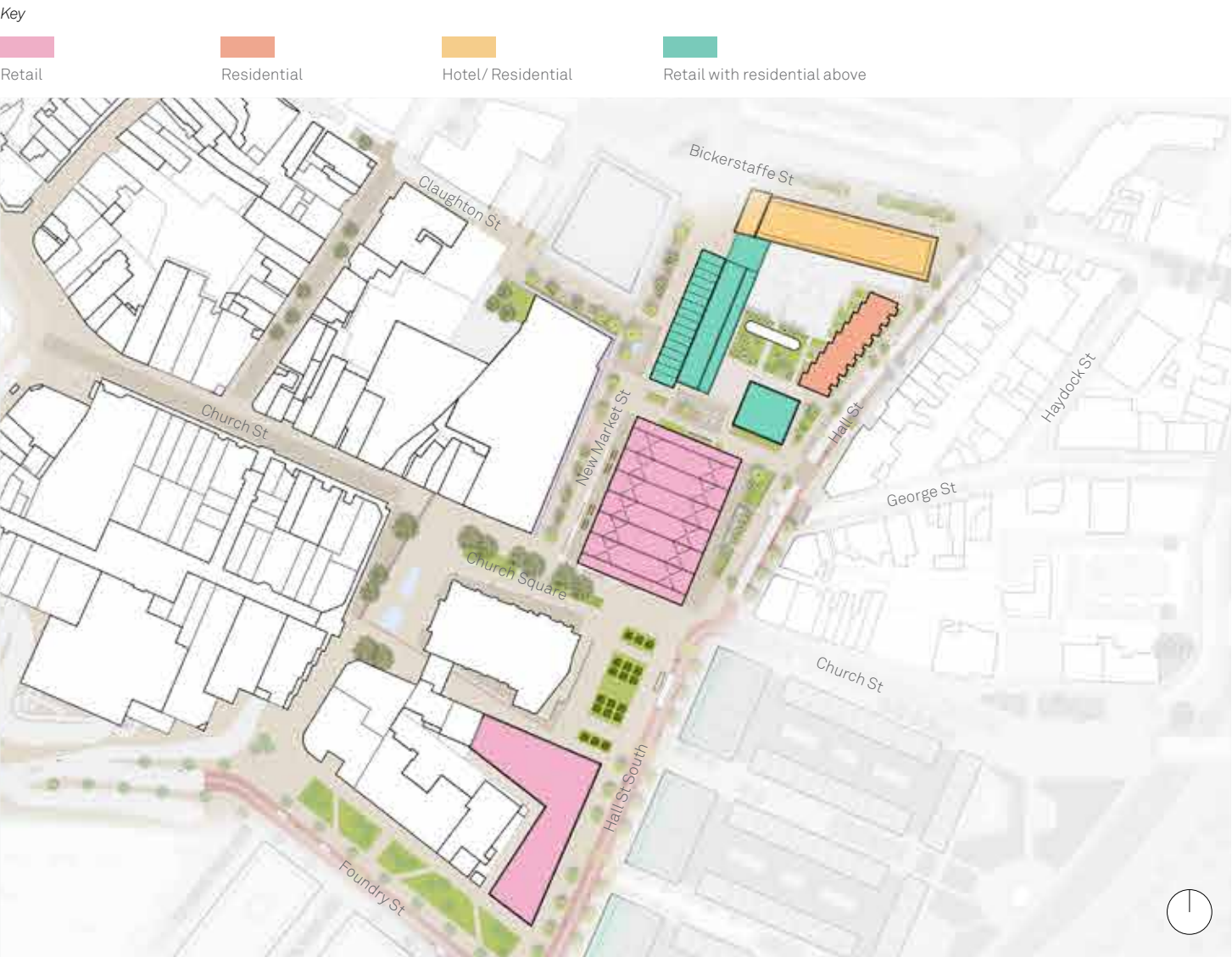
The Zone provides the opportunity to create a strong heart and focus to the town centre.

The key move within the Central Retail Zone proposes to increase permeability through the town centre, connecting Church Square back with the town and its key arrival points (bus and rail station) and public spaces. This reintroduction of the historic street pattern breaks down the monolithic scale and footprint of the Hardshaw Shopping Centre making it easier to move and find your way through the town centre.

Design Principals

- Re-introducing the original historic street pattern to encourage this permeability, breaking up monolithic buildings and introducing development of a more appropriate and 'human-scale' befitting of a town centre.

- Open up the desire lines improving natural way-finding i.e., simple & intuitive permeability. Ensuring pedestrians and cyclists can permeate through the town centre safely and with ease using the towns way-finding points by drawing upon the heritage analysis of the site and street patterns.
- Improve and link public space, historic, cultural buildings, canal and existing positive streets.
- Encouraging activity at ground floor level through active frontages, opportunity to make the most of the street and public realm, and creation of 'spill-out' space for occupiers and stallholders.
- High quality public realm and street-scape, with soft landscaping and greening.
- Positive active frontages. Street-scapes are activated through leisure, retail and residential use at ground level, providing an active 24/7 town centre.
- Creating a density appropriate to a town centre location.





Zone 2: Civic and Heritage

The Civic and Heritage Zone includes some of the town's key heritage buildings and assets, which are recognised given that the zone includes two conservation areas – George Street and Victoria Square (N.B. Victoria Square is adjacent to but not within the application site boundary). The conservation area status means that heritage is a key planning consideration, and in line with the NPPF, development coming forward in these locations must respect the conservation area, and where possible seek out opportunities for new development that enhance and make a positive contribution to heritage assets contained within the area.

Design Principles

- The creation of a joined-up transport hub, bus, rail, and cycle linking to destination buildings and public spaces with onward connections to retail.
- Arrival points feed through the retail zone increasing footfall providing places to dwell linking to all the 4 zones and dispersing beyond.

- Open up the desire lines improving natural way-finding i.e., simple & intuitive permeability. Ensuring pedestrians and cyclists can permeate through the town centre safely and with ease using the town's way-finding points by drawing upon the heritage analysis of the site and street patterns.
- Improve and link public space, historic, cultural buildings, canal and existing positive streets.
- Positive active frontages. Street scape's are activated through leisure, retail and residential use at ground level, providing an active 24/7 town centre.
- Create dwell spaces.
- Infill gap sites creating a density and quality appropriate to a town centre location.
- Respecting the character and appearance of the conservation areas – George Street and Victoria Square.



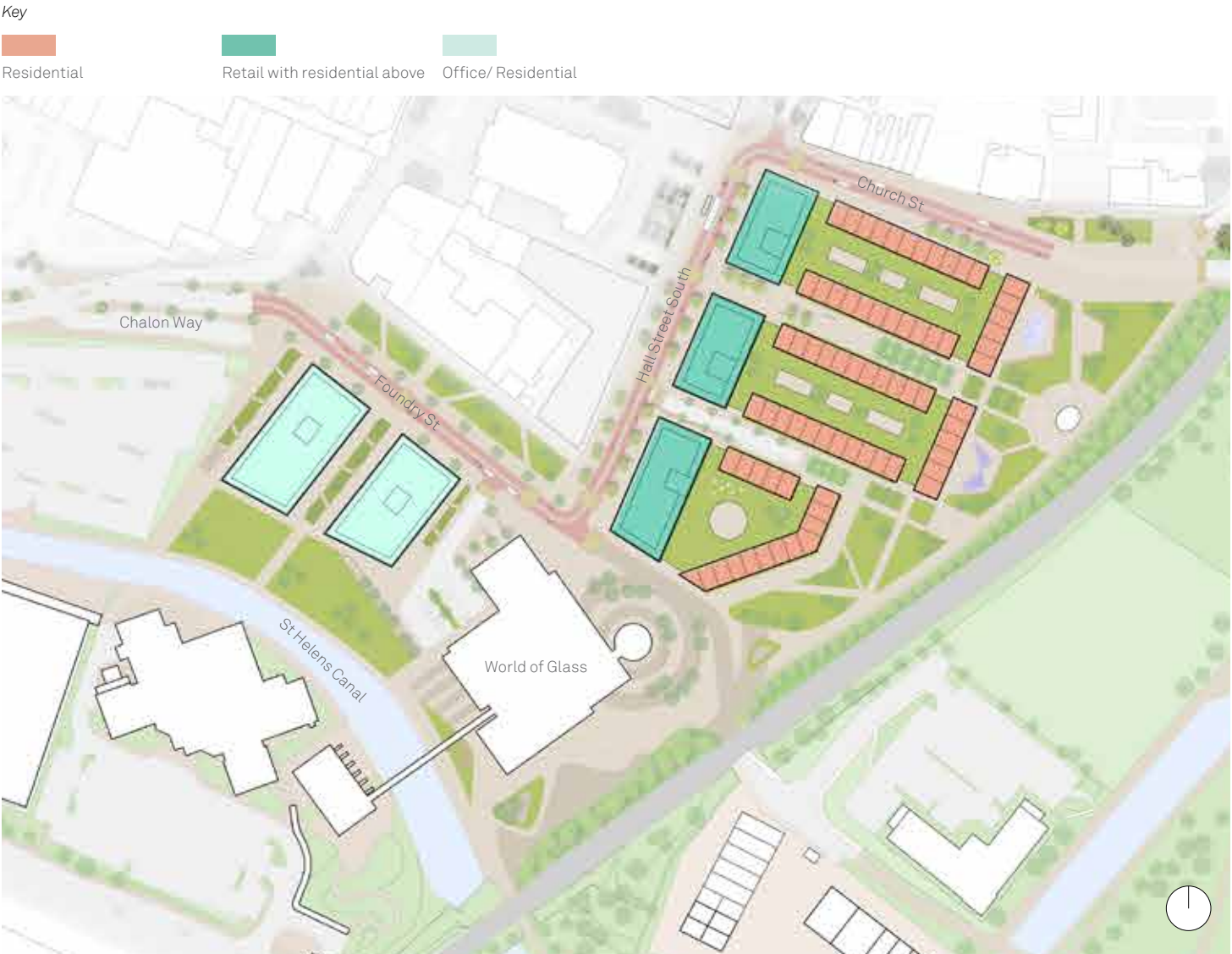
Zone 3: Discovery

The de-engineering and re-routing of Chalon Way opens up this side of the town and the canal basin, drawing it back into the town centre.

Design Principles

- The new Discovery Park provides a green amenity space and positive edge to the town and rail line. The park creates a new space for residents and workers of the town centre plus a new setting for The World of Glass and enhanced green public realm.
- Open up the desire lines improving natural way-finding i.e. simple & intuitive permeability. Ensuring pedestrians and cyclists can permeate through the town centre safely and with ease using the towns way-finding points by drawing upon the heritage analysis of the site and street patterns.
- Positive active frontages. Street are activated through leisure, retail and residential use at ground level, providing an active 24/7 town centre.

- Arrival points feed through the retail quarter increasing footfall providing places to dwell linking to all the zones and beyond.
- Creating a density appropriate to a town centre location whilst respecting our historic buildings.
- Consolidate car parking supply to make car parks more efficient, reduce circulating vehicles in the town centre and promote sustainable travel.



## 5.0 Masterplan Parameters



As this is a hybrid application with matters reserved, the plot parameters illustrate the maximum extent of the development. Plots should be developed to hold the street, with the exception of plot 11. This plot does not sit within the same defined street pattern. Its key relationships are with Foundry Street and St Helens Canal.

A number of the plots will contain more than one block/ building within them. These development plots enable development to front outwards, to positively animate the streets with active frontages.

The plots ensure that the masterplan is flexible to allow a number of different options to be developed at reserved matters stage, whilst protecting the core principles of the masterplan. The plot depth ensure, that the master planning structures are in place for sustainable building principals to come forward.

The development plots allow for different massing solutions to be delivered within the confines of the parameters, whilst retaining flexibility about the form. The following chapters set out these strategies in more detail.

Plots

- 1 Extension to Gamble Institute
- 2 Bus Station
- 3 Commercial Office
- 4 Mixed use development, retail, hotel and residential
- 5 M&S Building Wrap
- 6 Market Hall
- 7 Retail and residential
- 8 Retail and residential
- 9 Retail and residential
- 10 Retail and office
- 11 Residential or commercial office

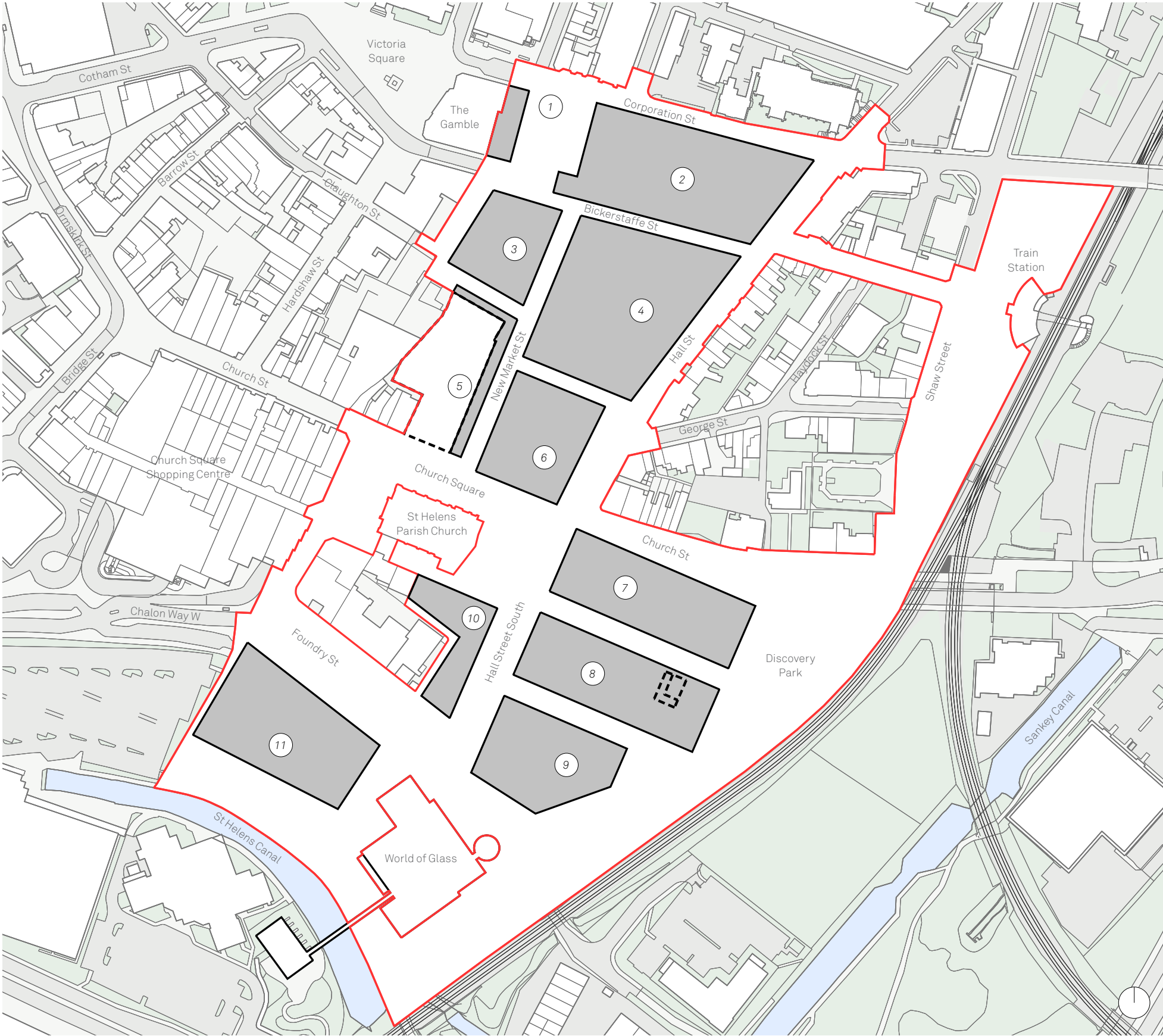
Note: See 5.2 for definitive list of uses

Key

Application Boundary

Development Plot

Buildings to be retained





# 5.2 Development Plot Uses

## Ground Floor Plan

The use within each block has been carefully determined to achieve the overall masterplan vision and to ensure flexibility in the delivery and future uses. To ensure the correct street character some of the blocks have been subdivided to lock this into the proposal.

At the heart of Bickerstaffe Square the new St Helens Bus Station (Plot 2) is bounded by the Gamble Extension (Plot 1), a new office development (Plot 3), a hotel with ground floor restaurant and foyer or residential use (Plot 4), the Theatre Royal, Holy Cross and Saint Helens Catholic Church and the Millennium Centre. The Gamble extension gives the building a new access and active frontage to the new Bickerstaffe Square, forming new public open space between the office, Theatre Royal and the bus station.

New Market Street, a new retail street, connects the bus station with Church Square, opening up the view of St Helens Parish Church tower, drawing people into the retail core past the new shops (Plots 4/5) and Market Hall (Plot 6).

The urban residential character of the George Street Quarter is reintroduced to Hall Street creating active frontages and activity at street level, as opposed to the blank façades of the current Hardshaw Shopping Centre. The character of the George Street Quarter pulled south along the edge of Discovery Park. The new residential streets with town houses (Plots 7/8/9) and front doors connect Church Square with the Discovery Park, a new amenity for the town and positive edge to the rail line.

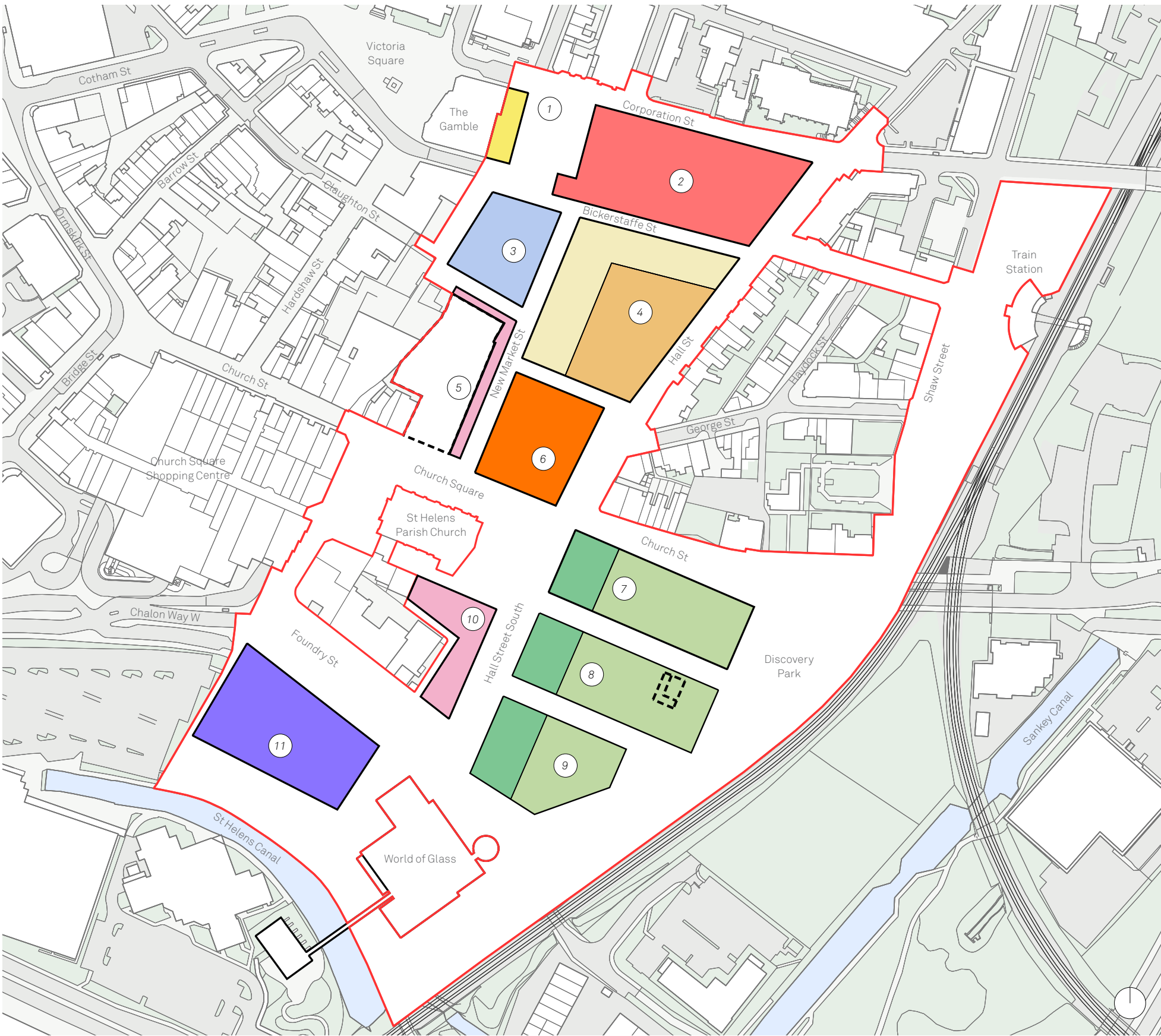
Hall Street is proposed to be extended, becoming Hall Street South, through Church Square down to The World of Glass putting it back in the heart of the town. It pulls people past the new retail and F&B offer fronting Church Square (Plots 7/8/10) and South Hall Street (Plots 8/9/10).

Linking the town centre with St Helens Canal new residential and or commercial blocks (Plot 11) will create streets with and F&B or retail offer at ground floor addressing the canal side.

### Key

- Application Boundary
- Use Class E(g)(i-ii), E (b) 200m<sup>2</sup> max
- Use Class C1, C3
- Use Class E (a-f), Sui Generis (Drinking establishments)
- Use Class C1
- Use Class E (a-f)
- Sui Generis (Drinking establishments, ancillary bus station facilities)
- Use Class E(a-f), F1(b-e), F2(b)
- Sui Generis (Drinking establishments ancillary bus station facilities)
- Use Class E(a-f), E(g)(i-ii)
- Use Class C3

- Use Class E(a-f)
- Sui Generis (Drinking establishments)
- Use Class C3
- Use Class E(a-f)
- Sui Generis (Drinking establishments)
- Use Class E(a-f)
- Sui Generis (Drinking establishments)
- Use Class E(a) & E(b)
- Sui Generis (Drinking establishments)
- Sui Generis - Bus Station
- Use Class E(a-f) 150m<sup>2</sup> max
- Plot Number



5.3 Development Plot Uses  
Upper Floor Plan

The use within each block at the upper level has been carefully determined to achieve the overall masterplan vision and to ensure flexibility in the masterplan delivery and future uses.

At the heart of Bickerstaffe Square the new St Helens Bus Station (Plot 2) is bounded by the Gamble Extension (Plot 1), a new office development (Plot 3), a hotel or residential (Plot 4), the Theatre Royal, Holy Cross and Saint Helens Catholic Church and the Millennium Centre. The hotel or residential at the upper level brings an active frontage to Bickerstaffe Square and provides passive surveillance for the bus station.

Offices (Plots 3) occupy the upper levels of New Market Street on its western and more civic edge. Its eastern side behind the hotel, residential apartments and town houses (Plot 4) occupy the upper levels drawing the residential character of the George Street Quarter over and into the town centre at an appropriate scale. The apartments bring life and passive surveillance to the streets with their front doors breaking up the retail below and further activating the streets.

The residential character of the George Street Quarter is pulled south, new streets creating links with Church Square and South Hall Street bringing the new Discovery Park into the town centre. Apartments overlook Church Square and South Hall Street at the upper levels giving scale to the square and holding its edge. Moving back towards the park town houses and duplex apartments create a more intimate scale that activates the streets with front doors and passive surveillance for Discovery Park.

Linking the town centre with St Helens Canal residential or commercial blocks (Plot 11) will create streets addressing the canal side and new Discovery Drive.

Key

Application Boundary

Use Class E(g)(i-ii)

Use Class C3

Use Class E(a-f), F1(b-e), F2(b)  
Sui Generis (Drinking establishments)

Use Class E(g)(i-ii)  
Use Class C3

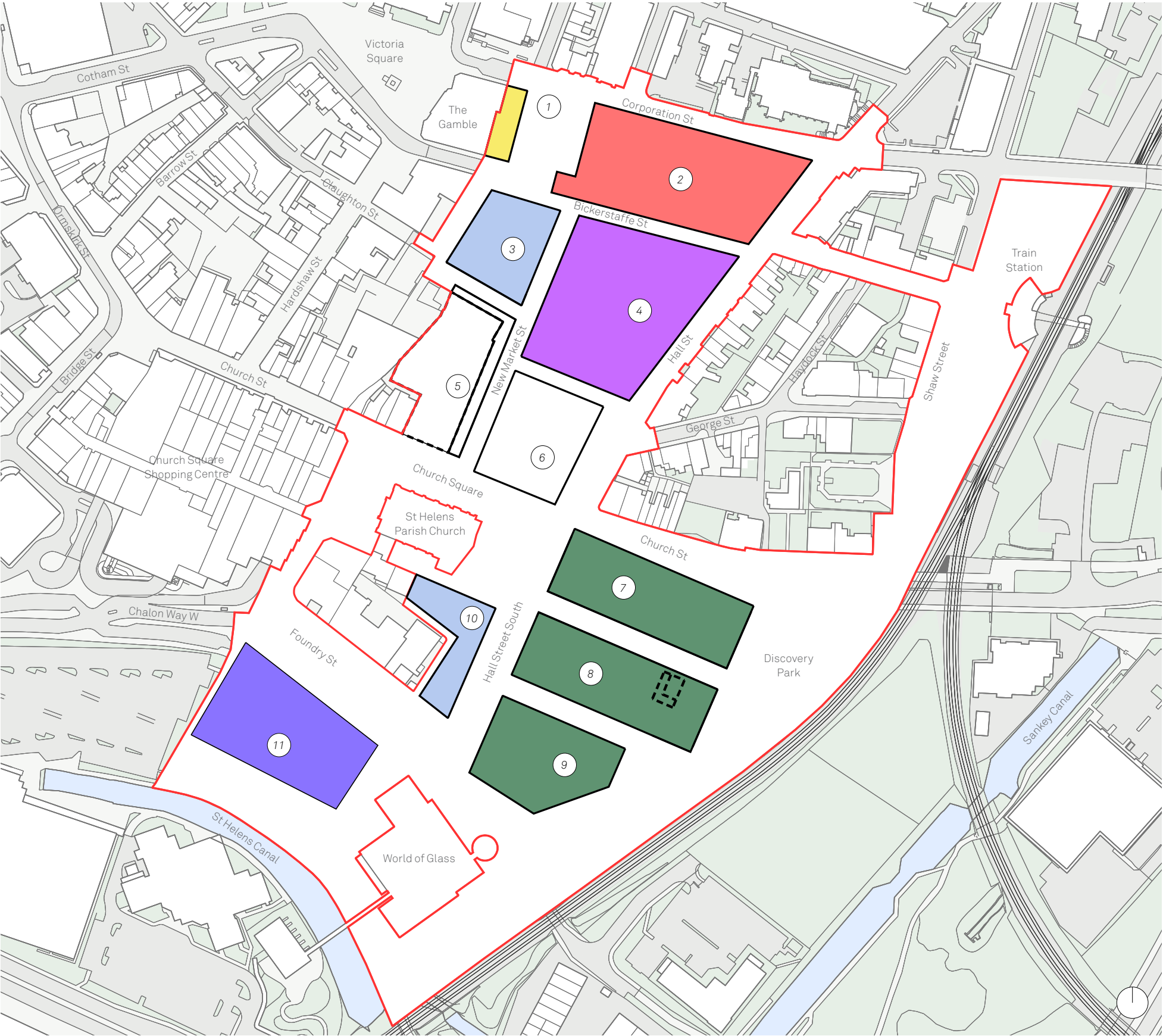
Use Class C1, C3

Sui Generis - Bus Station

Buildings to be retained

1

Plot Number





# 5.4 Development Plot Heights

## Minimum Development Heights

This is a outline application with matters reserved. The plot parameters illustrate the minimum height of each of the development plots. The massing across the masterplan has been carefully determined to work with and respect the historic grain of the town centre.

The minimum heights proposed ensure that only buildings of an appropriate scale can be brought forwards. The same philosophy as the maximum heights applies to the minimum, the height articulation of the blocks being the same just at a smaller scale.

The buildings in the Civic and Heritage Quarter such as the Town Hall, Gamble Institute, Century House, Wesley House and Theatre Royal are prominent buildings standing taller than most. The masterplan at its northern edge rises up to meet them at an appropriate civic scale, holding the edge of the new Bickerstaffe Square and overlooking the bus station.

This scale is reduced as it merges with the George Street Quarter, former M&S building and new residential blocks (Plots 7, 8 & 9) along the edge of the Discovery Park. Increasing the scale of these 3 blocks holds the edge of Church Square and South Hall Street and give it an appropriate scale alongside St Helens Parish Church and the Market Hall (plot 6).

The slight increase in height of these central blocks creates a central corridor leading from the transport hub to the north, through the town centre towards the St Helens Canal edge bringing this key asset back into the town centre alongside the World of Glass.

Key

Application Boundary

Ground Floor Only

G+1

G+2

G+3

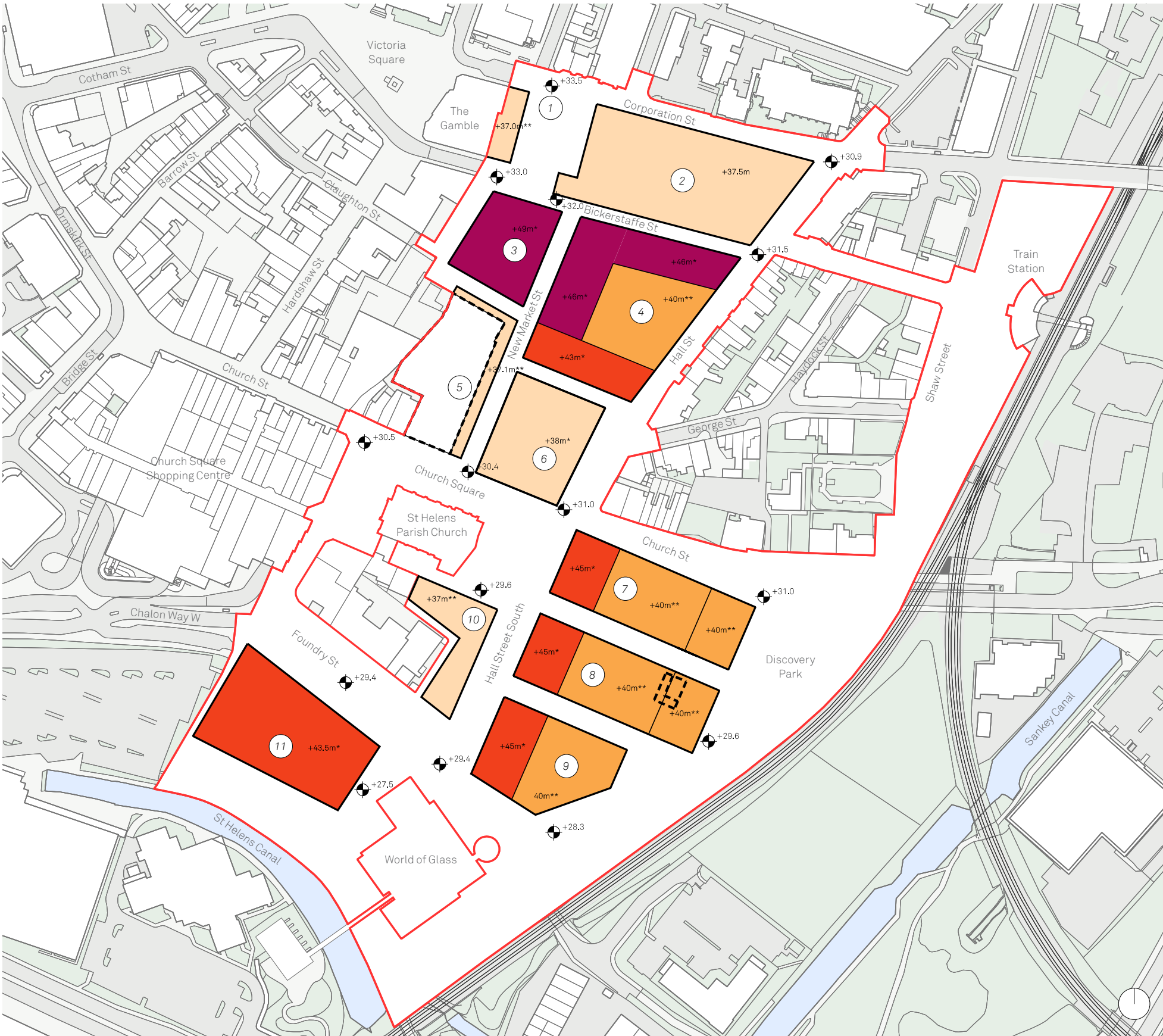
G+4

G+5

Buildings to be retained

1

Plot Number





5.5 Development Plot Height  
Maximum Development Heights

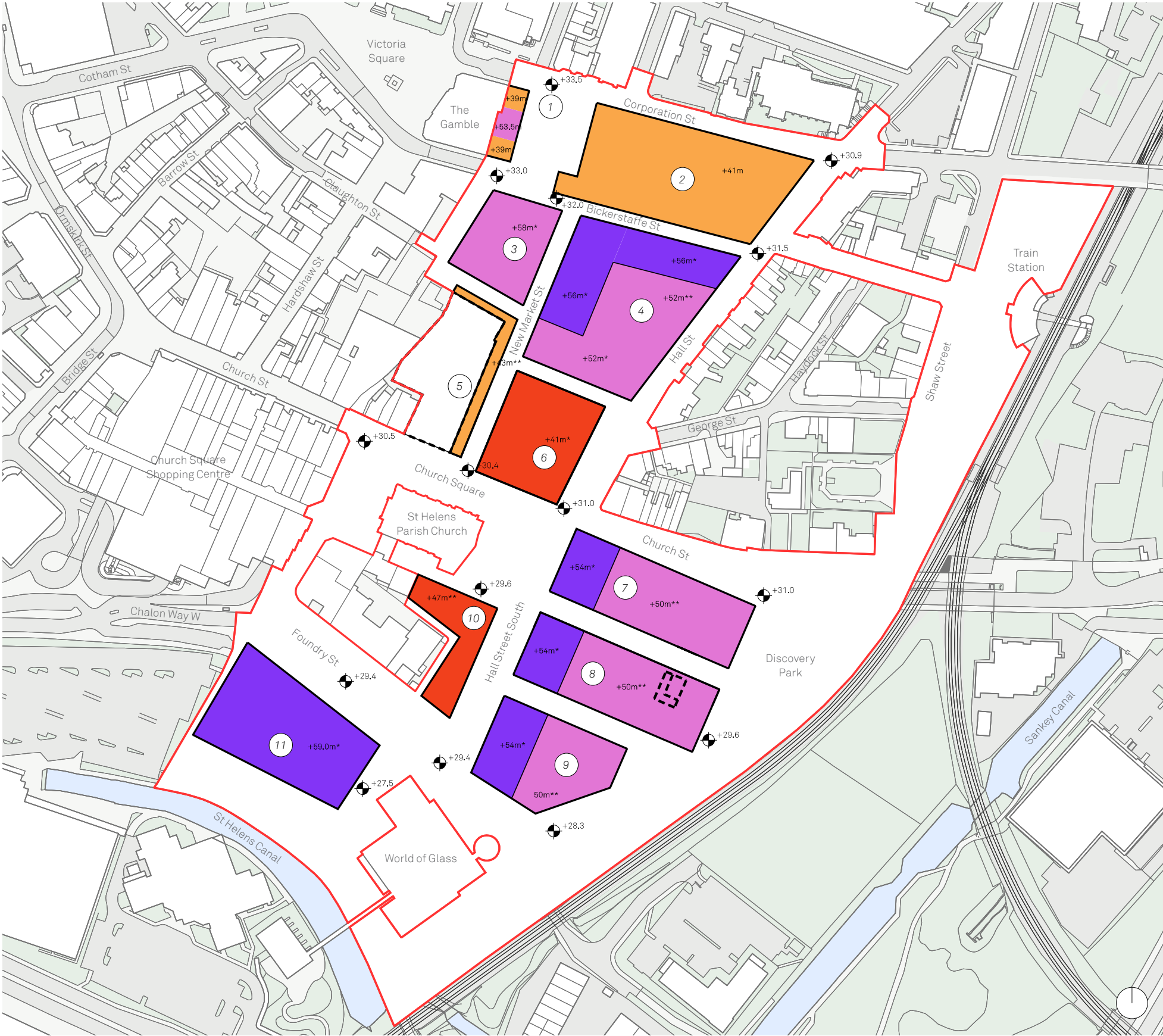
This is a outline application with matters reserved. Plot parameters illustrate the maximum height of each of the development plots. The massing across the masterplan has been carefully determined to work with and respect the historic grain of the town centre.

The heights allow for articulation of the blocks to create moments, such as point blocks on corners with extra height, markers that will improve the natural wayfinding.

The building in the Civic and Heritage Quarter such as the Town Hall, Gamble Institute, Century House, Wesley House and Theatre Royal are prominent buildings standing taller than most. The masterplan at its northern edge rises up to meet them at an appropriate civic scale, holding the edge of the new Bickerstaffe Square and overlooking the bus station.

This scale is reduced as it merges with the George Street Quarter, former M&S building and new residential blocks (Plots 7, 8 & 9) along the edge of the Discovery Park. Increasing the scale of these 3 blocks holds the edge of Church Square and South Hall Street and give it an appropriate scale alongside St Helens Parish Church and the Market Hall (plot 6).

The slight increase in height of these central blocks creates a central corridor leading from the transport hub to the north, through the town centre towards the St Helens Canal edge bringing this key asset back into the town centre alongside the World of Glass.



Key

- Application Boundary
- Ground Floor Only
- G+1
- G+2
- G+3

- G+4
- G+5
- Buildings to be retained
- Plot Number

# 5.6 Development Plot Access and Movement Creating Connections

A key move within the masterplan was to improve the permeability of the town centre. Linking different areas and assets across the town centre and beyond, drawing people through the retail and F&B offer and improving natural way-finding.

The public space will be designed with a hierarchy of streets, with managed vehicle, cycleways and pedestrian routes. The key objective is to ensure that these are positioned appropriately to provide safe and sustainable permeability through the town centre linking up with the existing network.

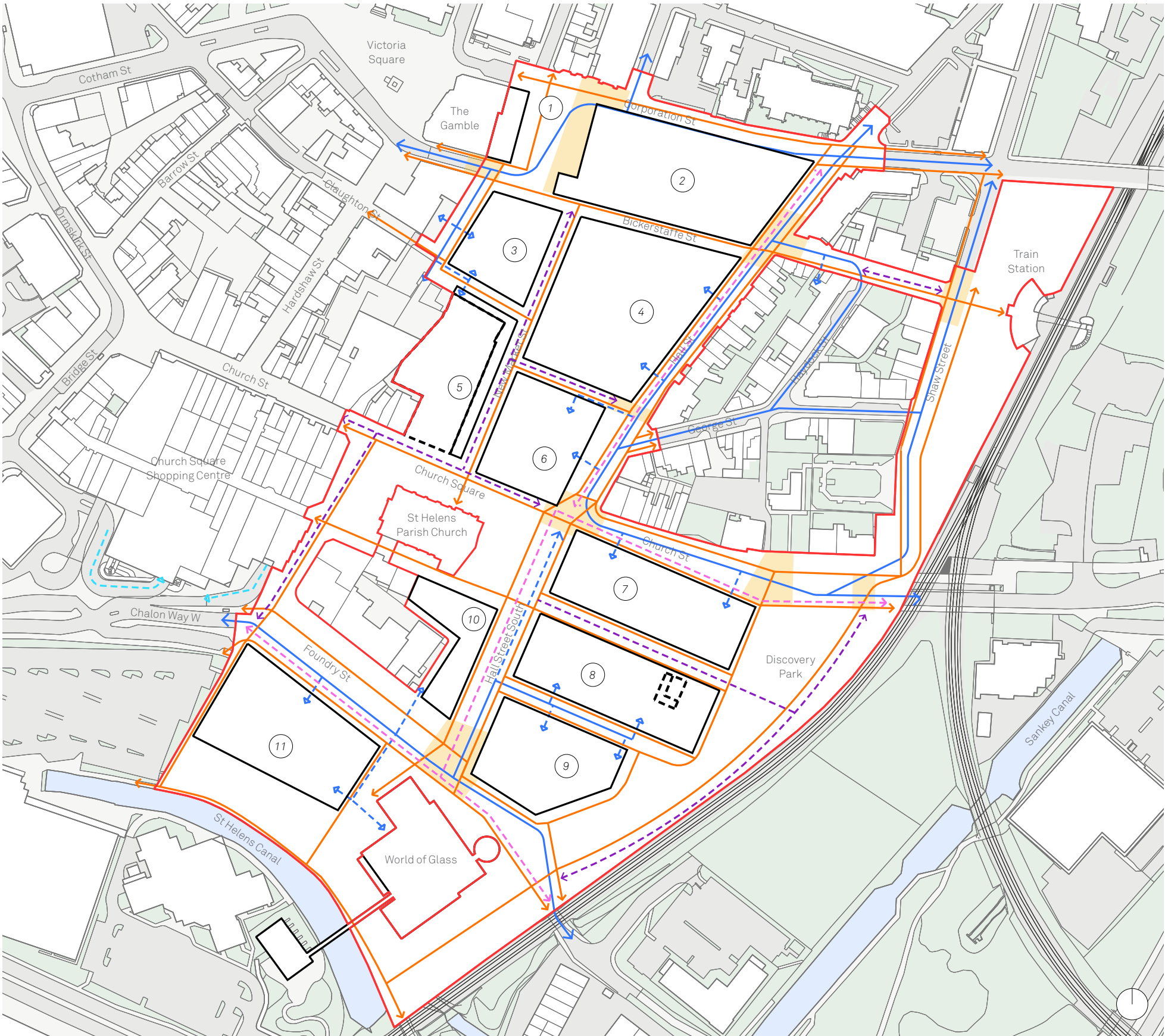
Streets will be designed to ensure people feel comfortable to walk, cycle, stop and socialise. Cycle infrastructure will be built into the plan to help facilitate low car movement and to promote low-carbon lifestyles.

A preferred elevation has been indicated for service and vehicular access to the development plots. Where possible car parking is provided within the podiums or courtyards to ensure plots are outward looking developments with active frontages.

- Key
- Application Boundary
- Vehicular route
- Cycle route in pedestrianized zone
- Designated cycle lane
- Pedestrian route
- Controlled vehicular access

- Vehicular access on this elevation
- Existing controlled vehicular access
- Key pedestrian crossing treatment
- Buildings to be retained
- 1

Plot Number





The masterplan restores the historic street pattern, a human scale linking up the town centre and improving way-finding. Providing a series of exemplary street and spaces that are sensitive to the past and are fit for the future, designed to create a vibrant town centre and neighbourhood.

The network of streets, squares and green spaces are key to the success of how the town centre functions and performs as a place for people but also in tackling the climate emergency through both the methods of travel but also the balance / detail of hard and soft materiality and drainage.

A series of new public spaces will knit the site back into the surrounding streets, defining key entrances and creating places to dwell. The new squares form part of a bigger strategy, creating a hierarchy of spaces for people across the masterplan. The public squares form an integral element of the town centre masterplan and consist of a mix of upgrading existing squares through to the reconfiguration of existing highway infrastructure, to create new squares that provide opportunities for activity and a setting to key buildings and locations within the town centre.

Key

Application Boundary

Predominantly hard public open space  
(with vehicle access where required)

Predominantly soft public open space

Key pedestrian crossing treatment

Primary vehicle street

Secondary vehicle street

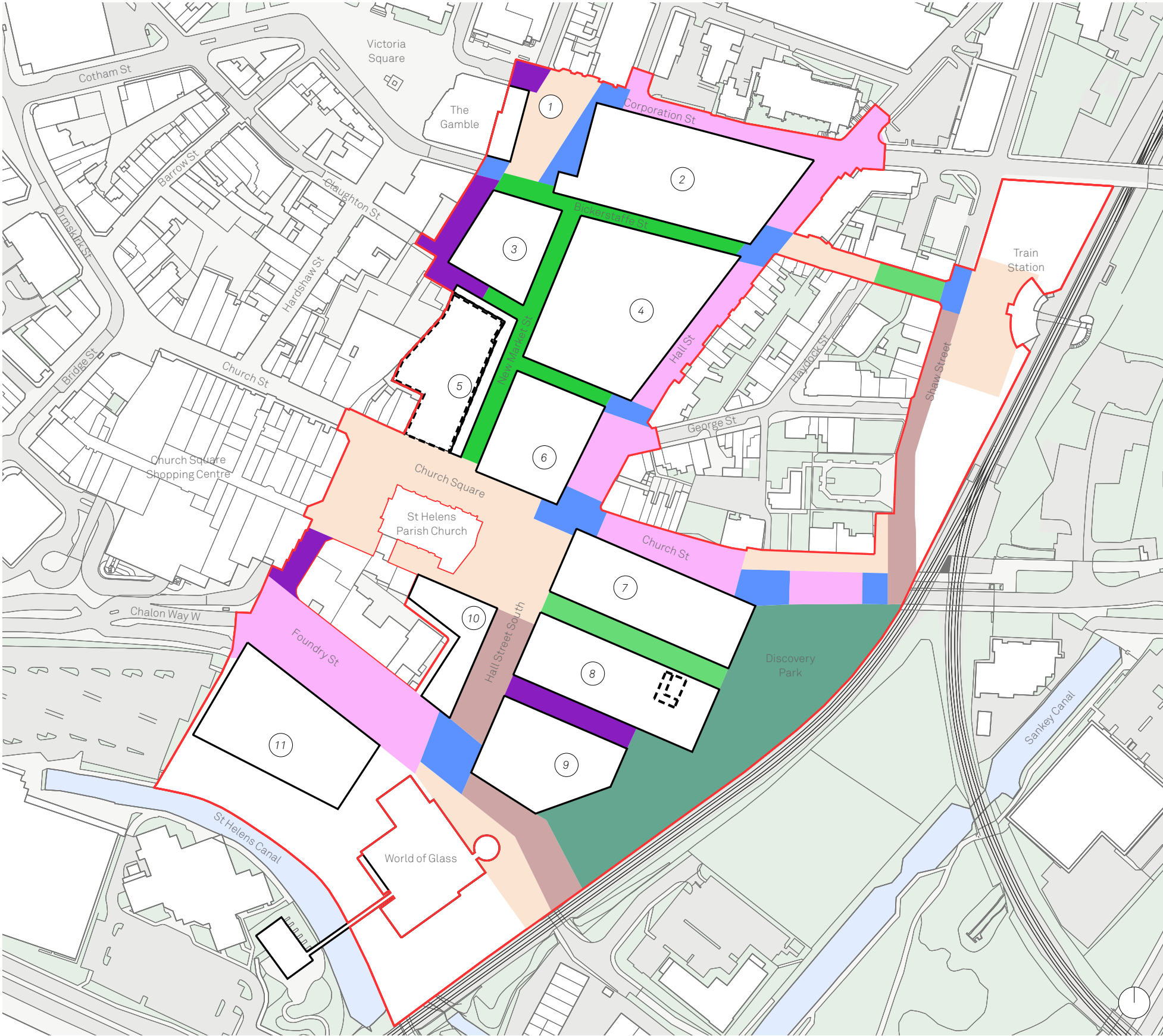
Tertiary vehicle street

Pedestrian Street

Buildings to be retained

1

Plot Number





## 6.0 Illustrative Phasing

The masterplan has been broken down into a series of 4 delivery zones with a number of development plots within each. Its envisaged that delivery zone 1 and 2 will be the first to be delivered which will have a positive impact on redeveloping the town centre with zones 3 and 4 following.

**Delivery Zone 1:** Requires the demolition of the bus station and remaining terraces between the Gamble Building and Millennium Centre. Delivering the bus station (Plot 2) and the public realm connection it through to the station. It creates a joined up transport hub and new public square, Bickerstaffe Square, in-front of the Theatre Royal. The rear extension to the Gamble (Plot 1) will come forward in-line with the works St Helens Council plan for the refurbishment of the Gamble Building.

During the construction of the bus station it will likely need to be relocated or the stops dispersed throughout the town centre. This will be done in collaboration with Mersey Travel and St Helens Council to minimise disruption.

**Delivery Zone 2:** Requires the demolition of the Hardshaw Shopping Centre and Car Park. Delivering one of the key moves within the masterplan, New Market Street, connecting the transport hub directly with the heart of the town. The development plots being delivered in one phase are the office (Plot 3), a mixed uses block (Plot 4) consisting of retail at ground floor, hotel overlooking the bus station and residential apartments and town houses, former M&S wrap (Plot 5) plus the market hall (Plot 6).

**Delivery Zone 3:** Requires the demolition of St Mary's Shopping Centre and multi-storey car park and re-routing of Chalon Way. The proposed street pattern and infrastructure will be put in place including Discovery Park, setting out the vision. This realises the key masterplan move with zone 2, to improve the north -south permeability and positioning the World of Glass and St Helens Canal back into the town centre.

Plots 7, 8 & 9 linking Church Square and Discovery Park can come forward as one or a series of phases. The plots have been designed to give the option to deliver the retail element with apartments above fronting Church Square and Hall Street South first and later deliver with the residential to the east.

**Delivery Zone 4:** No demolition is required for the delivery of this zone. The site of Plot 10 will have been demolished as part of delivery zone 3 works and Chalon Way multi-storey car park has already been reduced to just its base. The delivery of plots 10 and 11 can then come forwards independently or as one. Delivering Plot 10 first would be a priority as it completes the north-south link and the final edge of Church Square.

The phasing plan is illustrative only at this stage. The Applicant welcomes a planning condition requiring confirmation of the future phasing of the delivery of the proposed development.

Key

Application Boundary

Delivery Zone 1- Public Realm

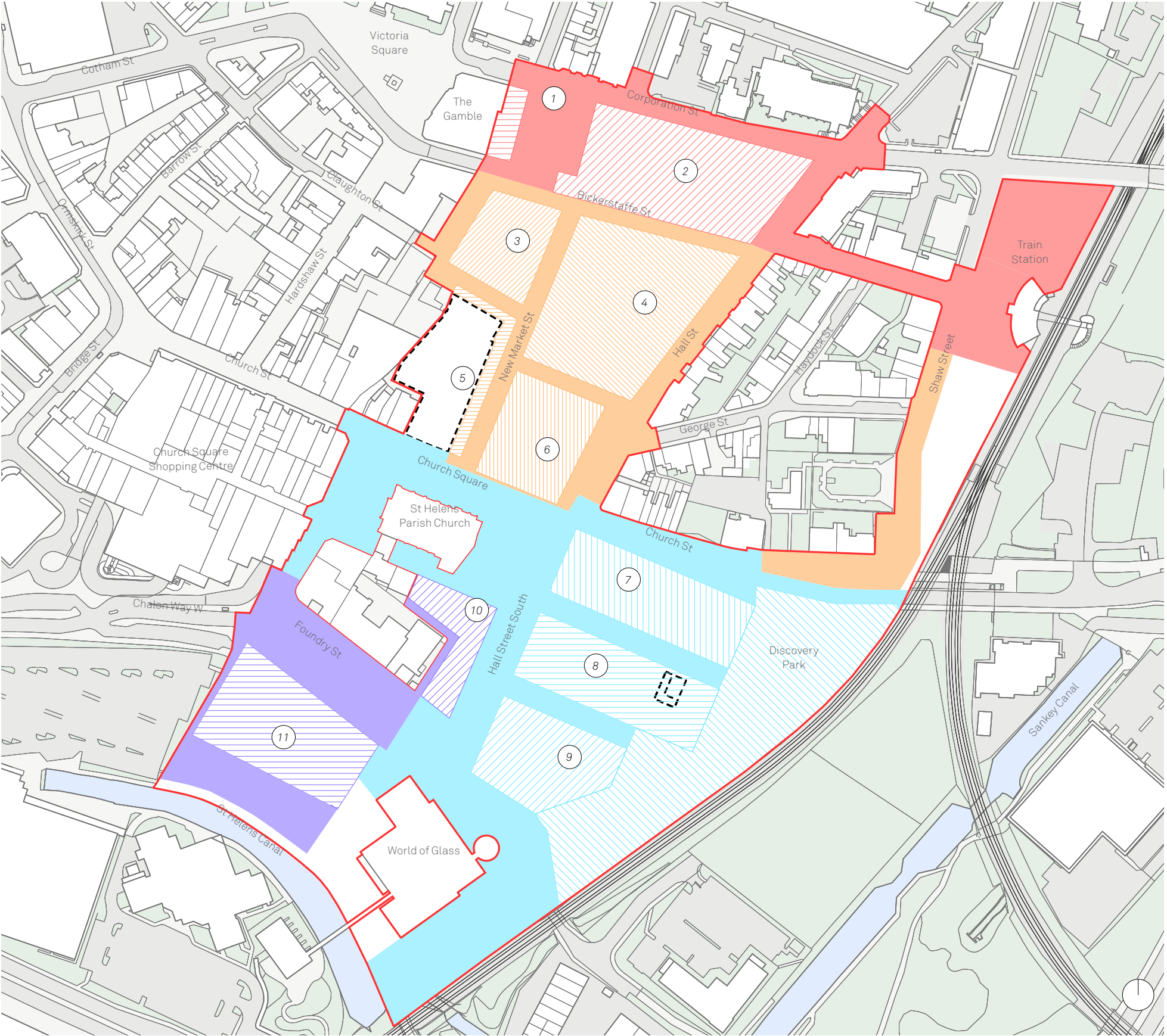
Delivery Zone 2- Public Realm

Delivery Zone 3- Public Realm

Delivery Zone 4- Public Realm

1

Plot Number



## 7.0 Area Summary



7.1 Area Summary  
Option A and B

ST HELENS MASTER PLAN PHASE 1				
Development Summary Schedule - Option A				
Plot Number	Use Classes	Plot Total GIA (m²)	Notes	Plot Total GIA (m²)
1	E (a-f) Retail/leisure/food & drink/commercial and Sui Generis (drinking establishments)	340	Use to be combined with accessible access to the Gamble Note the plot total is based on the extension footprint only 340m² and potential 130m² space to be located for ancillary bus station facilities	470
	F1(b-e) Learning and non-residential institutions			
	F2(b) Community	130		
	Sui Generis (Ancillary bus station facilities)			
2	Bus Station (Sui Generis)	1,000	Bus station accommodation, does not include the canopies	1,200
	E(a-f) Retail/leisure/food & drink/commercial	150	Approx one unit ground floor	
3	E(g)(i-ii) Office/ Research	7,500		7,700
	E(b) Restaurant/café	200	Approx one unit ground floor	
4	C1 Hotel	5,510	155 Beds	12,967
	C3 Resi	5,574	65 Units, 40 Parking Spaces	
	E(a-f) Retail/leisure/food & drink/commercial	1,753	Retail with some F&B offer at ground floor (excludes hall street) Note the plot total includes potential 130m² space to be located for ancillary bus station facilities	
	Sui Generis (Drinking establishment)			
	Sui Generis (Ancillary bus station facilities)	130		
	E(a-f) Retail/leisure/food & drink/commercial	650		
Drinking establishment (Sui generis)				
6	E(a) Market	2,000	Market hall	2,000
	E(b) F&B Sales		The market will have provision for a food court/ street food offer	
7	Drinking establishment (Sui generis)	9,037	90 Units, 34 parking Spaces	9,590
	C3 Resi			
	E(a-f) Retail/leisure/food & drink/commercial	553	Retail/ commercial overlooking Hall Street South at ground floor	
8	Drinking establishment (Sui generis)	9,037	90 Units, 30 Parking Spaces	9,590
	C3 Resi			
	E(a-f) Retail/leisure/food & drink/commercial	553	Retail/ Commercial overlooking Hall Street South	
9	Drinking establishment (Sui generis)	9,905	88 Units, 26 Parking Spaces	10,650
	C3 Resi			
	E(a-f) Retail/leisure/food & drink/commercial	745	Retail/Commercial overlooking Hall Street South	
10	Drinking establishment (Sui generis)	750	Ground floor	4,200
	E(g)(i-ii) Office Research			
	C3 Resi	3,450	Upper floors	
11	E(a-f) Retail/leisure/food & drink/commercial	7,700	90 Units, 45 Parking spaces	8,200
	E(a-f) Retail/leisure/food & drink/commercial	500	Ground floor	
	E(g)(i-ii) Office Research	Option B Only		

ST HELENS MASTER PLAN PHASE 1					
Development Summary Schedule - Option B					
Plot Number	Use Classes	Plot Total GIA (m²)		Notes	Plot Total GIA (m²)
1	E (a-f) Retail/leisure/food & drink/commercial and Sui Generis (drinking establishments)	340		Use to be combined with accessible access to the Gamble Note: This includes potential 130m² space to be located for ancillary bus station facilities	470
	F1(b-e) Learning and non-residential institutions				
	F2(b) Community	130			
	Sui Generis (Ancillary bus station facilities)				
2	Bus Station (Sui Generis)	1,000		Bus station accommodation, does not include the canopies	1,220
	E(a-f) Retail/leisure/food & drink/commercial	150		Approx one unit ground floor	
3	E(g)(i-ii) Office/ Research	7,500			7,700
	E(b) Restaurant/café	200		Approx one unit ground floor	
4	C1 Hotel	9,529		Option A only	11,412
	C3 Resi			106 Units, 40 Parking Spaces	
	E(a-f) Retail/leisure/food & drink/commercial	1,753		Retail with some F&B offer at ground floor (excludes hall street) Note: This includes potential 130m² space to be located for ancillary bus station facilities	
	Sui Generis (Drinking establishment)				
	Sui Generis (Ancillary bus station facilities)	130			
	E(a-f) Retail/leisure/food & drink/commercial	650			
Drinking establishment (Sui generis)					
5	E(a-f) Retail/leisure/food & drink/commercial	650		Ground floor	650
	Drinking establishment (Sui generis)				
6	E(a) Market	2,000		The market will have provision for a food court/ street food.	2,000
	E(b) F&B Sales				
	Drinking establishment (Sui generis)				
7	C3 Resi	9,037		90 Units, 34 parking Spaces	9,590
	E(a-f) Retail/leisure/food & drink/commercial	553		Retail/ restaurant overlooking Hall Street South	
	Drinking establishment (Sui generis)				
8	C3 Resi	9,037		90 Units, 30 Parking Spaces	9,590
	E(a-f) Retail/leisure/food & drink/commercial	553		Retail/ restaurant overlooking Hall Street South	
	Drinking establishment (Sui generis)				
9	C3 Resi	9,905		88 Units, 26 Parking Spaces	10,650
	E(a-f) Retail/leisure/food & drink/commercial	745		Retail/ restaurant overlooking Hall Street South	
	Drinking establishment (Sui generis)				
10	E(a-f) Retail/leisure/food & drink/commercial	750		Ground Floor	4,200
	Drinking establishment (Sui generis)				
	E(g)(i-ii) Office Research	3,450			
11	C3 Resi	780		Option A only	14,508
	E(a-f) Retail/leisure/food & drink/commercial				
	E(g)(i-ii) Office Research	13,728		Ground and upper floors	



Block Location Plan

NOTE:  
The development options only effect plots 4 and 11 in the area schedules.

The parts of the table shown in green represent changes that have been made to the area schedule since submission of application reference P/2022/0212/HYBR.

## 8.0 Public Realm and Access



## Streets and Spaces

The masterplan provides a series of exemplary streets and spaces that are sensitive to the past, progressive and are designed to create a vibrant future for St Helens Town Centre.

The network of streets, squares and green spaces are key to the success of how the town centre functions and performs as a place for people, but also in tackling the climate emergency through both the methods of travel but also the balance / detail of hard and soft materiality and drainage.

**The Masterplan**

1. Train Station Plaza
2. Discovery Park
3. Church Square
4. Bickerstaffe Square
5. World of Glass Square
6. New Market Street
7. Hall Street
8. New Hall Street South

A series of new squares will knit the site back into the surrounding streets, defining key entrances and creating active meeting places. The squares form part of a bigger strategy, creating a hierarchy of spaces for people across the masterplan. They are anticipated to consist of a balance of hard and soft materials to create variety, ensuring they can be used for many different purposes and contribute to biodiversity.

The public squares form an integral element of the town centre masterplan and consist of a mix of upgrading existing squares through to the reconfiguration of existing highway infrastructure, to create new squares that provide opportunities for activity and a setting to key buildings and locations within the town centre.





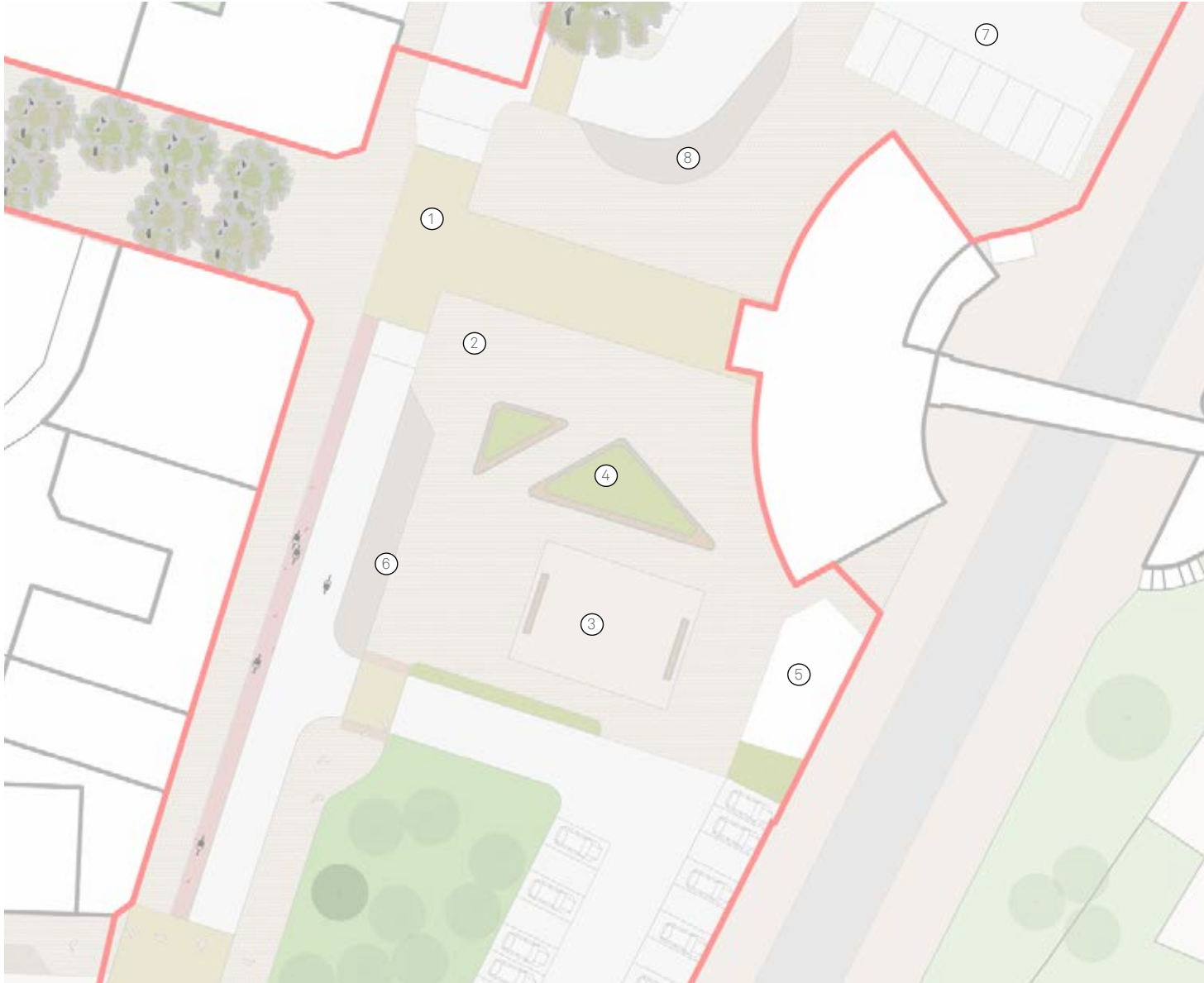
# 8.2.1 The Squares

## Train Station Plaza

This is the key gateway into the town centre for visitors arriving by train, providing the opportunity to give a positive first impression of St Helens as a place. A high quality public square provides an environment that pedestrians can breathe and take stock of their surroundings before becoming familiar with the direction in which they want to travel. It can be a place to meet and spend time before catching a train. This space needs to be designed to guide people and offer a sense of place that celebrates St Helens with high quality materials, lighting, tree planting, seating and artwork being key to the success of the space. Key facilities such as cycle storage, short stay drop off / pick up laybys and access to taxis should be provided. A pedestrian super crossing across Shaw Street to connect with the pedestrianised Bickerstaffe Street will make it legible for people to orientate themselves towards the town centre.

### Key Design Principles

1. A strong pedestrian link with a pedestrian super crossing.
2. Vertical lighting elements providing strong wayfinding markers emphasising entrance.
3. Art, water and tree planting to enrich and animate public realm.
4. Flexible social space creating greener environment and encouraging people to dwell.
5. Cycle Hub facilities.
6. Rationalised drop-off / pick up space to the edge of space so pedestrians are prioritised.
7. Blue Badge Parking.
8. Taxi rank.





8.2.2 The Squares  
Bickerstaffe Square

Through the redevelopment of the Bus Station and the demolition of existing buildings, there has been a key urban design move within the Masterplan to create a public space that provides a setting for the Theatre Royal, the new Gamble extension and the proposed office block.

It is important that the public realm design for this whole area is designed and treated in such a way that is befitting of these key civic assets and that pedestrian activity is encouraged and dwell time facilitated.

Key to this, is ensuring that bus movements don't overly dominate the environment and dated highway infrastructure doesn't clutter and detract from pedestrian movement and the general look and feel of the square. This will be achieved through the use of high quality surface materials, soft landscaping and a well considered design approach where pedestrians feel comfortable to cross the space.

Key Design Principles

- 1. Spill out space for the Theatre and The Gamble Extension.
- 2. High quality pedestrian crossings along key desire lines to connect key assets.
- 3. Use of high quality materials across the space to emphasise pedestrian focus.
- 4. Use of soft landscaping and street trees to green the square and guide pedestrian circulation.





# 8.2.3 The Squares

## World of Glass Square

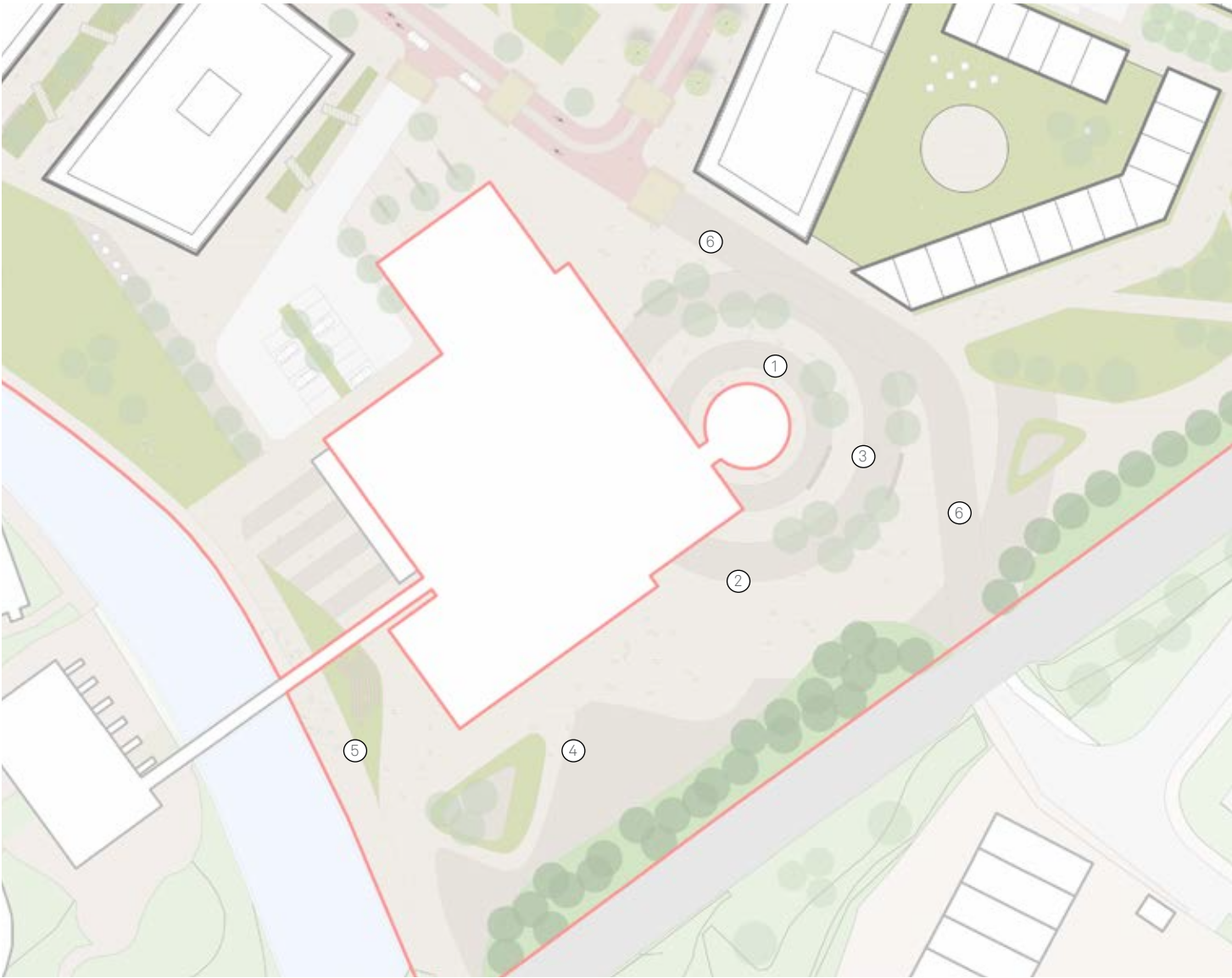
The World of Glass is a key asset for the town and the public space around this building provides a great opportunity to compliment, celebrate and enhance the internal offer.

The public realm can be used to strengthen the inside / outside relationship and provide a taste for visitors that may encourage them to step inside and experience the museum and its facilities.

Located at the southern end of the proposed Discovery Park, this space has fantastic potential to provide external exhibition space and host events that add to the museums annual programme, aswell as an outdoor classroom to educate and teach students and visitors about the town's glass heritage. There are opportunities for displaying external glass artwork within the public realm as a reference / homage to the towns glass trade and enforce a sense of tangible pride and sense of place that is uniquely St Helens. The treatment of this Square can provide a better visual and physical connection between The World of Glass and the town centre to the north.

### Key Design Principles

1. Create a sense of arrival and give a strong first impression.
2. Opportunities for external exhibition space.
3. Extend the programs of the museum beyond the building.
4. Creation of outdoor teaching and learning spaces.
5. Reconnect with the canal encouraging visitors to engage with the history of 'The Hotties'.
6. Strengthen the connection across the street to tie into town centre and Discovery Park.



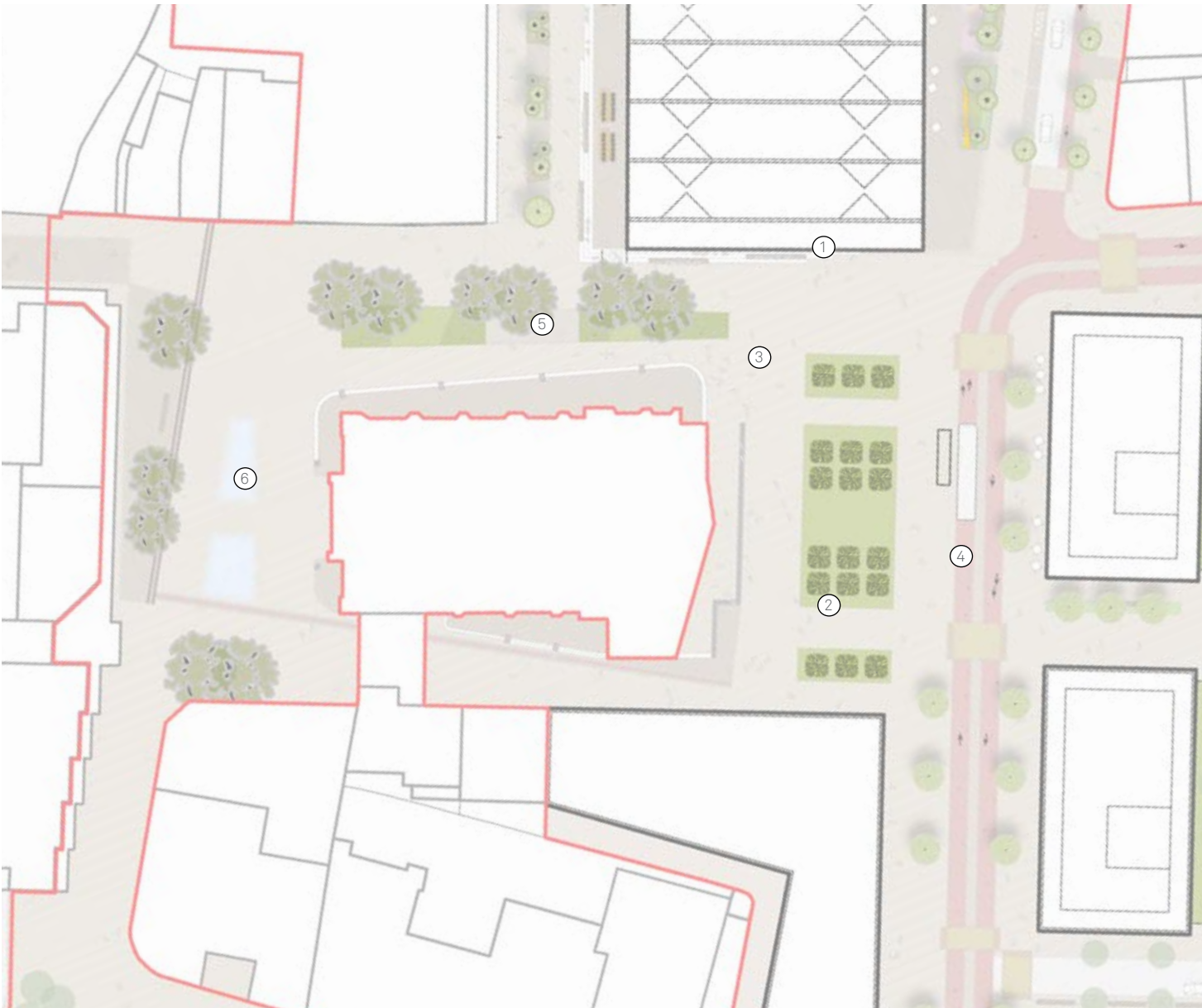


8.2.4 The Squares  
Church Square

This public space is seen as the centre of the town and with new building developments proposed to it's northern and eastern edges with strong active frontages, there is an opportunity to upgrade this square and create a fantastic space that reinforces its standing as the main civic space and heart of the town. Through the demolition of the St Mary's Centre, Church Square will increase in size considerably and with this is an opportunity to further enhance the setting around the church and introduce features that soften the mainly hard space, whilst introducing interactive elements that encourage people to dwell and meet and celebrate the town's heritage. This area can compliment the western side of the church which functions as a key event space. Successful Public Squares are those with strong edges and the design of this space will look to facilitate that relationship between the buildings and the public realm. The use of high quality surface materials will emphasise Church Square within the wider hierarchy of streets and public spaces within the town.

Key Design Principles

- 1. Encourage opportunities to activate the edges with building spill out space - including dining from the market hall.
- 2. Green the space with pockets of soft landscaping and rain gardens.
- 3. Explore ways of activating the space through interactive features and artwork.
- 4. Extend the Square eastwards across to new development plots on the east of space.
- 5. Upgrade the existing civic hard space with quality furniture and additional suds planting.
- 6. Retain the principle of a hard event space to the west of the Church.





# 8.3 The Streets

## Street Typologies

The Chartered Institute of Highways and Transport (CIHT) CIHT have established street typologies which vary in their layout, form and appropriateness to location in regards the type and volume of pedestrian and vehicle movement. Reference is made to these when defining street typologies for St Helens.

### Street Typologies

**Enhanced Streets** - These are the existing streets within the Masterplan which will be enhanced through de cluttering of unnecessary street furniture and signage. Widening of footpaths and re-surfacing with high quality materials. Street tree planting and inclusion of rain gardens where possible.

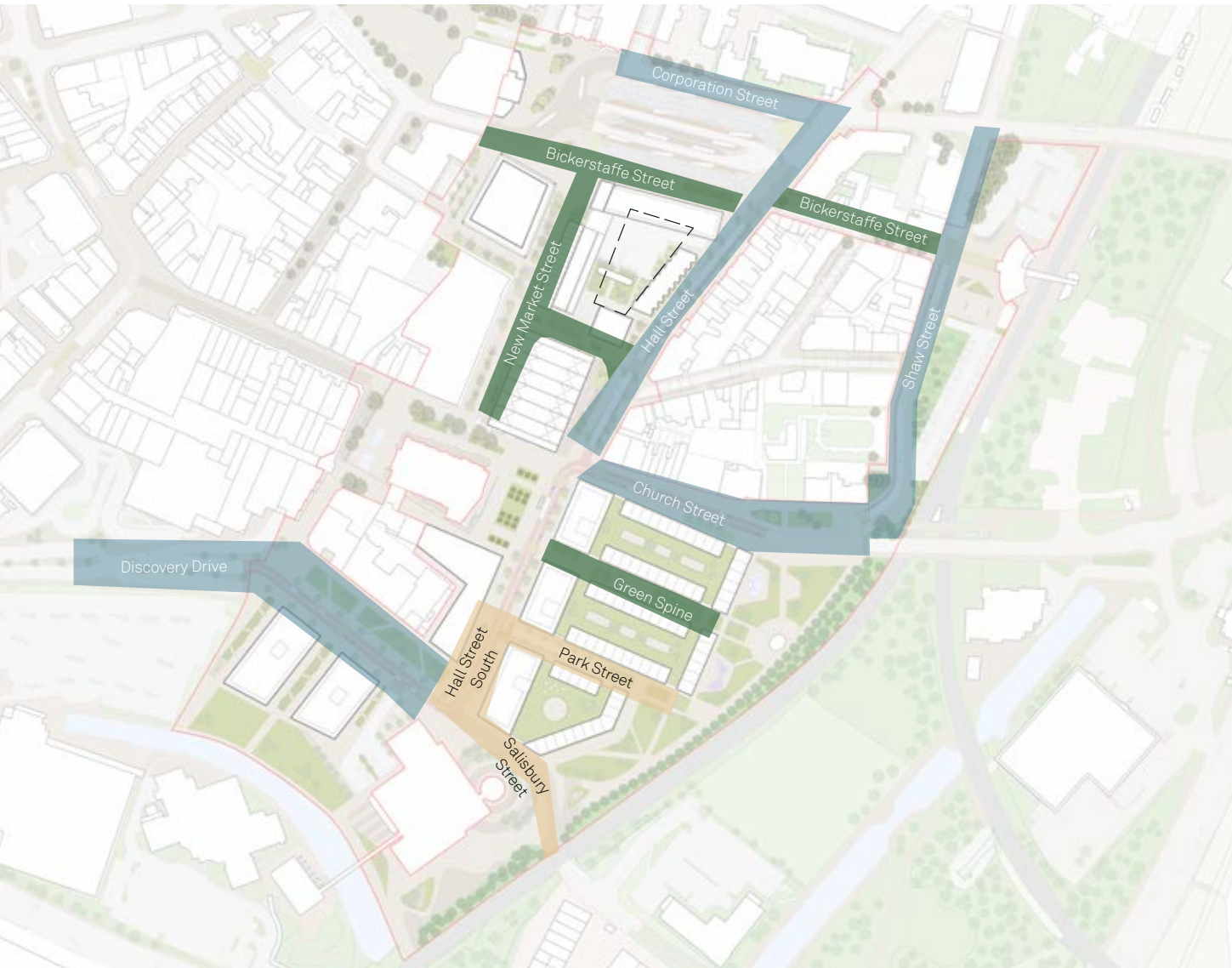
**Informal Streets** These streets will have a delineation between footway and carriageway but low kerbs and materiality that makes the street look and feel more pedestrian friendly. Is a transition street and aimed to reduce vehicle speeds

**Pedestrian Streets** - Controlled access only for vehicles. Surfaces are flush and pedestrians / cyclists have priority. High quality surface materials across the width of the street with street trees, planting and furniture to encourage dwell time.

The design of St Helens' needs to encourage, cater for and facilitate use by people from all walks of life and be inclusive to all user groups. The public realm design, which includes all the streets and the public open spaces, must retain this core principle at the heart of the design process. The healthy street indicators were formed in London to make sure people's health was at the centre of decision making within public realm design and to encourage walking, cycling and public transport.

### 10 Healthy Street Indicators

1. **Healthy streets approach:** <https://www.londoncouncils.gov.uk/our-key-themes/transport/healthy-streets>.
2. Pedestrians from all walks of life: Streets and public spaces should be welcoming places for everyone to walk, spend time and engage in community life.
3. People choose to walk and cycle: Walking and cycling are the most sustainable ways to travel. To encourage and enable people to walk and cycle the public realm should reduce the volume of motor traffic and improve the experience of being on the street.
4. Clean air: Improving air quality delivers benefits for everyone and reduces unfair health inequalities.
5. People feel safe: The whole community should feel safe at all times. People should not feel worried about road danger or experience threats to their personal safety.
6. Not too noisy: Reducing the noise impacts of motor traffic and improve the ambience of street environments.
7. Easy to cross: Making streets easy to cross is important to encourage more walking and to connect communities. Direct routes and being able to cross streets at their convenience.
8. Shade and shelter: Providing shade and shelter from high winds, heavy rain and direct sun enables everybody to use streets whatever the weather.
9. Things to see and do: People are more likely to use streets when their journey is interesting and stimulating with attractive views, buildings, planting and street art and where other people are using the street.
10. People feel relaxed: A wider range of people will choose to walk or cycle if the streets are not dominated by traffic and if pavements are not overcrowded, dirty, cluttered or in disrepair.



## Healthy Streets Indicators





8.3.1 The Streets  
Enhanced Streets

The Enhanced Streets are existing streets within the town centre that provide the primary movement for vehicles, pedestrians and cyclists to move through the town centre. These streets are to be designed with reference to the relative street typologies set out in the CIHT and Healthy street design principles. To encourage people to choose to walk and cycle and facilitate the benefit from all of the social, environmental and wellbeing that comes with high quality public realm and streets. These streets should have a delineation between carriageway and footpaths but footpaths should be widened where possible and decluttered. Delineated cycle lanes should be encouraged where there is sufficient space to include them and in line with current best practice and national guidelines. SuDS in the form of rain gardens and tree pits should also be explored. These key streets are a critical opportunity to tackle the climate emergency and an opportunity to reduce carbon emissions.

**Hall Street**  
With the redevelopment of the former Hardshaw site into a high quality mixed use development with active frontages in the form of the new townhouses fronting on to Hall Street, this will transform the street by providing activity and natural surveillance on both sides. The street will benefit from a better human scale and a better ratio of building to street width to create a strong residential street in the heart of the town centre.

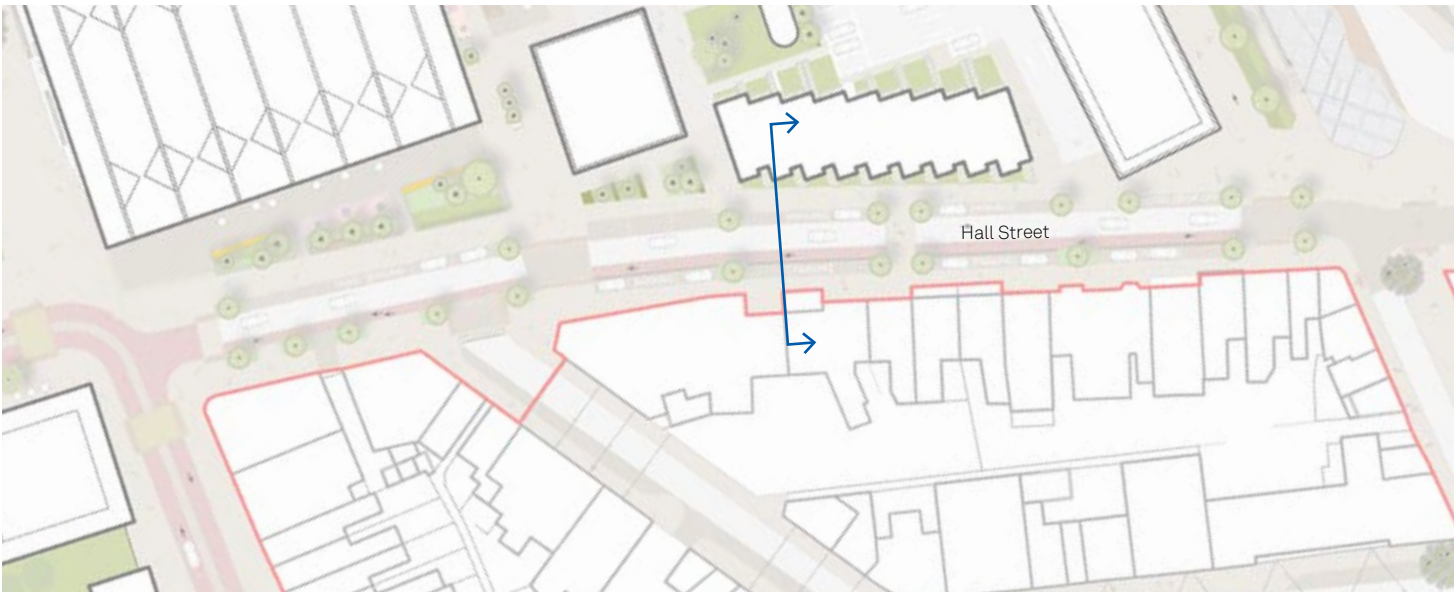
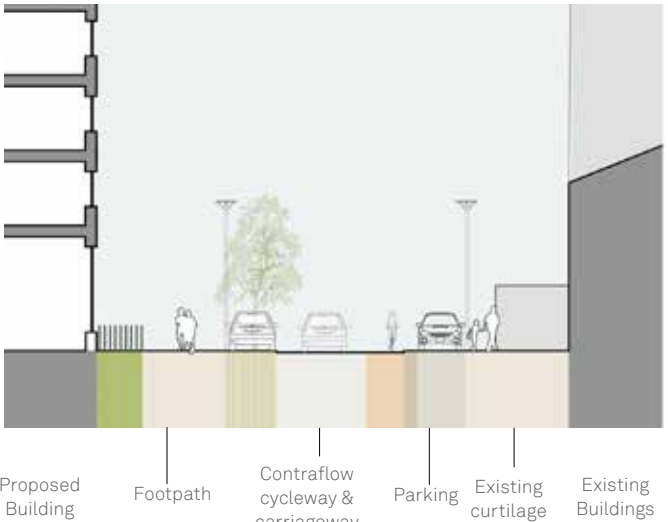
The carriageway can be tightened up to slow vehicles and the widening of footpaths with high quality surface materials. A street design rationalises the property interface through quality boundary treatments and softening the street where possible through street trees and rain garden planting.

A reconfiguration of the streets parking, loading bays and taxi rank locations and layout into a more efficient arrangement will provide a more legible and comfortable streetscene. The potential

to introduce a cycle lane along Hall Street will be explored to facilitate and encourage active travel through the town centre.

Key Design Principles

- 1. Emphasise key junctions to aid orientation and strengthen pedestrian crossing points.
- 2. Introduce defensible boundaries to townhouse units through use of railings and hedge planting to soften the street.
- 3. Generous footpaths with high quality paving materials.
- 4. Narrow the carriageway through physical kerb widths and visually through use of surface materials and vertical features.
- 5. Rationalise location of parking bays and taxi rank.
- 6. Potential cycle lane to encourage active travel.





8.3.2 The Streets  
Enhanced Streets

**Corporation Street**  
Corporation street to the north of the bus station will be turned from a one way road eastwards into a two way road. Through the re-design and closing of Bickerstaffe Street to vehicles, it is necessary to introduce two way movement. The street will also benefit from decluttering and the removal of unnecessary guard railing through a reconfigured relationship with the new bus station to create a better pedestrian experience and interface with the Church and east west pedestrian movement.

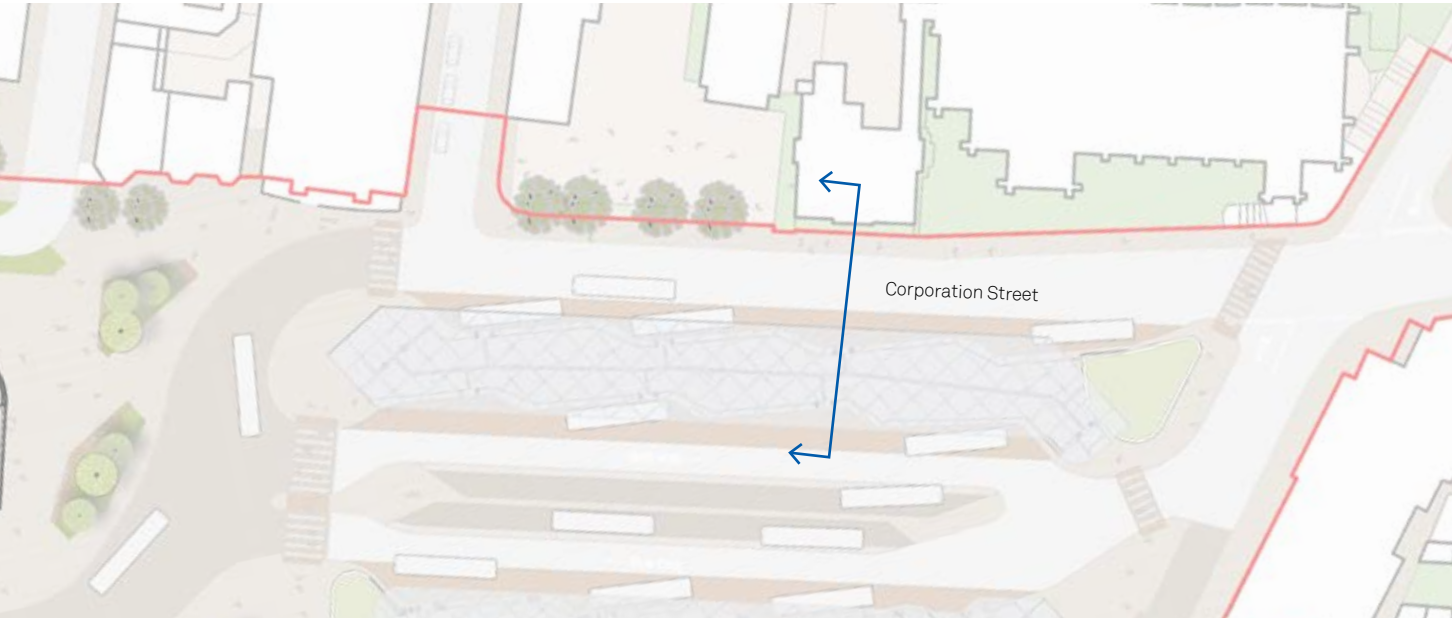
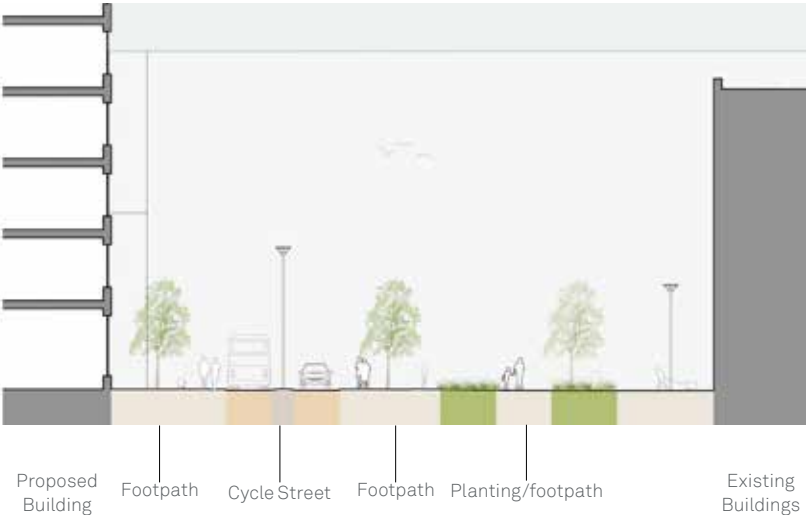
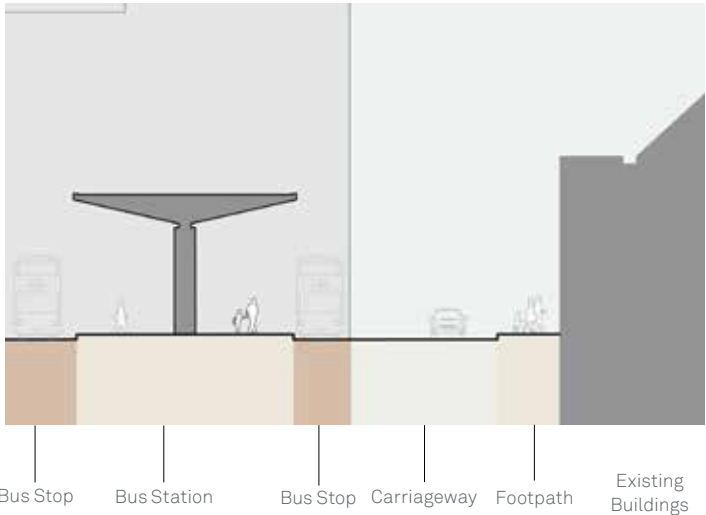
High quality pedestrian crossings at both the east and west ends will facilitate better access between the Theatre and the town centre and north / south movement along Hall Street.

- Key Design Principles**
- 1. Declutter the footpaths from unnecessary furniture and signage.
  - 2. Convert street to Two-Way Traffic movement.
  - 3. Widen Northern footpath and introduce street trees.
  - 4. Have a strong interface and relationship with bus station.
  - 5. High quality pedestrian crossings.

**Discovery Drive**  
Formerly called Chalon Way, an overly engineered piece of highway infrastructure, it provided a physical and visual barrier to pedestrian movement into the town centre from the neighbourhoods and town assets to the south. A key move identified in the masterplan was to remove the Chalon Way roundabout and general through traffic movement around this southern side of the town centre. This has provided an opportunity to downgrade the extent of the infrastructure associated with this street. Narrowing of the carriageway into a tightened two way street and utilising the space leftover as an opportunity to encourage high quality building spill out space that activates the street and introduce suds planting, street trees and other potential uses such as children's play, lighting and artwork.

A segregated cycle lane along Discovery Drive would provide a key cycle connection in and out of the town centre and connections through to the stadium and Discovery Park and the train station. This should be explored in regards to the overall transport, access and movement strategy.

- Key Design Principles**
- 1. Narrowed carriageway.
  - 2. Generous footpaths with high quality paving materials.
  - 3. Softening of the footpath areas with rain gardens and tree planting where possible.
  - 4. Potential for a segregated two way cycle lane.
  - 5. Opportunities for play, artwork and high quality, controlled spill out space / activity.





8.3.3 The Streets  
Enhanced Street

Church Street

Church Street will be transformed into a high-quality enhanced street with the key move being the removal of the Chalon Way roundabout. Church Street is a relatively wide street in places especially with the new Discovery Park on the south-eastern side. Care must be taken to design the street to ensure it still has engaging edges and is a place where pedestrians and cyclists feel comfortable moving through and spending time. Design tools such as keeping the carriageway widths reduced and providing horizontal and vertical deflections to visually narrow the road will help in reducing vehicle speeds. Courtesy crossings at key pedestrian desire lines and the use of high quality materials both on footpaths, kerbs and crossings will help create a balanced streetscene. A key design principle for Church Street is the visual and physical connection between the historic Quaker Meeting House and Discovery Park. The design language of the street

should reference this. Due to the width of Church Street, there is potential to integrate a segregated cycle lane and also explore opportunities for suds to make a real positive impact on reducing carbon emissions and encouraging more pedestrians and cyclists to occupy the street whilst increasing biodiversity and improving the publics physical and mental wellbeing.

Key Design Principles

1. Narrow the carriageway and potential for central median.
2. Potential for a segregated two way cycle lane.
3. Generous footpaths with high quality paving materials.
4. Softening of the footpath areas with rain gardens and tree planting where possible.
5. Pedestrian courtesy crossings on key desire lines.
6. Visual and physical material connection between Quaker Meeting House and Discovery Park.

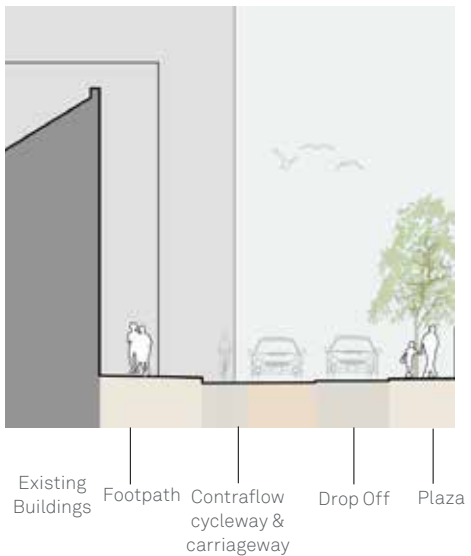
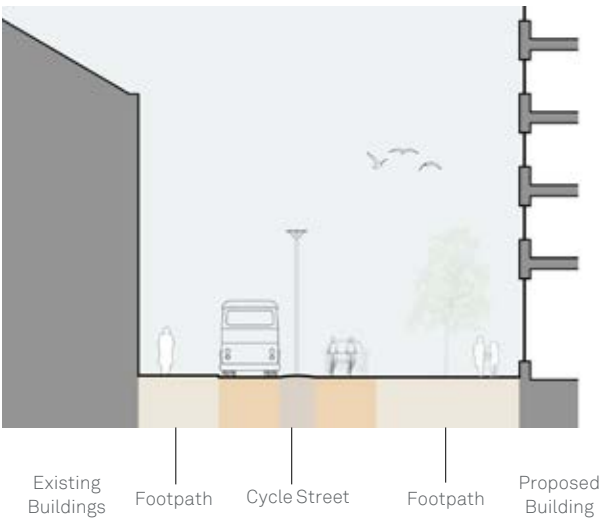
Shaw Street

Currently Shaw Street is a one way northbound street from Church Street up to the train station and two way from Corporation Street to the train station roundabout. The design intent is to remove the roundabout at the train station entrance and replace this with a public space that creates a better sense of arrival for pedestrians arriving in the town centre off the train. A pedestrian super-crossing will better connect people crossing Shaw Street and onto Bickerstaffe Street as they move between the train station and the bus station / town centre. Shaw Street has the potential to be opened up to two way vehicle movement along its length and provide better connections between the northern neighbourhoods and the new Discovery Park, stadium and World of Glass to the South. A re-configuration of the junction with Church Street will be required to facilitate this two way vehicle access as well as a rationalisation of the existing

coach and on street parking. The car park by the train station will largely remain as is and new drop off laybys can be provided adjacent to the new plaza space on Shaw Street. Street trees, high quality footpath materials and potential for a segregated cycle lane

Key Design Principles

1. Potential to change the street to a two way road or reverse flow direction
2. Removal of the roundabout and introduction of pedestrian super-crossing on alignment with Bickerstaffe Street
3. Potential for Two-Way Cycle Lane
4. Reconfigure junction with Church Street to allow two way movement
5. Rationalise existing on-street parking, coach drop and proposed train station drop off layby.





8.3.4 The Streets  
Informal Streets

These streets are intended to provide a transition between the Enhanced Streets and Pedestrian Streets. The design of these streets will facilitate this transition through a number of ways including the reduction in kerb height, tighter carriageway widths, lower vehicle speed limits and the change in surface materials within the carriageway and footpaths and general more pedestrian friendly look and feel to the streetscene. These streets are New Hall Street South, Salisbury Street and Park Street. These are labelled on the adjacent plan and are focussed around the southern side of the town centre adjacent to The World of Glass and the proposed southern residential plot to the north of this. The street design of these informal streets should further reduce vehicle speeds and facilitate pedestrian activity.



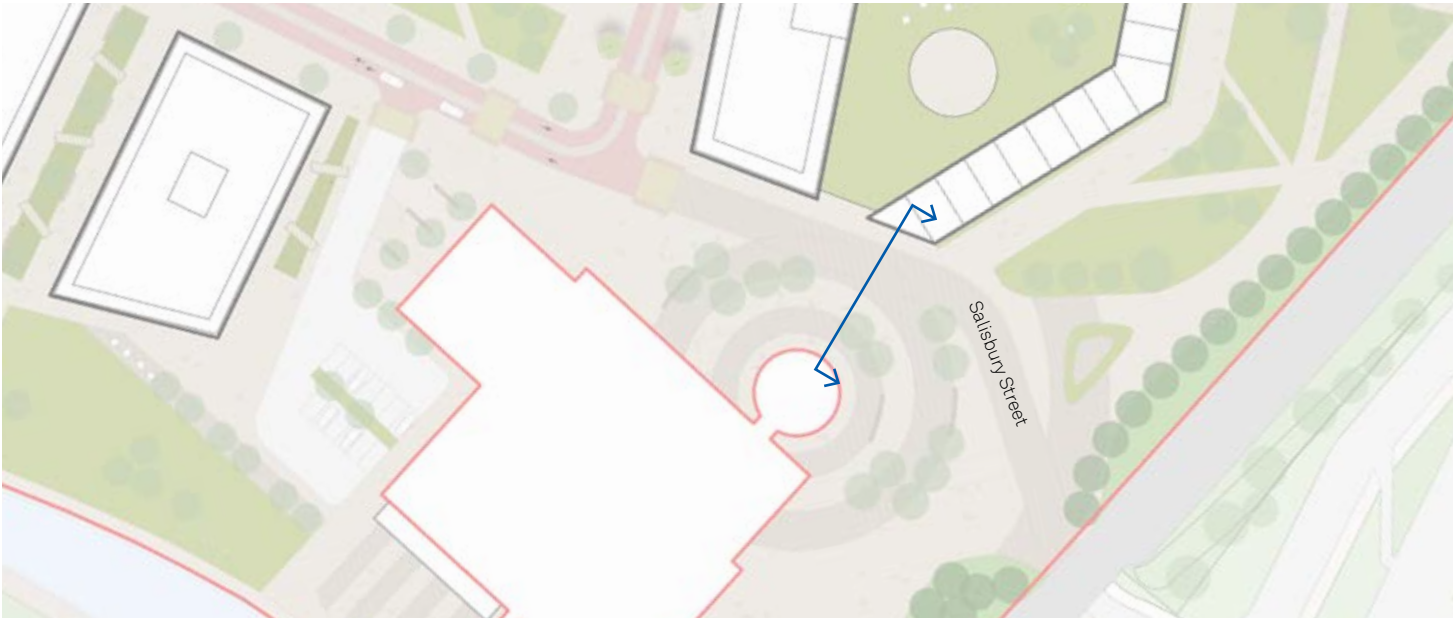
Salisbury Street

This street provides access between Discovery Drive to the north of the new World of Glass Square and through the existing railway bridge to access the Chalon Way Industrial Estate. This is a two way access only street and therefore vehicle numbers will be limited. Traffic signals at the bridge will be retained. The proposals for this street are focussed around the western side of the railway line as it cuts through between Discovery Park to the north and The World of Glass Square to the south. Features such as narrow carriageway widths, low kerbs and materiality within the street should be included to make vehicles feel like a guest and that they are passing through a pedestrian focussed environment. This street is the primary movement link between the town centre and the Totally Wicked Stadium. On match and event days, this is a busy thoroughfare for pedestrians and therefore the street design should facilitate this by making it accessible and comfortable for

this movement. On the Eastern side of the railway line, the street provides access to the existing building units including the St Helens Chamber before terminating and turning into a pedestrian footpath link across the canal towards the stadium.

Key Design Principles

- 1. Low kerbs and a change in surface materiality to emphasise transition to a more pedestrian focussed environment.
- 2. Facilitate strong pedestrian desire lines through suitable footpath widths, crossings and materiality associated with movement between the Park, Town Centre, The Canal and The Stadium.





8.3.5 The Streets  
Informal Streets

Hall Street South

The southern section of Hall Street South provides access to the southern and central new residential development plots to the east between this street and Discovery Park. Between Park Street and Church Street, Hall Street South becomes 'access only' for restricted vehicles only such as buses and emergency vehicles. There will be no through traffic permitted for general vehicles. The northern half of this street then becomes an extension to the Eastern side of Church Square. Creating a high quality pedestrian focussed civic space in the heart of the town that encourages spill out activity from the New Market building to the north and the ground floor commercial / retail uses the new building plots on the Eastern edge. The southern section of this street becomes that transition between the Enhanced Street of Discovery Drive to the South and the public square to the North. Rain gardens, street trees and a cycle lane are key features that should be integrated where possible within the street layout.

Key Design Principles

1. A narrow carriageway with low kerbs but still a delineation between footways and carriageway.
2. Use of high quality surface materials.
3. Explore opportunities for street trees and SuDS in the form of rain gardens
4. Potential to integrate a segregated cycle lane.
5. Facilitate access to Park Street
6. Through traffic access control measures for the Church Square section of street to control which vehicles connect through to Hall Street / Church Street.

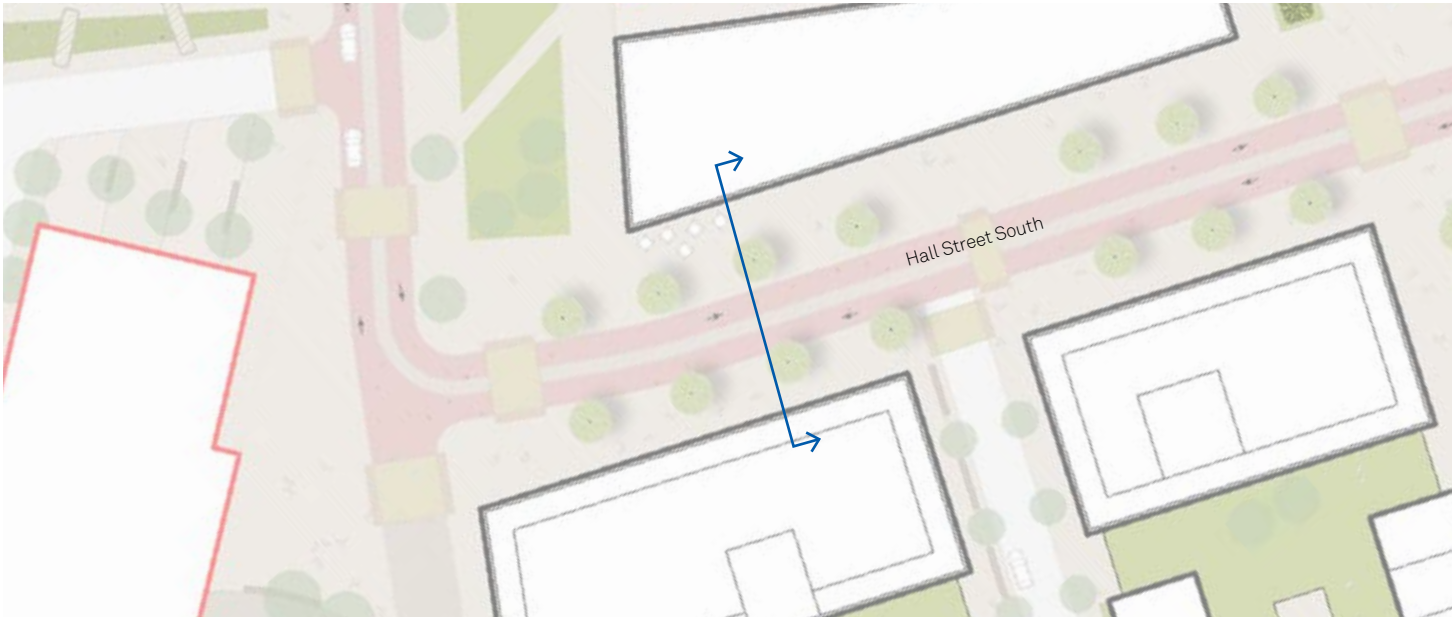
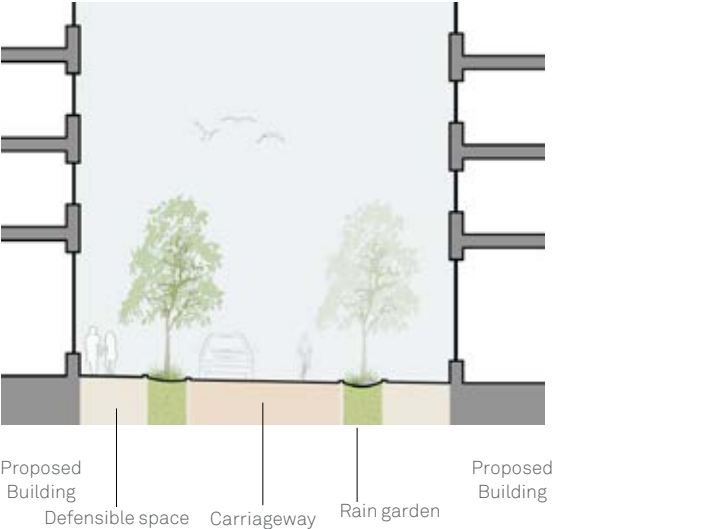


Park Street

This new street runs East to West between the southern and central development plots with the Park on the Eats and Hall Street South to the West. This street provides vehicle access only to these two development plots to the podium racks and emergency / service vehicle access to the street frontage. A further narrowing of the carriageway and street layout, design, boundary treatment and character should emphasise this step change in vehicle numbers and speed. Thus creating a pedestrian focussed street that encourages people to occupy the street and vehicles feel like a guest. With strong active residential focussed frontages, this street will have a strong and comfortable human scale.

Key Design Principles

1. Access to development plot podium car parks.
2. Defensible space at building thresholds.
3. Low vehicle speeds.
4. Softening of the street with trees and planting.
5. Potential for introduction of play elements
6. A pedestrian focussed look and feel





8.4 The Streets  
Pedestrian Streets

The new Pedestrian Streets are a significant part of the public realm strategy. With only emergency and controlled service vehicles permitted. These streets facilitate pedestrians and cyclists to activate the streets and move through and dwell in the external environment of the town centre.

These streets must look and feel high quality and have strong interfaces with their edges, have trees and interesting features to look at to encourage people to walk and cycle, for shops and cafes to spill out and activate the street and for families to feel comfortable enough to meet and play.

The streets should be of a human scale and have a comfortable enclosure ratio between the height of adjacent buildings and the width of the street. They have been located along strong movement desire lines between key assets so there is sufficient number of people to occupy the street. Key views with landmarks at their ends is encouraged to aid legibility and enhance the sense of place.

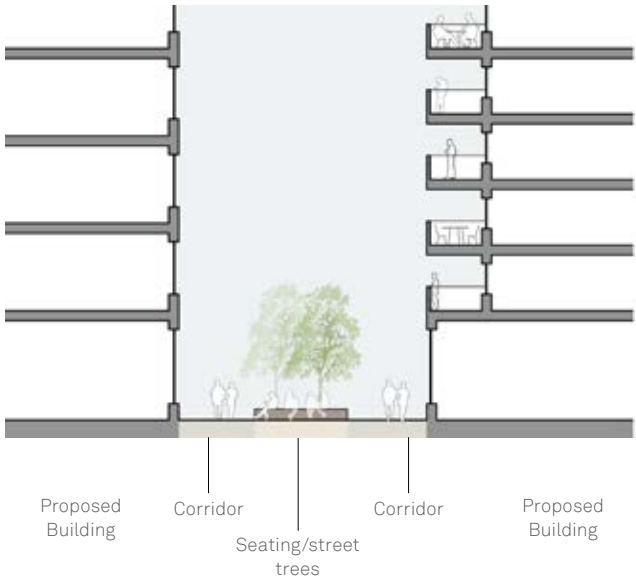


**New Market Street**

A high quality pedestrianised street that provides a direct link between the bus station and Church Square. Framing views southwards of St. Helens Parish Church spire, it is animated by shops, cafés and businesses that spill out and activate the space. Active frontages and a range of interesting interfaces at the edges of the street will enhance the strong relationship between the internal/external environments and breathe life into this street as a hub of activity. A strong pedestrian desire line between the bus station and the new Market building and retail core will ensure a bustling life. A simple and elegant treatment to the public realm is needed with high quality materials, street trees and furniture but not clutter the space and allow room for people to occupy.



- Key Design Principles**
1. Emphasise the views of the Parish Church spire.
  2. Use high quality surface materials.
  3. Arrange seating into groups and favour south facing towards the sun and church.
  4. Explore opportunities for subtle artwork and / or heritage interpretation.
  5. Activate the edges by encouraging and providing space for pavement cafés.





8.4.1 The Streets  
Pedestrian Streets

Bickerstaffe Street West

This street is closed to general through traffic and becomes a strong pedestrianised axis route between the train station to the east, the bus station and through to Bickerstaffe Square and Victoria Square further West. This street provides an opportunity to allow the bus station to breathe and for pedestrians to spill out and dwell. High quality surface materials and rain garden planting on the interface between the street and the bus station will create a fantastic sense of arrival for those coming into the town centre via bus.

Key Design Principles

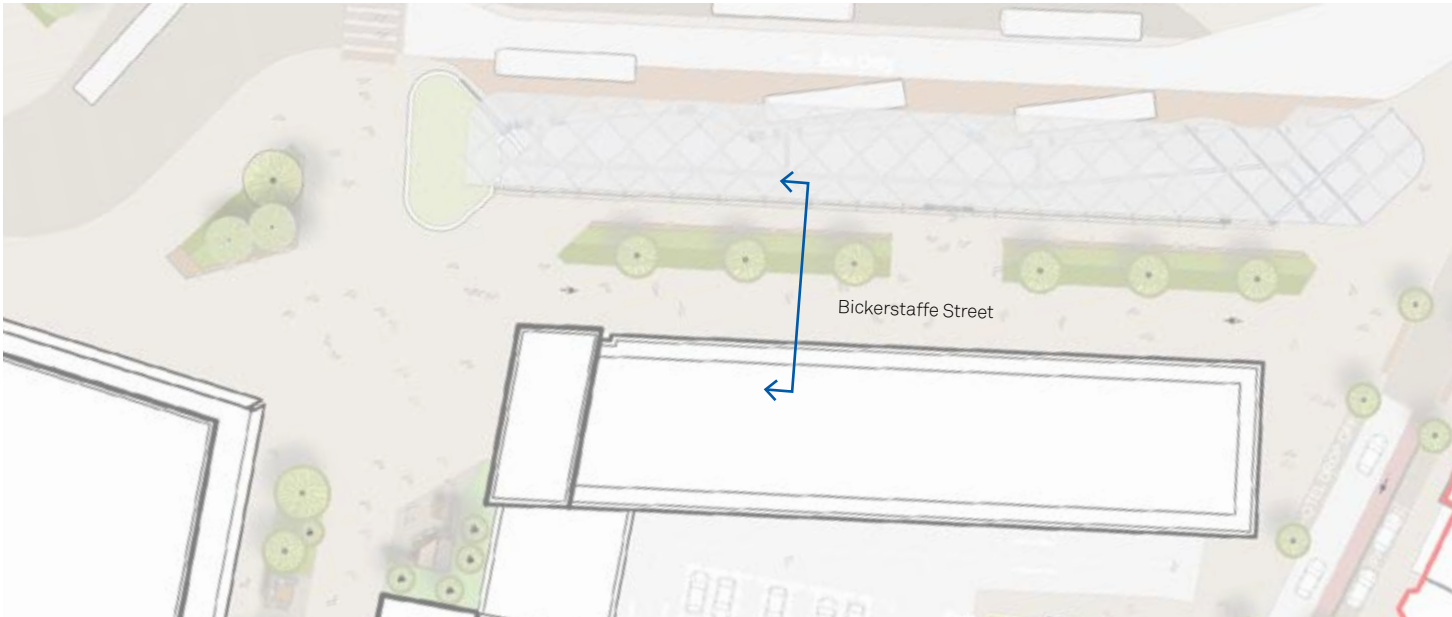
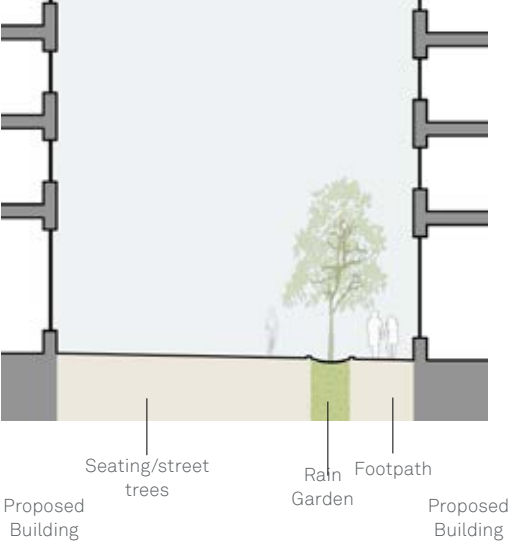
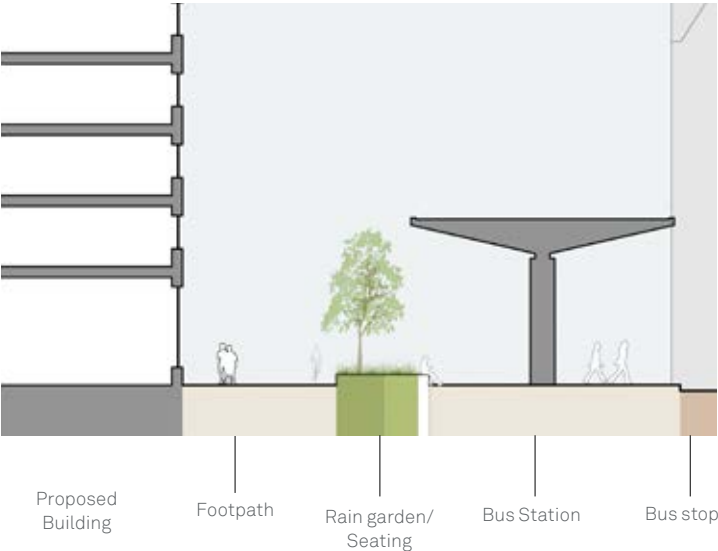
- 1. Spill out space for the Hotel at ground level.
- 2. A nice, clear wide pedestrian route creating a strong movement axis.
- 3. Generous footpaths with high quality paving materials.
- 4. Rain gardens to soften the street and enhance biodiversity.
- 5. Street furniture including seating to encourage people to dwell.

Green Spine

Designed to feel like an extension to Discovery Park, this street will feel lush and verdant and act as a green finger reaching into the town centre. The key landmark of the Church to the West and a feature within the park to the East will book end the views and draw people along. Defensible boundaries to the properties fronting onto the street will soften and add a sense of privacy to the dwellings but natural surveillance and a relationship between the external and internal forms must be retained. Opportunities for childrens play and other uses should be provided to encourage human activity and life whilst accommodating through movement.

Key Design Principles

- 1. A pedestrianised route with views maintained between the Church Spire and the Park.
- 2. Introduce defensible boundaries to townhouse units through use of railings and hedge planting to soften the street.
- 3. Generous footpaths with high quality paving materials.
- 4. Street trees to edges to create an avenue but allow views through
- 5. Explore opportunities for childrens play and other facilities to activate the street





# 8.5 The Park

## Discovery Park

The Park is a key public space providing a mix of uses for all. A place for residents and the surrounding communities to feel welcome and come together. This dynamic space responds to pressing national social and health agendas

Four overarching themes have been embedded into the design of the park:

- Heritage - The Park must have a sense of place that relates to St Helens and its local history.
- Play - Provide opportunities for children of all ages to play and interact.
- Movement - Enhance the experience of existing and proposed desire lines for pedestrians and cyclists.
- Nature - Tackle the climate emergency by providing opportunities for a variety of flora and fauna to flourish.



Place and Community

Unique  
Engaging  
Social



Health and Recreation

Exercise  
Sport  
Leisure



Accessibility  
and Connections

Safe  
Inclusive  
Legible



Climate Change  
and Biodiversity

Habitats  
Air quality  
Water retention





8.5.1 The Park  
Discovery Park Concepts

The Park engulfing the town

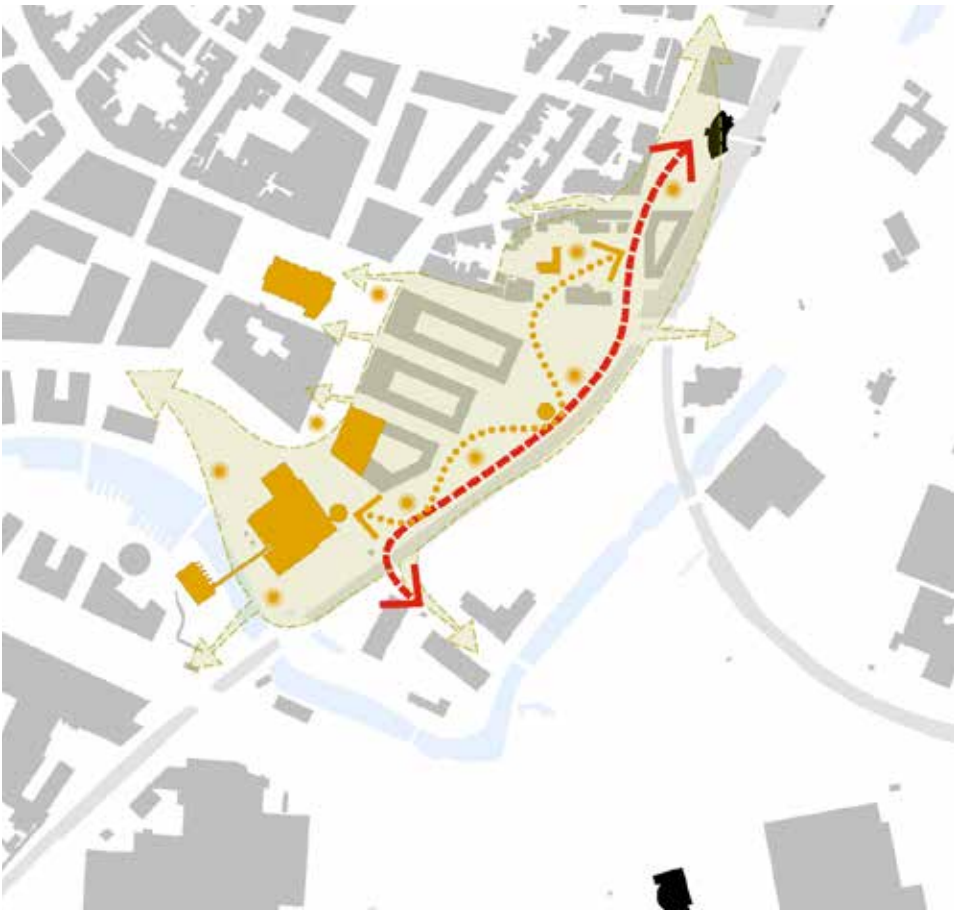
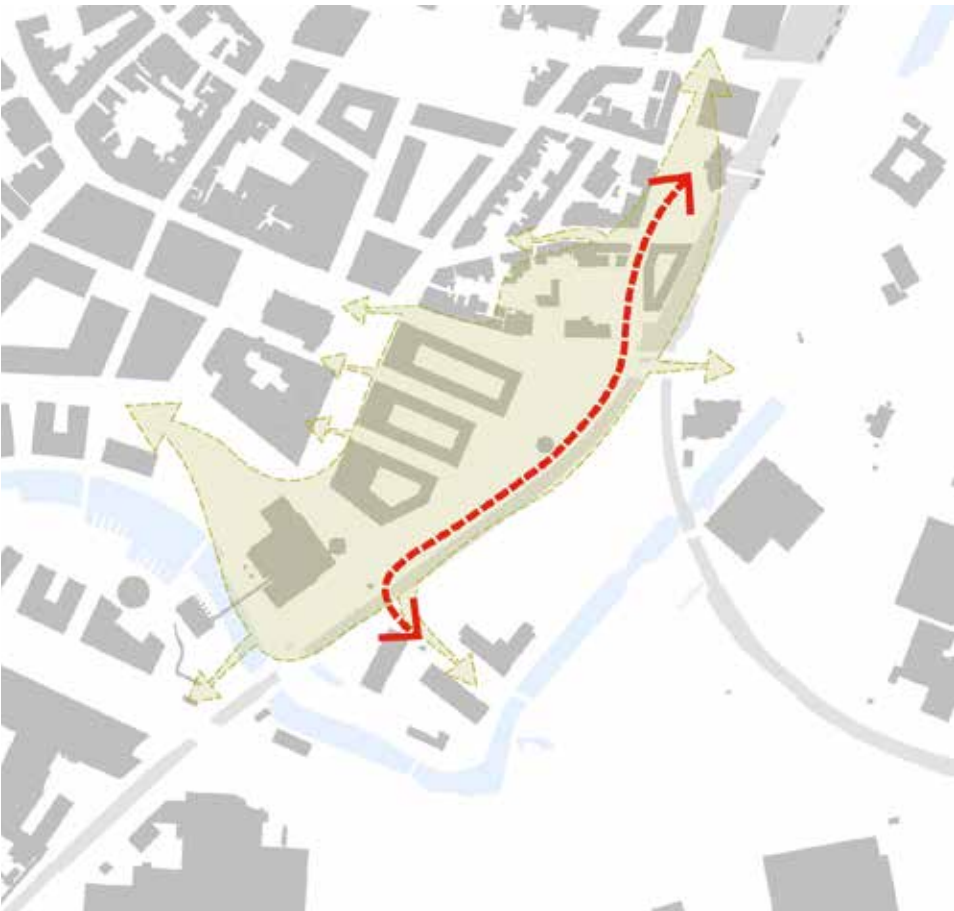
- A safe and attractive green corridor which buffers the railway tracks and engulfs the surrounding streets.
- The park will lead people down to the re-discovered canal, encouraging people to engage with the water.
- A series of play, gathering and relaxing spaces and lush planting.

Procession to the Stadium

- A strongly defined route through the new park leading to the stadium from the train station and retail core.
- Separate cycleway and wide footpath create an effective means of moving crowds on match days.
- Well connected to town centre to ensure revenue from fans for local businesses.
- Lighting, benches, flag poles, banners to provide an interesting route that builds anticipation for fans.

Cultural Heritage Trail

- The introduction of a meandering route which links some of the towns cultural assets such as the Friends Meeting House and World of Glass museum.
- Increasing recognition of these attractions and improving access.
- Celebrate heritage through 'glass themed' public art installations and interpretative elements telling stories surrounding the history of the town such as the renowned 'float process'.
- Introduction of wayfinding signage at the station directing people through the discovery park towards the museum.





8.5.2 The Park  
Discovery Park

This new green space in the heart of the town centre will become a significant asset for the residents, visitors and workers of St Helens. Access to high quality green spaces within urban environments provides a number of positive benefits for social, economic and the global climate emergency by increasing the quantum and variety of flora and fauna within what is currently a relatively hard environment. The Park will offer a number of ways for people to escape the hustle and bustle of urban life and find tranquillity through connecting with nature, a way to increase dwell time and encourage people into the town and spend more time. Biodiversity will be increased by creating opportunities for a range of habitats, capturing and storing stormwater runoff.

The Park will include opportunities for social interaction at a variety of levels, with ways to exercise, relax and play. A key pedestrian and cycle movement route through the park will be a strategic link between the train station and the World of Glass and Stadium. There is the potential to include a heritage trail through the Park with artwork celebrating the towns rich history. Landmark features within the park aligned with key views and desire lines will link the Park with existing historic assets such as the Church and Quaker House. The softening of adjacent streets can act like green fingers emanating outwards and extending the boundary of the Park.

Key Design Principles

- 1. Improve wayfinding and emphasise the park as a gateway to the World of Glass and the stadium.
- 2. Make the street an extension of the park with trees and planting and better incorporate Friend's Garden.
- 3. Shared space plaza - reducing the impact of the road to allow leisure spill out space integrated into the park.
- 4. Integration of public art to reveal identity and sense of place associated with glass making heritage.
- 5. Residential interface incorporating gardens and play spaces.



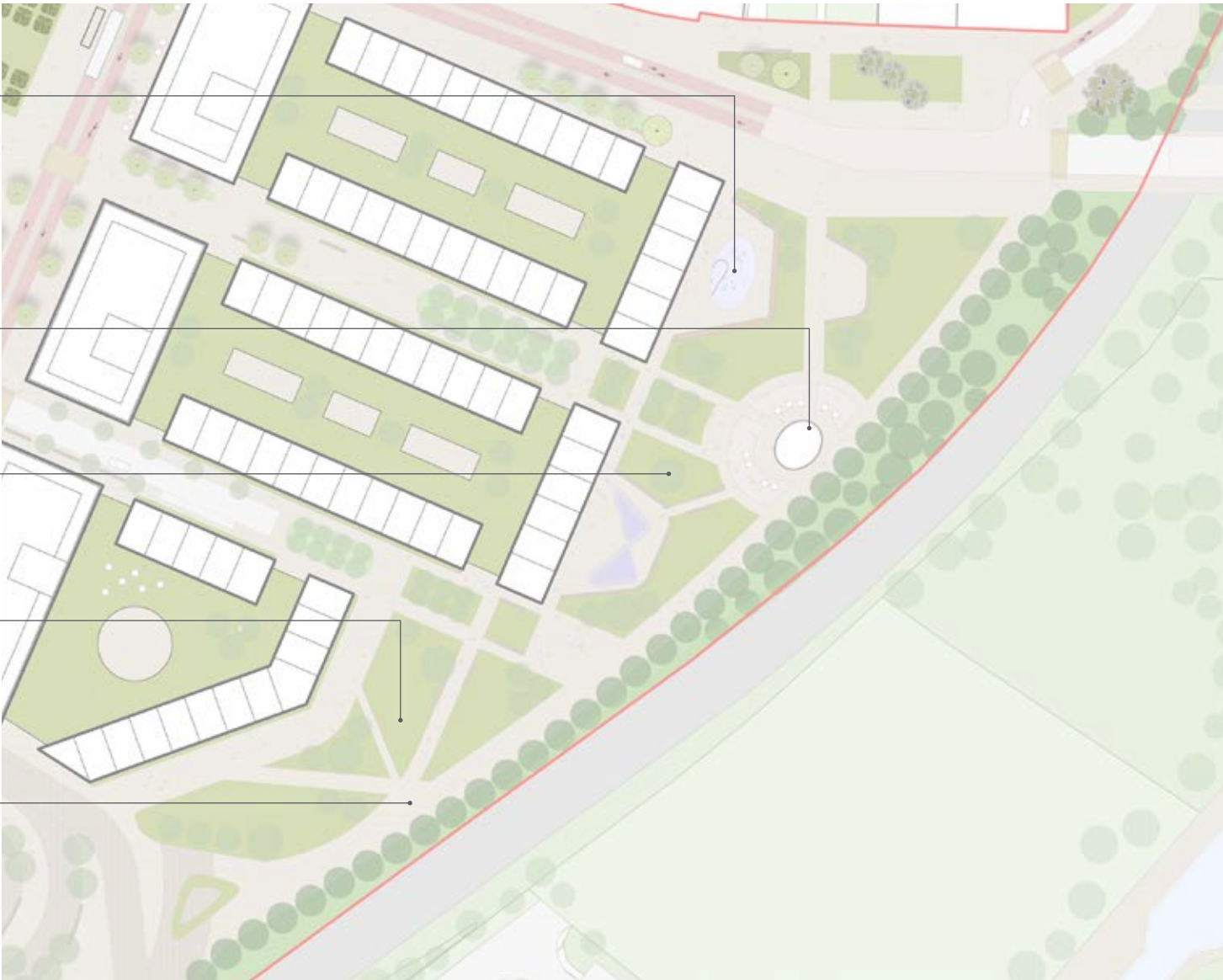
**Play**  
A mix of childrens play equipment and games facilities that encourage people of all ages to engage with each other and the external environment

**The Pavilion**  
A kiosk / building structure that aligns with the church and provides an opportunity for people to meet and have facilities that encourage dwell time within the park

**The Lawn**  
Areas of open lawn for sport and recreation. A safe, overlooked and accessible space for games, picnics and general relaxation

**The Meadows**  
Areas of space devoted to encouraging a range of planting rich in biodiversity and habitat creation with opportunities for retention rain gardens and swathes of meadow / marginal planting

**The Procession**  
A strong processional footpath route linking the train station with World of Glass and the Stadium. Opportunities for artwork and heritage features celebrating the Town's rich history along the route





## 8.5.3 The Park Artist Impression of Discovery Park





## 9.0 Plot Briefs




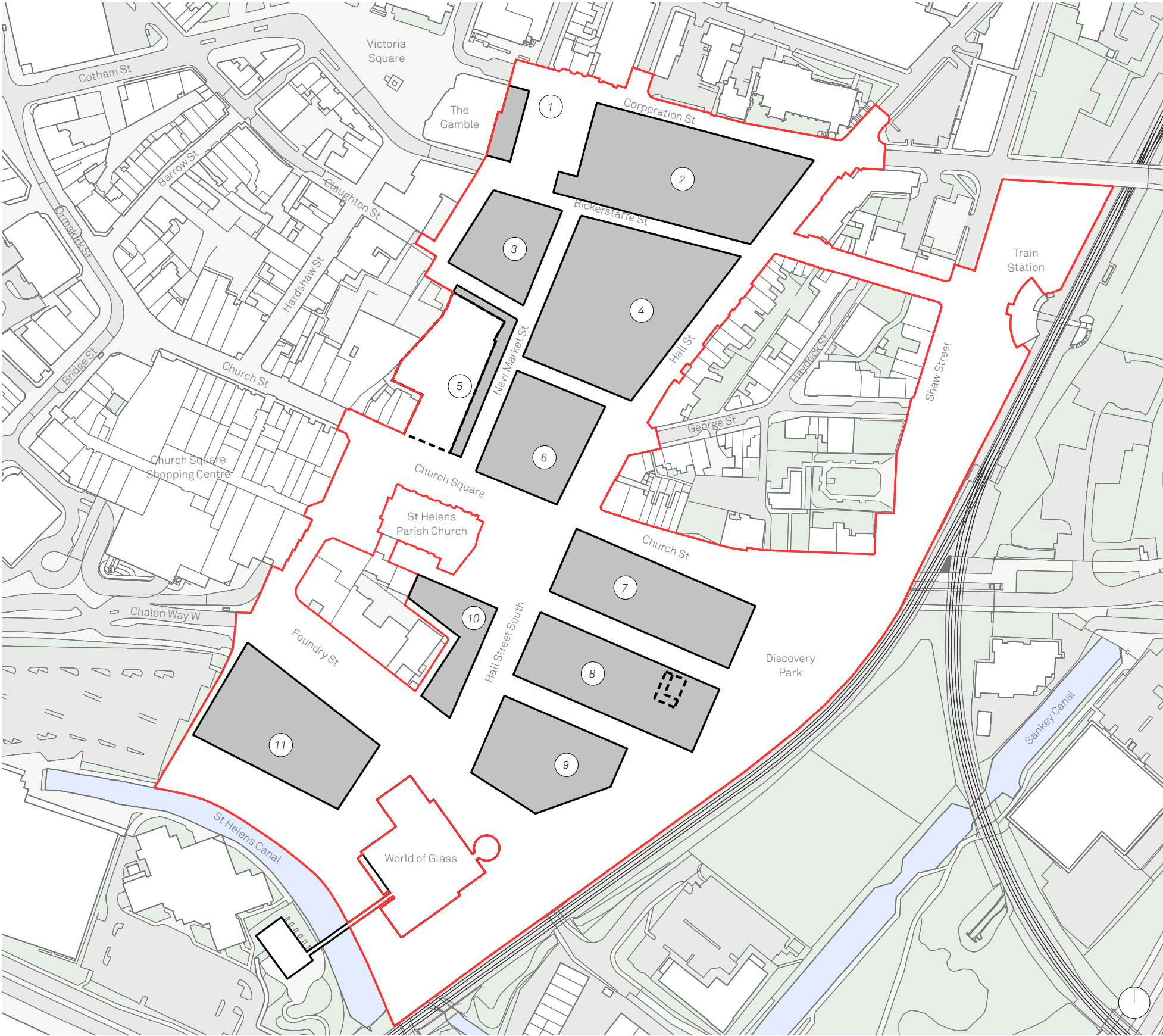
9.1 Development Plots  
Parameters Plan

Plots

- 1 Extension to Gamble Institute
- 2 Bus Station
- 3 Commercial Office,
- 4 Mixed use development, retail, hotel and residential
- 5 Retail
- 6 Market Hall
- 7 Retail and residential
- 8 Retail and residential
- 9 Retail and residential
- 10 Retail and office
- 11 Residential or commercial office

Key

 Application Boundary





# 9.2 Development Plot 1 Gamble Extension

68

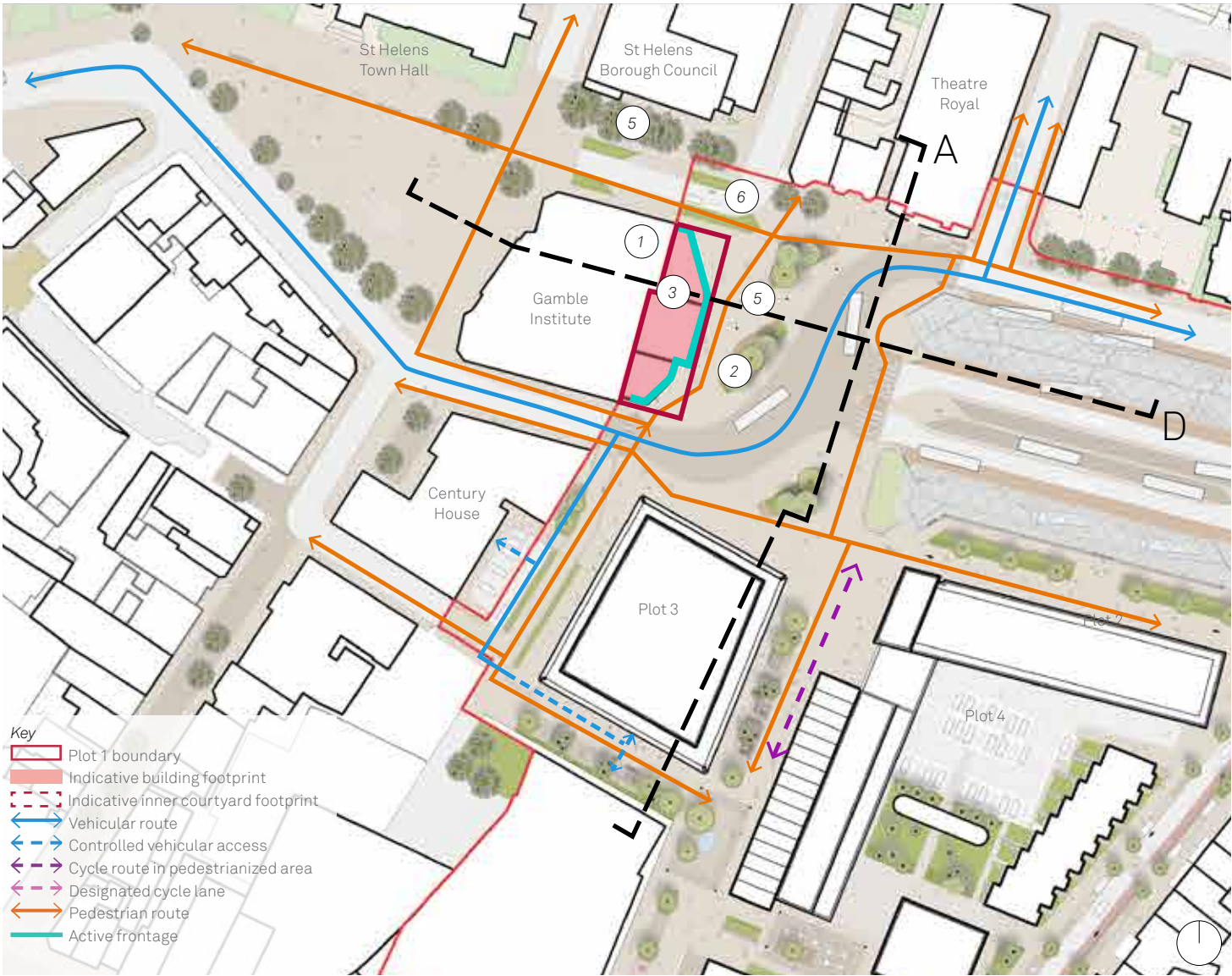
Plot 1 is the rear extension to the Gamble Institute giving the building an active frontage to Bickerstaffe Square and make the Gamble fully accessible. The extension forms the western edge of Bickerstaffe Square creating a new area of public open space between Plot 3 (office) to the south, Plot 2 (bus station) to the east and St Helens Theatre Royal to the north. Opening up Bickerstaffe Square is a key part of the masterplan strategy to improve the north south permeability as it will link New Market Street with the residential area to the north.

Plot 1 is expected to be a cafe, shop or community/ learning use similar to the Gamble Institute. The development height ranging from 1 to 5 storeys.

Plot 1 will Deliver:  
One or a mixture of the following uses, a commercial business or service [Class E (a-f)], a learning and non-residential institution for example museum/ library/ exhibition space [F1 (b-e)], a local community use such as a meeting place [F2 (b)] or a drinking establishment/ ancillary bus station facilities (Sui Generis).  
  
Level access/ lift giving access to the ground floor of the Gamble and accessible WC provision if required.

**Brief Objectives**  
To give the Gamble building a front to Bickerstaffe Square that will make the building fully accessible. The brief needs to be developed with the council in line with the refurbishment and use of the building and its relationship with Bickerstaffe Square.  
  
The new building should meet St Helens Council Sustainability Strategy.  
  
**Layout**  
1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.  
2. The primary frontage must face onto Bickerstaffe Square and Bickerstaffe Street.

**Access and Movement**  
3. Main entrances on the primary elevation.  
4. Pedestrian routes are expected to circulate round the plot and Gamble Building.  
5. Cycle parking to be accommodate along Bickerstaffe Square and Corporation Street.  
6. Service access likely to be form Corporation Street opposite Wesley House.



Plot 1 Illustrative Masterplan



22-Handyside Street, Coffey Architects: Contemporary take on a traditional form, animated at night, welcoming.



9.2 Development Plot 1  
Gamble Extension

Massing

- 7. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the provision for a pitched roof.
- 8. The existing elevation of the Gamble will likely effect the proposed massing once an accurate survey is carried out so that awkward junctions are not created with the existing openings and details.



Ideas Store, Adjaye Associates, London: Timber structure, colour, active street.



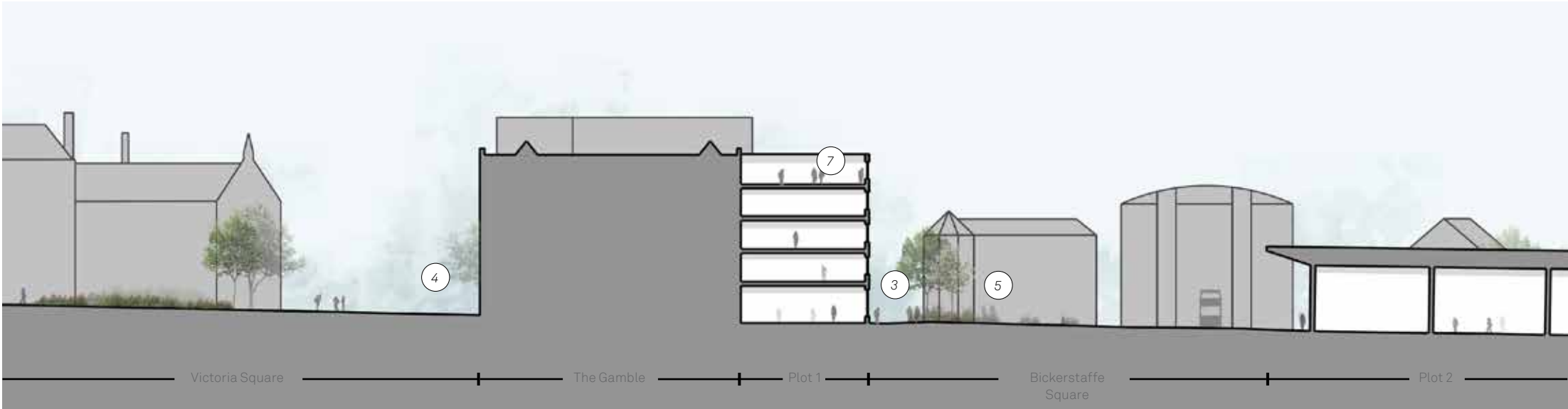
Norwich College, Coffey Architects: facade depth and animation at night.



CGI of an Office Extension, Manchester, JMA: Extension to a listed warehouse, complementary material palate and use of planting.



Section A, illustrative section showing maximum development heights



Section D, illustrative section showing maximum development heights



Lego House, Billund: Public space engaging with and extending the building.



# 9.3 Development Plot 2 Bus Station

70

The new bus station is located at the heart of the civic and heritage quarter, it forms a transport hub with the train station. This is achieved by new high quality public realm linking them with clear way-finding welcoming you to the town centre with potential cycle storage and possible retail offer. Plot 2 is expected to be single storey in the main but may have some administrative accommodation at first floor.

**Plot 2 will Deliver:**  
A bus station and travel centre (Sui Generis bus station) with possible retail unit [E (a-f)] approximately 150m2.

**The Brief**  
Part of a joined up travel hub made up of the bus station and train station. It will be connected by new improved landscape, the bus station should have a direct visual connection down Bickerstaffe Street from the Train Station. The view of the bus station should be considered from above as it will be overlooked by the hotel/ residential use (Plot 4) and the office (Plot3).

The bus station consist of the following:

- Minimum of 9 operational bus stands
- Minimum of 8 layover stands
- 2 entrance & exit points for buses to be provided at opposite ends of the facility to ensure access can be maintained in the event of a road closure on the highway network.
- Bus manoeuvres required to use each stand in the facility need to be acceptable to the bus operators in addition to being

demonstrated as feasible using Autotrack.

- Covered bus stands.
- Driver Facility
- Travel Centre
- Retail Unit

The new building should meet St Helens Council Sustainability Strategy.

**Layout**

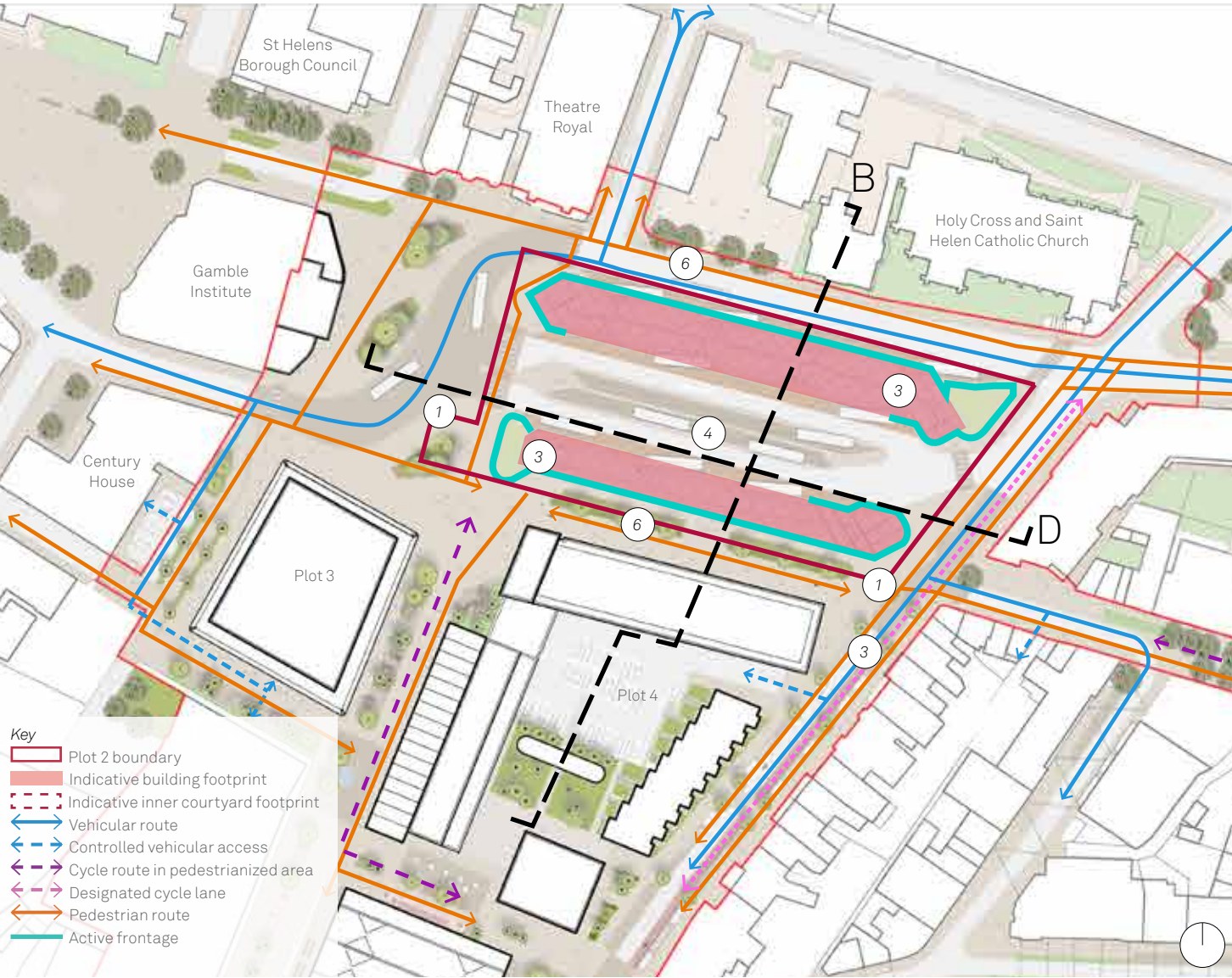
1. The parameter plot boundary sets out the maximum building line allowance for the development, this has been set to provide flexibility in the layout whilst maintaining the new public open space in front of the Theatre Royal.
2. It is not expected to deliver up to the plot boundary but the southern edge of the bus station should hold Bickerstaffe Street.
3. The bus station should be considered in the round but the

primary frontage is on the southern bus shelter addressing Bickerstaffe Street, extending round the elevation to address both the train station and Gamble approaches.

4. The design should consider the Corporation Street elevation to also be a primary elevation.

**Access and Movement**

5. Main entrances on the primary elevations to maintain ease of movement throughout the bus station.
6. Pedestrian routes are expected to circulate round and through the plot, ease of movement, safety and separation from the buses is key.
7. Cycle lane and parking to be accommodate within Bickerstaffe Square.



Plot 2 Illustrative Masterplan



View looking west along Bickerstaffe Street, Plot 4 on the right - Artists CGI



9.3 Development Plot 2  
Bus Station

Massing

- 8. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum.
- 9. The sections show are for the maximum building height, whilst the canopies need to accommodate a double decker bus their scale needs to be right along Bickerstaffe Street. To create a successful street where people want to dwell.



Cepezed bus station, Tilburg, Lucas Van Der Wee, integrated landscape and rain gardens.



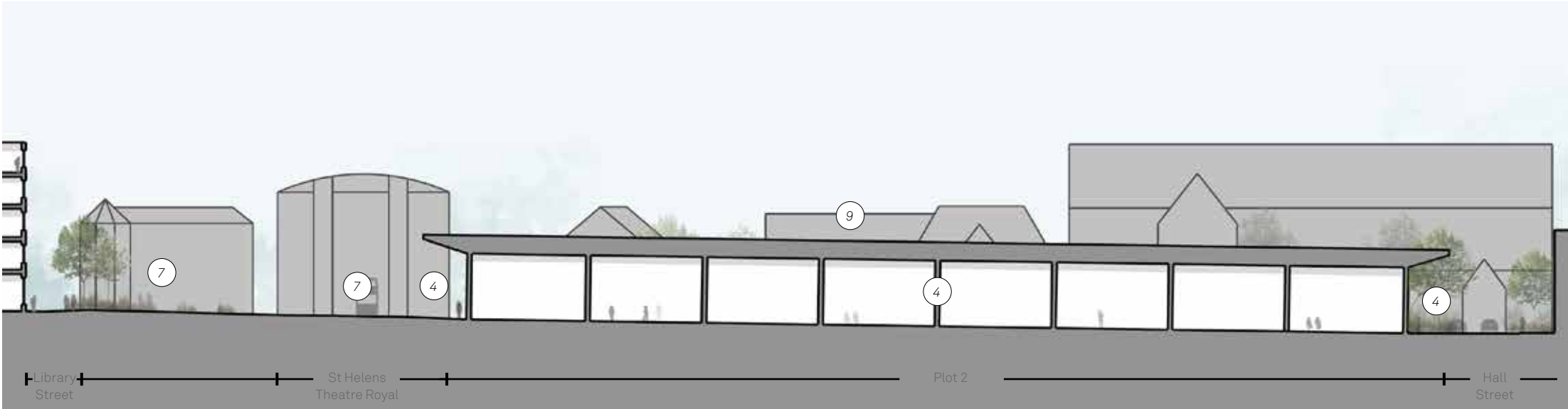
Stoke Bus Station, Grimshaw Architects.



Stoke Bus Station, Grimshaw Architects: Open, light, integration of street landscape and bus station canopy.



Section B, illustrative section showing maximum development heights



Section D, illustrative section showing maximum development heights



Artists impression looking west down Bickerstaffe Street



# 9.4 Development Plot 3 Commercial Office

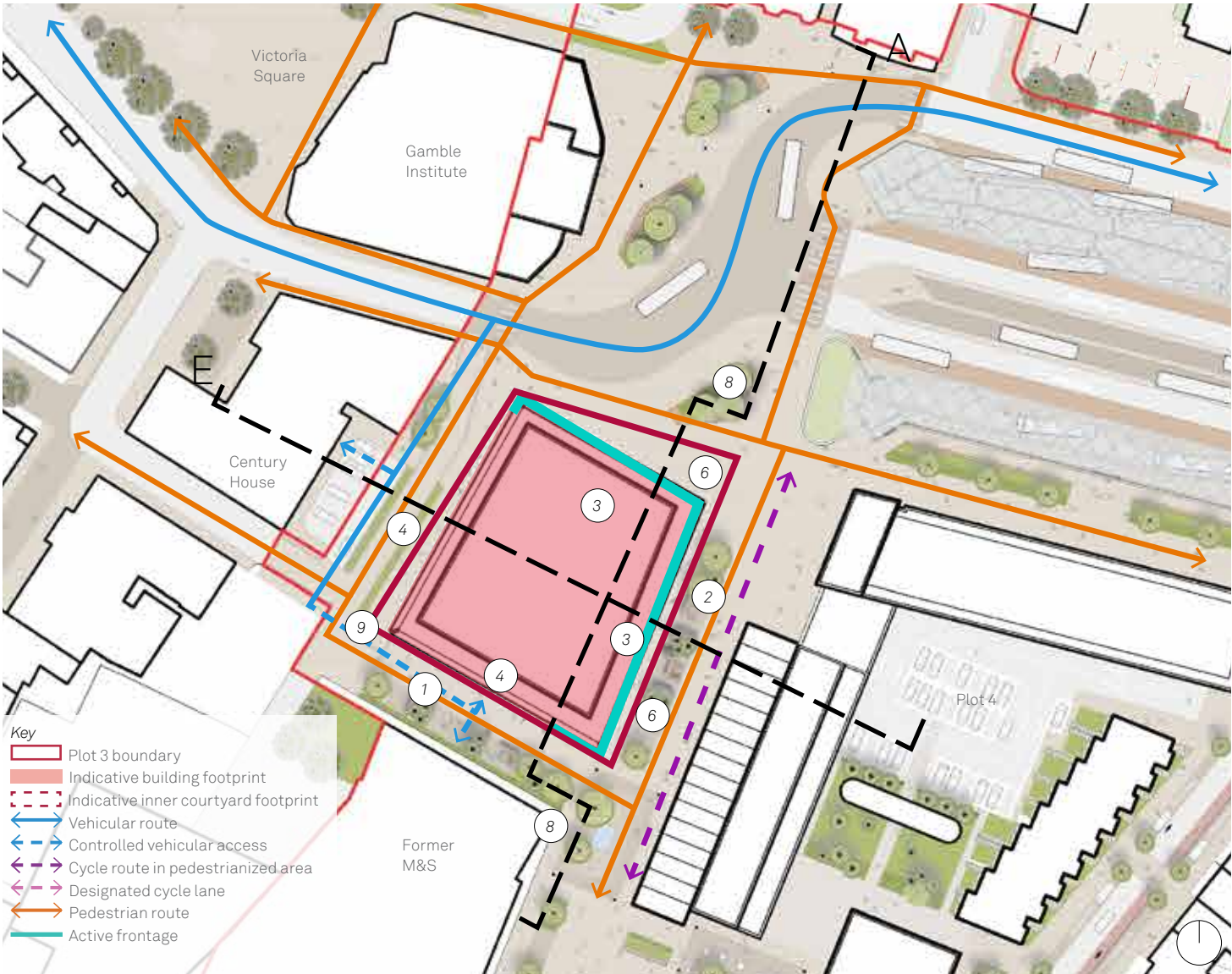
72

Located south of the Gamble Institute and opposite the Theatre Royal Plot 3 forms the south west corner of the new Bickerstaffe Square and bus station (transport hub). The plot forms a key active frontage to the Bickerstaffe Square and New Market Street, the introduction of New Market Street was a key masterplan move to link Church Square with the joined up transport hub. Plot 3 is expected to be a commercial office with food and beverage offer on part of the ground floor, the development height ranging from ground plus 3 to 4 storeys.

**Plot 3 will Deliver:**  
Commercial office, Use class E(g i-ii)  
Typical Floor GIA 1666m2 & NET 1474sqm  
Ground floor unit within the office demise with a food and beverage offer 200m2, Use Class E(b)

- Brief Objectives**
- A highly sustainable and carbon efficient building. Designed to meet UKGBC NZC framework requirements.
  - Operational energy calculated using BBP (Better Buildings Partnership) design performance standard.
  - Designed to enhance occupants health, well-being and productivity, embracing the WELL Building standard principles.
  - Be innovative in the selection of material and construction technologies, which are high quality, durable and low maintenance.
  - Certified using UK NABERS Scheme.
  - Designed to meet the UKGBC 2035 landlord energy intensity target of 35kWh/m2 and have a pathway to meeting the tenant intensity target of 35kWh/m2
  - Design to meet the GLA Benchmark for upfront embodied carbon and achieving a performance of 750kgCO2/m2, with an

- aspiration to achieve 500kgCO2/m2
  - Promote biophilia design
- Layout**
1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
  2. The development is expected to hold the street but doesnt need to be delivered up to the plot boundary. It should maximise the interaction with the street and form a consistent building line with the other plots.
  3. The primary frontage must face onto Bickerstaffe Square and New Market Street.
  4. The secondary frontage onto Claughton Street and opposite Century House should be as active as possible to contribute to the street scene.
5. Floor plate depths should be designed to maximise natural ventilation, maximise access to daylight for occupants and circadian lighting. Promoting biophilic design.
- Access and Movement**
6. Main entrances on the primary elevations.
  7. Pedestrian routes are expected to circulate round the plot perimeter, the majority of the perimeter of the building are pedestrianised or streets vehicular controls on them.
  8. Cycle lane and parking to be accommodate along Bickerstaffe Street and New Market Street.
  9. Service access to be from Claughton Street or opposite Century House, providing access to bin stores, cycle store and deliveries.



Plot 3 Illustrative Masterplan



View looking south down New Market Street, Plot 3 on the right and Plot 4 on the left - Artists CGI



9.4 Development Plot 3  
Commercial Office

**Massing**  
10. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the lift overrun and any roof plant.



Trafford Town Hall, 5plus Architects: Reception space providing play space for children, simple robust materials.



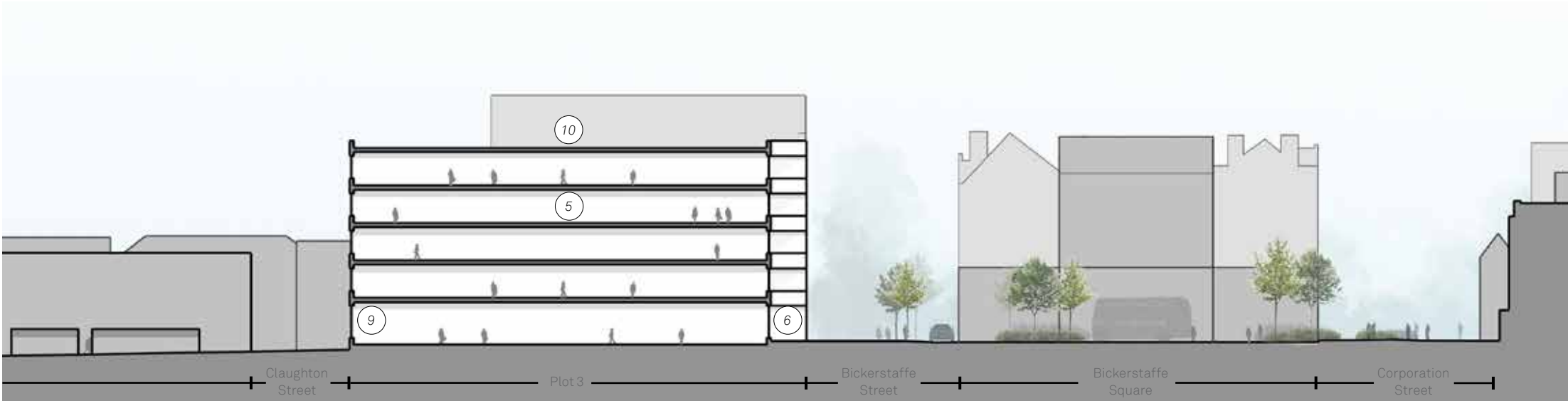
Office, Kuehn Malvezzi: Source bricks locally, regular bays both horizontally and vertically, in harmony with local context, simple construction.



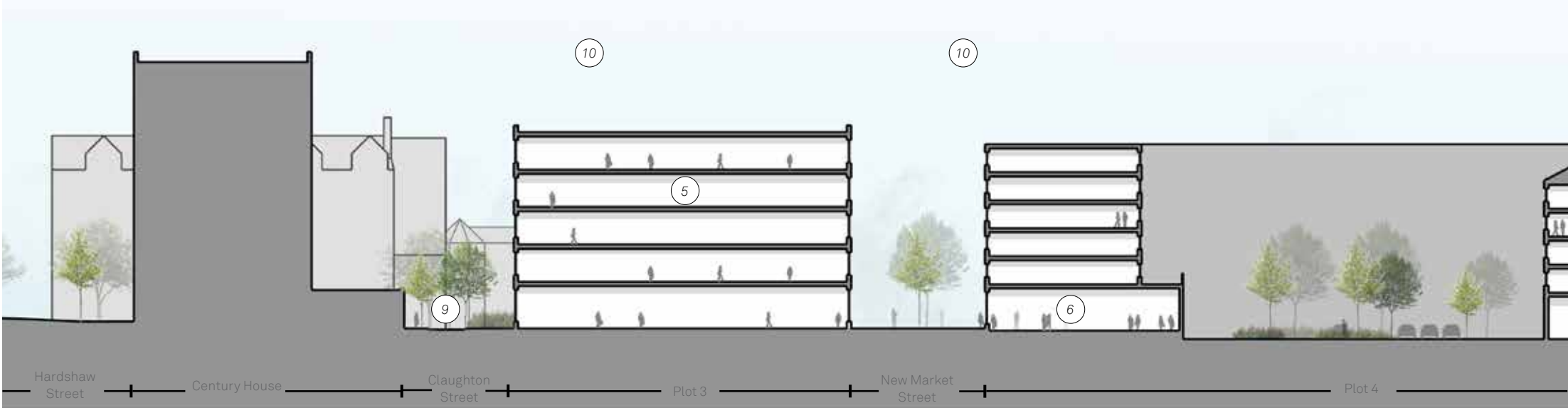
Sheppard Robson, Wardour Street: Modular Elements incorporating solar shading.

**Materiality**  
A simple palate of materials is anticipated with the predominant building material expected to be brick to reflect the wider town centre character. Brick choice will have to be carefully chosen to complement the surrounding buildings such as the Gamble Institute, Town Hall and Corporation Buildings.

The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Section A, illustrative section showing maximum development heights



Section E, illustrative section showing maximum development heights



Artists impression looking west down Bickerstaffe Street



# 9.5 Development Plot 4 Mixed Use

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Plots 4 is a mixed use plot indicatively made up of 3 blocks, a hotel or residential, an apartment block with retail below and town houses/ maisonettes. The hotel/ residential block is located on the south eastern edge of the new bus station (Plot 2) 150m to the west of the train station. The eastern side of the hotel/ residential block forming one half of the gateway to Hall Street, a residential street in scale and character. Its here the town houses/ maisonettes could be located, complementing the existing eastern side of the street made up of 2 to 3 storey brick houses.

The apartment block with retail below is sited just south of the bus station forming its western edge and gateway to New Market Street, linking down to Church Square and St Helens church tower. Improving natural way-finding and drawing the footfall direct into the heart of the town centre.

Plot 4 will Deliver:

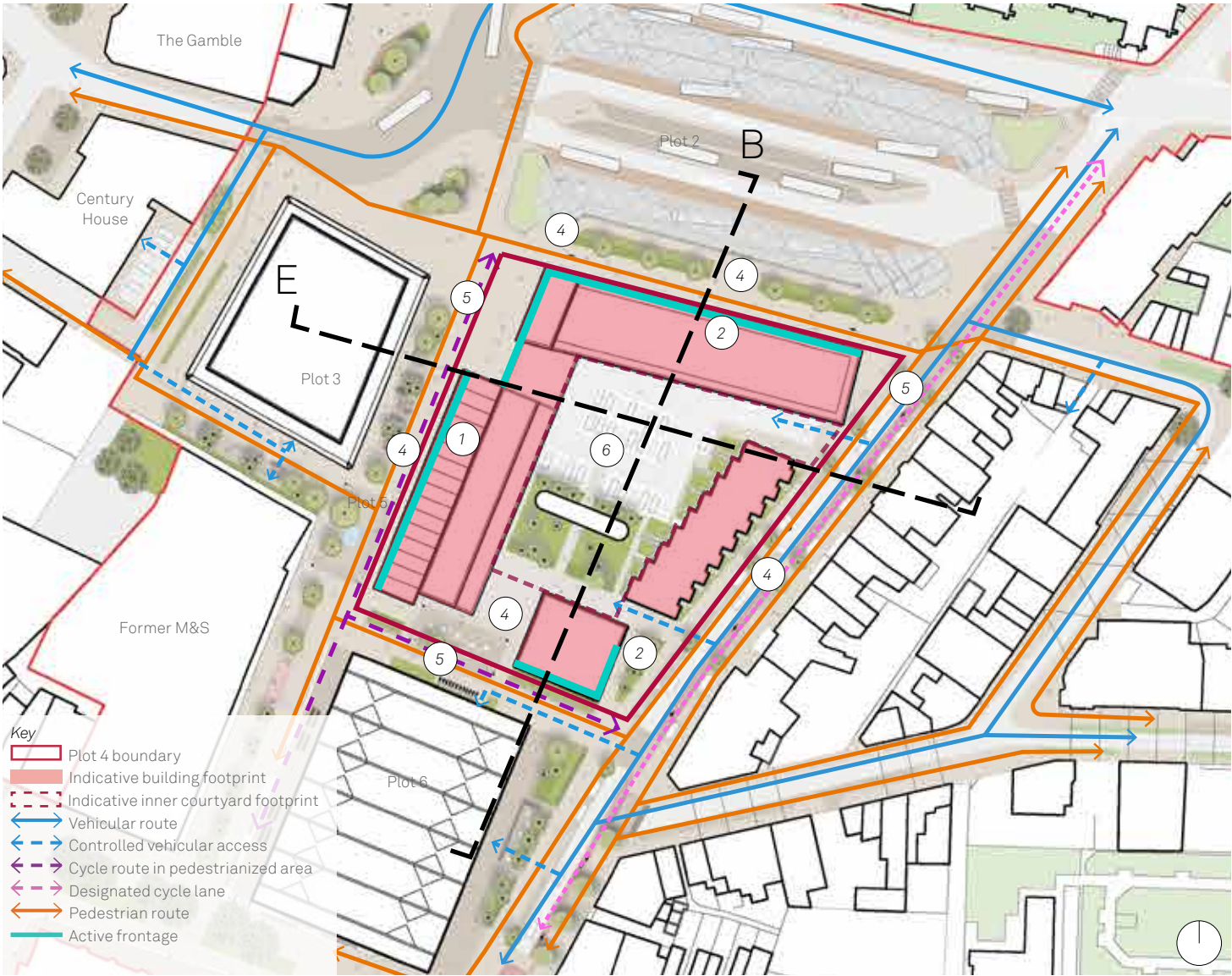
- Options A  
Hotel with ground floor restaurant. Use class C1 and Sui Generis (Drinking Establishment, ancillary bus station facilities)  
Hotel, max 155 bed  
Option B  
Apartments with retail below, use class C3, E (a-f), Sui Generis (Drinking establishments, ancillary bus station facilities)  
Apartments  
Parking (number shared with other apartments)  
Plus  
Apartments with retail below, use class C3, E (a-f), Sui Generis (Drinking establishments)  
49 Apartments  
24 parking spaces  
Retail  
Town Houses

Town Houses x16 max  
16 Parking Spaces in rear courtyard

**Brief Objectives**  
A homogeneous plot potentially made up of the 4 blocks with active frontages. The hotel restaurant and foyer at ground floor activating the edge of Bickerstaffe Square and approach from the station. Retail at ground floor on Bickerstaffe Square, down New Market Street and round past the market, made up of smaller local retailers. The elevation animated with depth providing the external amenity space for the occupants and passive surveillance of the streets.

- Sustainability Strategy**
- Maximize reduction in carbon at the point of completion and during operation.
  - Enhanced Biodiversity -Delivering a net gain in biodiversity, whilst providing high quality green spaces
  - Circular Economy - Eliminating waste and improving resource efficiency through circularity.
  - Health and Wellbeing - Delivering high quality spaces where people can live, work and thrive while positively impacting their physical and mental health.
  - Wider Sustainability - Capturing wider sustainability issues such as Water, Climate Adaptation, Transport & Certification
  - Residential units to aim for Passivhaus Standards.

All residential buildings should follow and meet the targets set: ECF



Plot 4 Illustrative Masterplan



View looking west along Bickerstaffe Street, Plot 4 on the left - Artists CGI



9.5 Development Plot 4  
Mix Use

- Part M (90% Visitable & 10% adaptable)Achieve Building for Life Gold Standard.
- Follow the Lifetime Homes 16 Design Criteria.
- Nationally Described Space Standards.
- All residential units to be constructed using non-combustible materials.

Layout

1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
2. The development is expected to hold the street but doesnt need to be delivered up to the plot boundary. It should maximise the interaction with the street.
3. The plot is in the round so every side should be considered as the primary frontage.

Access and Movement

4. Main entrances on the primary elevations.
5. Pedestrian routes are expected to circulate round the plot perimeter, the majority of the perimeter of the building are pedestrianised the exception is Hall Street.
6. Parking to be accommodated in the central courtyard accessed of Hall Street with the deliveries.



Hotel, Hampton by Hilton, Stockton on Tees



Timekeepers Square, town houses by Muse: Front doors on the streets, roof gardens and use of colour.

Massing

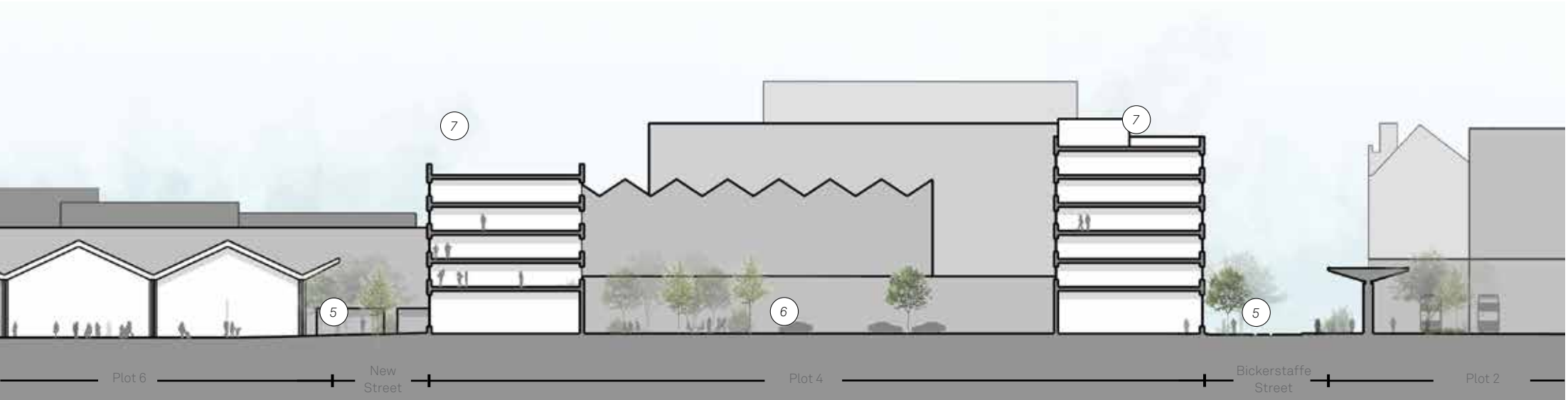
7. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the lift overrun, roof plant and pitched roofs.

Materiality

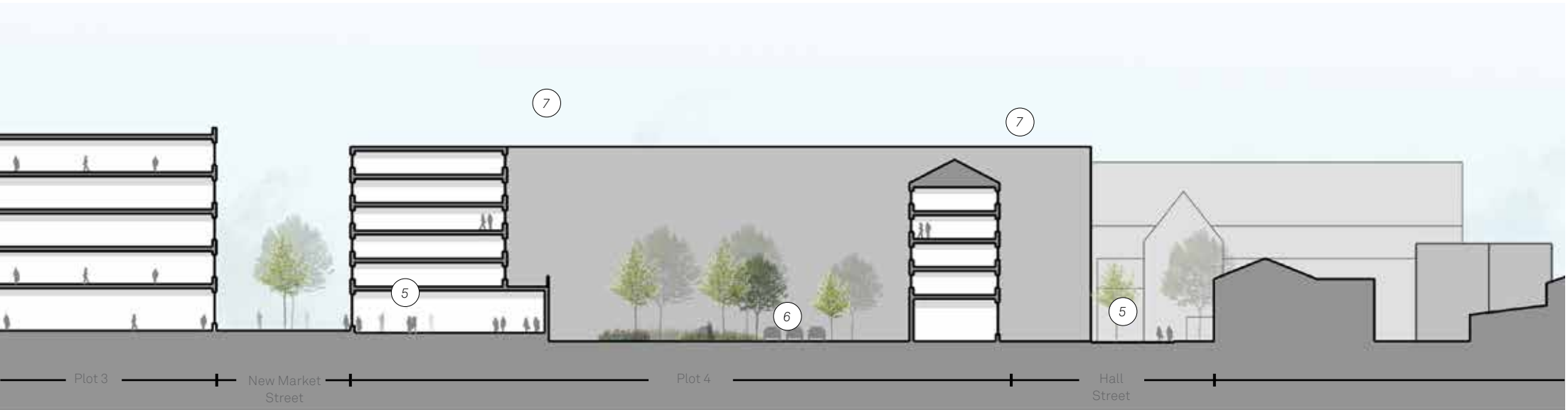
A simple palate of materials is anticipated with the predominant building material expected to be brick to reflect the wider town centre character. Brick choice will have to be carefully chosen to complement the surrounding buildings such as the Gamble Institute, Corporation Buildings and St Helens Parish Church. The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Artists impression looking south down New Market Street, Plot 4 on the left.



Section B, illustrative section showing maximum development heights



Section E, illustrative section showing maximum development heights



# 9.6 Development Plot 5

## Former M&S Wrap

76

Located on the southwest side of New Market Street opposite Plot 6 (Market Hall). Plot 5 wraps the side of the former M&S building, its southern elevation merging with the classical frontage of the former M&S forming the gateway to New Market Street with the Market Hall, directly opposite St Helens Parish Church, at the heart of the town centre.

The proposal will be 1 to 2 storeys wrap allowing access into the former M&S along its eastern elevation allowing the subdivision of this large retail unit. A single storey option would require the gable of the former M&S building to be made good and rendered, a possible location for a mural and opportunity to work with local artist.

Plot 5 will Deliver:  
Ground floor retail/ F&B offer, use class E (a-f),  
Sui Generis (drinking establishments)

**Brief Objectives**  
The new building will make good the exposed party wall of the former M&S building ensuring that full remaining elevation to Church Square is retained and supported. It will allow the former M&S building to be subdivided into smaller more suitable retail units.

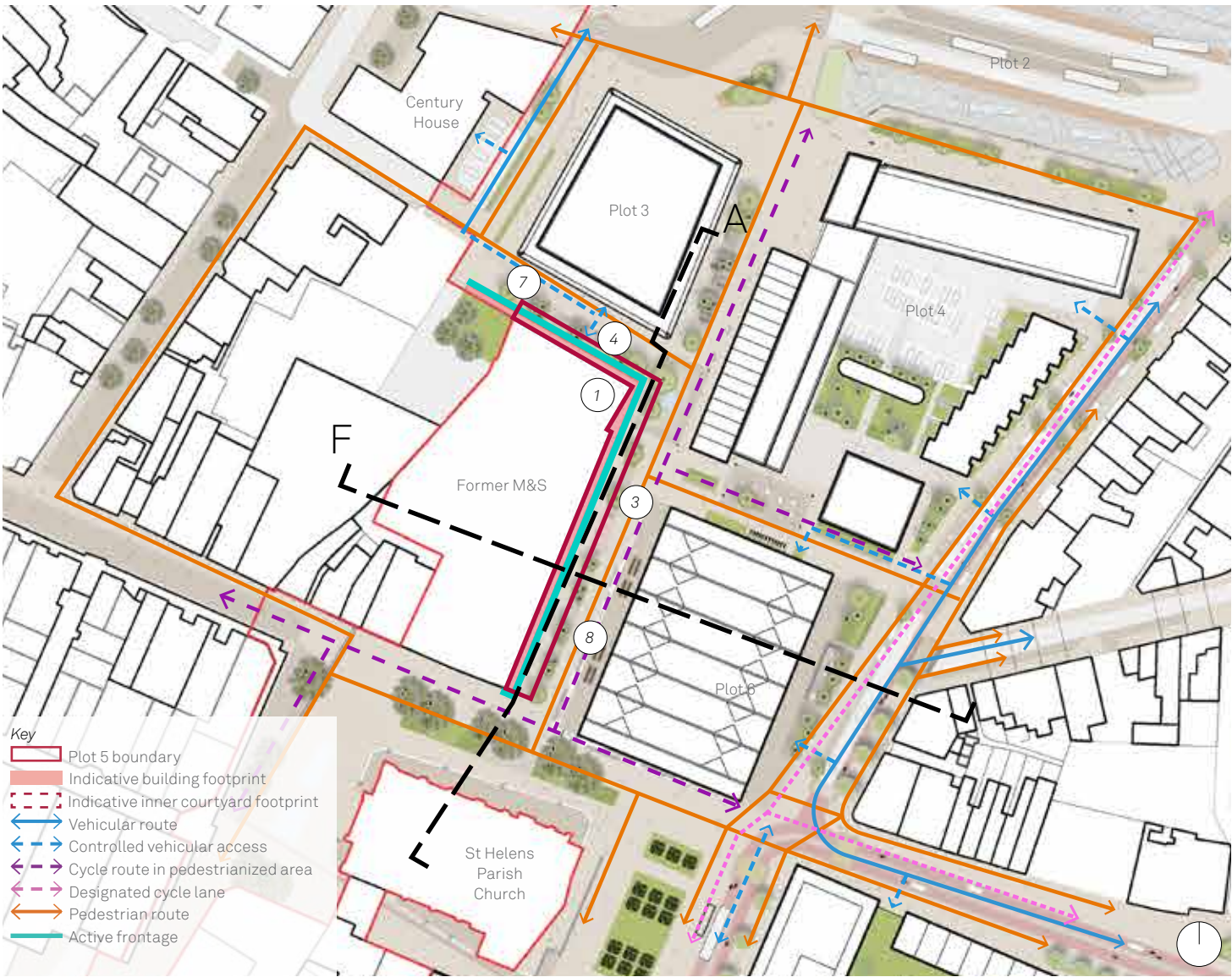
The new building should meet St Helens Council/ ECF Sustainability Strategy.

- Layout**
1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
  2. The development is expected to be delivered up to or close to the plot boundary to maximise interaction with the street and for consistent building line with the other plots.
  3. The primary frontage must face onto New Market Street.
  4. The secondary frontage onto Claughton Street and opposite Plot 3 is seen as the primary service access.

- Access and Movement**
5. Main entrances on the primary elevations for retail.
  6. Pedestrian routes are expected to circulate round 3 sides of the plot, Church Square, New Market Street and Claughton Street.

7. Service access to be from Claughton Street or opposite Plot 3, providing access to bin stores, cycle store and deliveries.
8. A zone in front of the retail units should be considered for seating and spill out.

- Massing**
9. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the lift overrun and any roof plant.
  10. The maximum height within the parameters plans allows for a pitched roof or any lift overruns.



Plot 5 Illustrative Masterplan



Back Turner Street, Manchester (CGI), JMA: Narrow block, elegant brick detailing in a historic context.



9.6 Development Plot 5  
Former M&S Wrap



Laboratory for UCL, Penoyre & Prasad Architects: Graphics and Cladding.



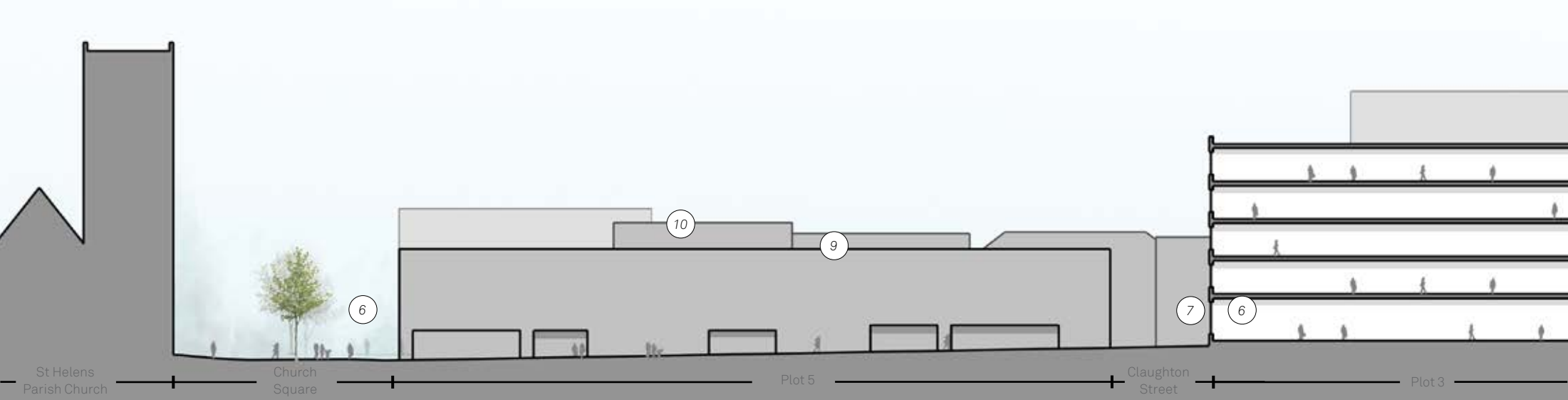
Shop Front, location and architect unknown: Graphics

**Materiality**  
A simple palette of materials is anticipated with the predominant building material expected to be brick to reflect the wider town centre character. Brick choice will have to be carefully chosen to complement the surrounding buildings such as St Helens Church, the Gamble Institute, Town Hall and Corporation Buildings.

The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Hotel, Rochdale Road, JMA: Open active frontage, simple elegant design.



Section A, illustrative section showing maximum development heights



Section F, illustrative section showing maximum development heights



Complimentary brick palate, attention to detail.



# 9.7 Development Plot 6 Market Hall

78

Plot 6 the Market Hall is located at the heart of the town centre. It hold a prominent position, fronting Church Square, one half of the gateway to New Market Street, it holds the corner of Church Street and Hall Street the eastern approach to the town centre across the rail line. Its location close to the transport hub will generate a large amount of its own footfall through the town centre.

**Plot 6 will deliver:**  
Market Hall, use class E (a) & E (b), Sui Generis (drinking establishments)  
Stalls of varying size of which 10 should be F&B.  
Communal Seating for approximately 200 diners.  
Dedicated washing up area and storage facilities.

**The Brief**  
The market will be the home of independent retail, a food and beverage hub and an important business incubator for local entrepreneurs. Accommodating a mix of temporary and permanent market stalls. It should have a modern look and feel with a light and airy hall. The layout internally should be low rise with a distinct seating and street food zone with mixed seating offer of communal and small tables. .

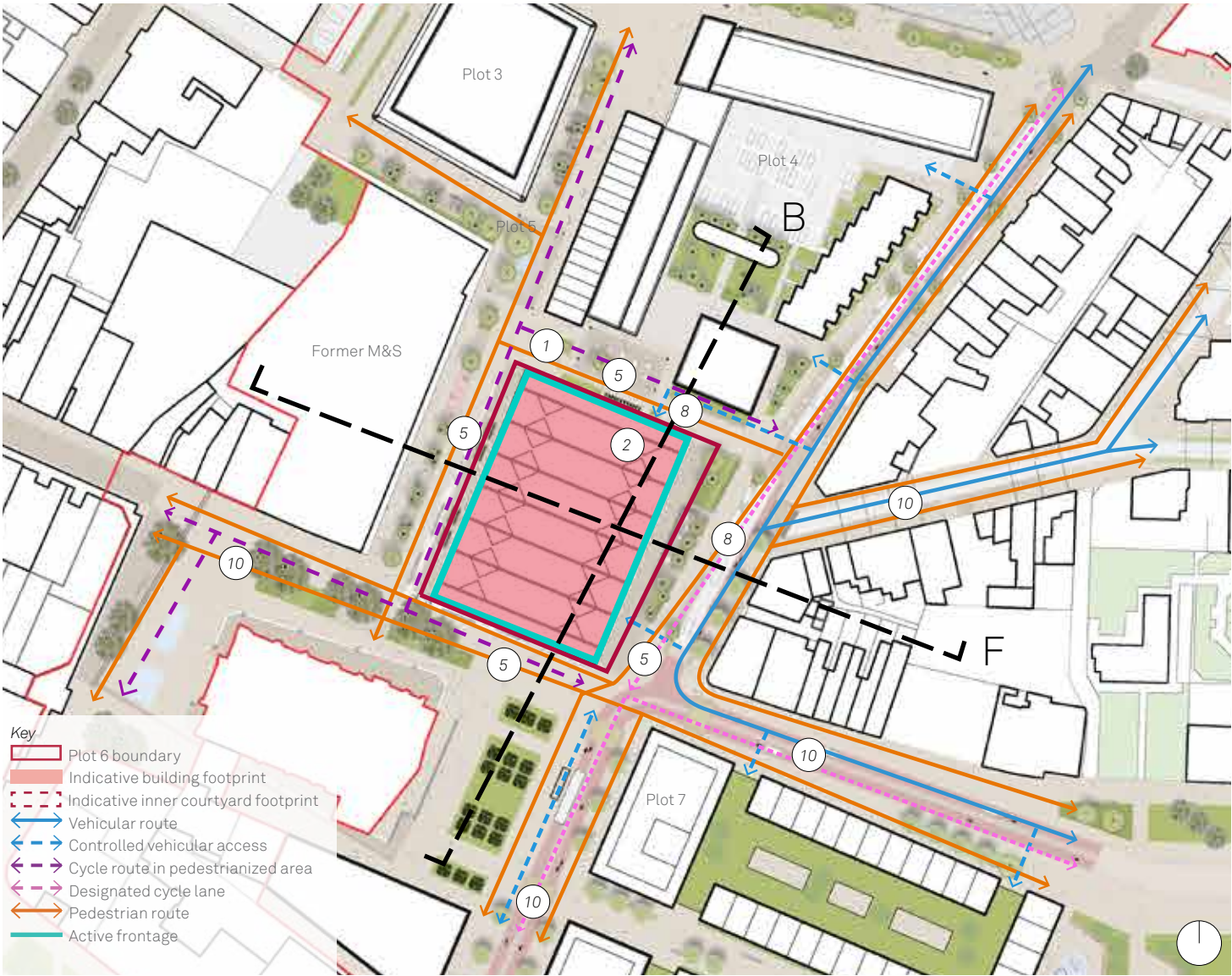
In the evenings it will transform into a vibrant food and beverage (F&B) and social destination with the ability to put on events. Integrated with the existing outdoor market serving complementary produce and improving the whole shopping experience.

The layout should encourage circulation with positive sight-lines. The market traders should be zoned by offer type, increased circulation areas will create a sense of spaciousness with communal seating areas to encourage social interaction. Market office, back of house facilities and WCs conveniently located.

- Layout**
1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
  2. The development is expected to hold the street but doesnt need to be delivered up to the plot boundary. It should maximise the interaction with the street and form a consistent building line with the other plots.
  3. The plot is in the round so every side should be considered as the

- primary frontage.
4. Greening of the extensive roof of the market should be explored, increasing biodiversity and providing a visual amenity for the surrounding plots overlooking the roof.

- Access and Movement**
5. Main entrances on the primary elevations.
  6. Pedestrian routes are expected to circulate round the plot perimeter, the majority of the perimeter of the building are pedestrianised.
  7. Cycle parking to be accommodate on Church Square and north of the market hall.
  8. Service access will primarily be from Hall Street and the north side of the hall, quieter pedestrian streets, for market traders, bins and deliveries.



Plot 6 Illustrative Masterplan



Artists CGI, looking north across Church Square to plot 6.



9.7 Development Plot 6  
Market Hall

Massing

- 9. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The height of the main roof should be high enough to accommodate a future mezzanine level should the need arise, this could be for an F&B offer or administrative functions.
- 10. Detailed design should consider approach views from the surrounding streets, the unique form of the market hall will act as a key way-finding marker within the town centre.



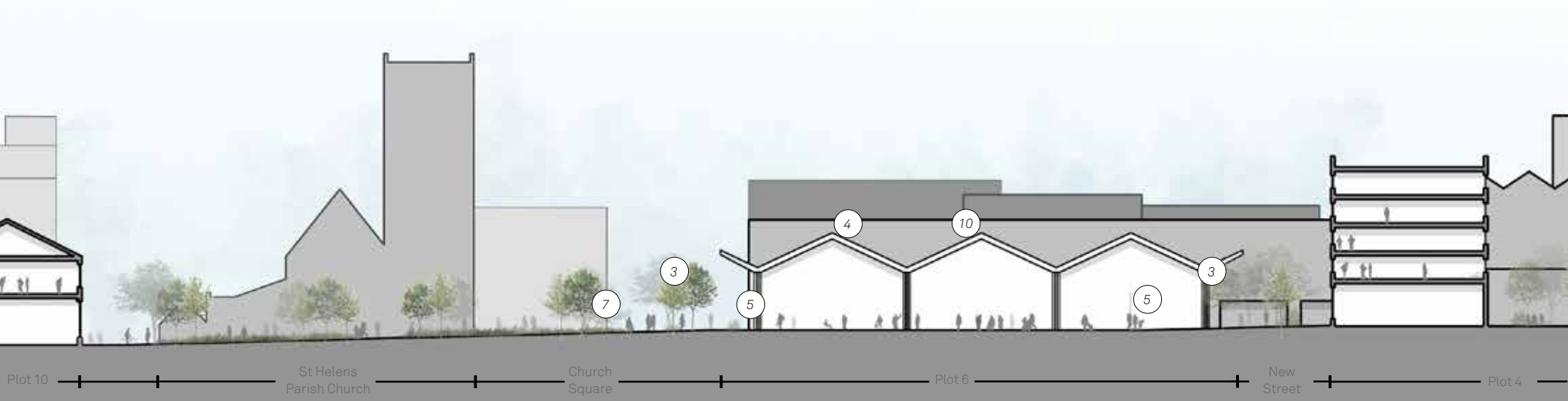
Forks Market hall, Winnipeg: open light flexible space for dining and events.



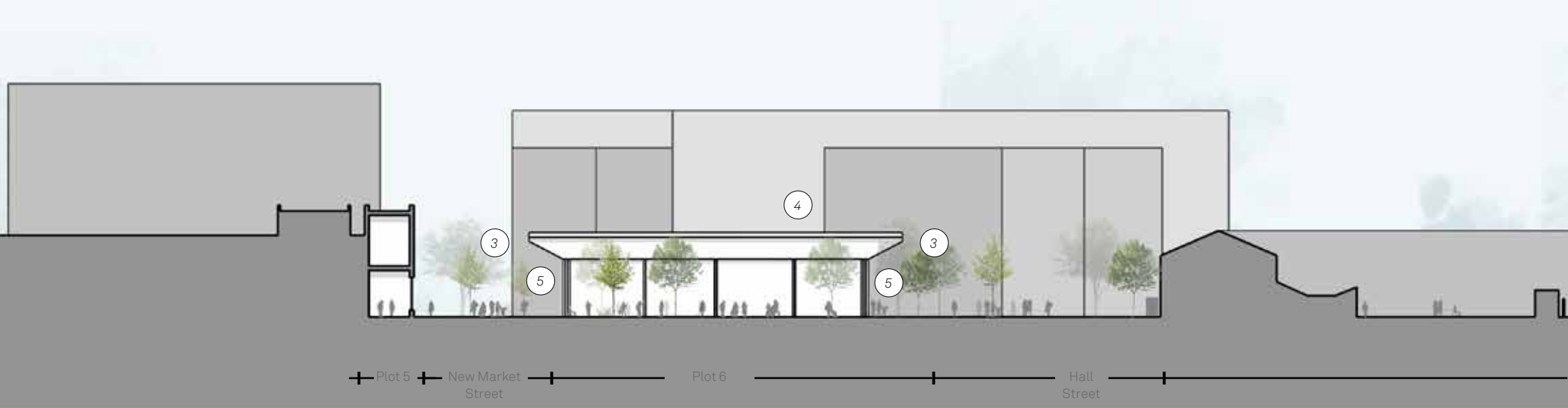
Link Market, Hong Kong: welcoming, open well lit stalls with clear signage.



Stockholm Market Hall, Tengbom Architects: light, open, warm and sustainable.



Section B, illustrative section showing maximum development heights



Section F, illustrative section showing maximum development heights

Materiality

The market is the jewel at the heart of the town centre by its use a unique typology. It can afford contrast with the predominantly brick character of the town centre, light weight steel, timber and glass, and at the same time pick up on historic cues and materiality such as the brick and stone.

All materials will have to be carefully chosen to complement the surrounding buildings.

The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Artists impression looking north towards Hall Street to Plot 6.



# 9.8 Development Plot 7, 8 & 9 Residential and Retail

80

Plots 7, 8 and 9 are located just west of the rail line and south of Church Street, the blocks front both Discovery Park and Hall Street South (Church Square). The streets between the plots linking the heart of the town centre to the new park replacing Chalon Way East. It creates a positive edge and new amenity for the town centre and new residence.

Fronting Hall Street South each of the blocks have retail, F&B & services at ground floor level with apartments above and potential to be taller at ground plus 5 storey to hold the edge of the square. Moving east towards the park the new residential streets are predominantly pedestrianised or have controlled vehicular access. The residential element is envisaged to be a mixture of 40% apartments and 60% town houses/ duplex apartments.

Plot 7 will Deliver:  
Retail/ F&B/ Services, use class Use Class E (a-f)  
Sui Generis (drinking establishments)  
90 Homes max  
34 Parking spaces max  
Plot 8 will Deliver:  
Retail/ F&B/ Services, use class Use Class E (a-f)  
Sui Generis (drinking establishments)  
90 Homes max  
30 Parking spaces max  
Plot 9 will Deliver:  
Retail/ F&B/ Services, use class Use Class E (a-f)  
Sui Generis (drinking establishments)  
88 Homes max  
26 Parking spaces max

## Brief Objectives

The 3 Plots provide a civic edge and scale to the east side of Church Square and Hall Street South with retail at ground floor. The potential for extra height compared to the rest of the block to give the blocks presence and hold the new street edge linking down to the World of Glass. Moving east between each of the blocks the streets link the heart of the town with Discovery Park. Town Houses activate the streets with front doors, windows and gardens, providing natural surveillance to the street and to the park. The park with children's play space creates a positive edge of the rail line and an amenity for the town houses and shoppers. Parking for each plot to potentially be delivered under a raised landscaped deck.

All residential buildings should follow and meet the targets set: ECF Sustainability Strategy

- Maximise reduction in carbon at the point of completion and

during operation.

- Enhanced Biodiversity -Delivering a net gain in biodiversity, whilst providing high quality green spaces
- Health and Wellbeing - Delivering high quality spaces where people can live, work and thrive while positively impacting their physical and mental health.
- Wider Sustainability - Capturing wider sustainability issues such as Water, Climate Adaptation, Transport & Certification
- Residential units to be Passivhaus Standards and where this conflicts with the Muse Sustainability Strategy the higher standard is to be adopted.
- Part M (90% Visit able & 10% adaptable)
- Achieve Building for Life Gold Standard.
- Follow the Lifetime Homes 16 Design Criteria.
- Nationally Described Space Standards.
- All residential units to be constructed using non-combustible



Plots 7,8,9 Illustrative Masterplan



Artists impression, aerial view looking north east across Church Square, plots 7 & 8 are visible on the right hand side.



# 9.8 Development Plot 7, 8 & 9 Residential and Retail

## Layout

- 1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
- 2. The development is expected to be delivered up to or close to the plot boundary to maximise interaction with the street and for consistent building line with the other plots.
- 3. The plot is in the round so every side should be considered as the primary frontage.
- 4. Car parking potentially concealed below a landscaped deck providing amenity to the homes.
- 5. Plot 9 a new backdrop to the World of Glass.

## Access and Movement

- 6. Main entrances on the primary elevations, streets with front doors.
- 7. Service access form the north and south streets connecting

Halls Street South with Discovery Park, allows the middle street to be car free.

- 8. Pedestrian routes are expected to circulate round the plot perimeter, the majority of the perimeter of the building are pedestrianised.
- 9. Cycle parking to be accommodate mainly on Hall Street South and the edge of Discovery Park.

## Massing

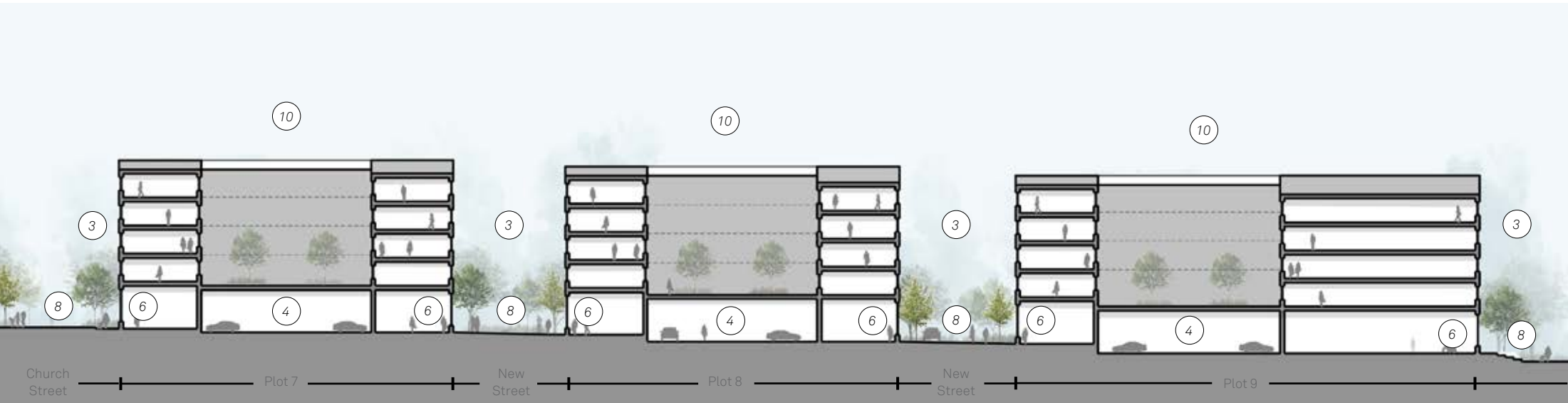
- 10. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the lift overrun and any roof plant.
- 11. Each of the plots are seen as being taller fronting Church Square then reducing to a more intimate scale then rising up again to front the Discovery Park.



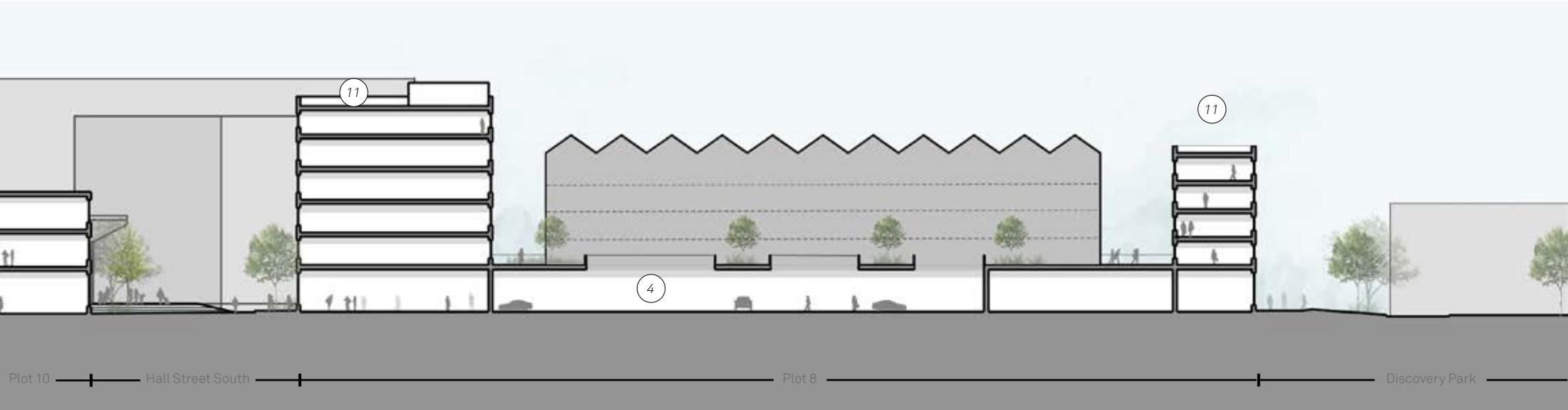
Timekeepers Square, town houses by Muse: Green streets and roof gardens.



Portobello Square, London: Lush planting and enticing public/private thresholds combine to create a pedestrian friendly street-scapes.



Section C, illustrative section showing maximum development heights



Section H, illustrative section showing maximum development heights

## Materiality

A simple palate of materials is anticipated with the predominant building material expected to be brick to reflect the wider town centre character. Brick choice will have to be carefully chosen to complement the surrounding buildings such as St Helens Church, The World of Glass, BT Building and St Georges Quarter.

The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Artists impression of Discovery Park looking towards Plots 7, 8 & 9.



# 9.9 Development Plot 10

## Retail & Office

82

Located on the southern edge of Church Square Plot 10 forms the backdrop to St Helens Parish Church. The block cranks forming the western edge of Hall Street South and the gateway from southeast corner of Church Square framing the view to the World of Glass. Very much bringing this key asset back into the town centre.

To the rear access needs to be maintained to the church and its hall. The building should be a minimum of 2 storeys fronting Church Square as it forms a key edge to a large civic space.

### Plot 10 will Deliver:

Ground floor retail/ F&B offer, use class E (a-f),  
Sui Generis (drinking establishments)  
Upper floor office, use class E (g)(i-ii)

### Brief Objectives

High quality affordable space with a high street address for possible local independent retail and F&B at ground floor with possible office space above.

The new building should meet St Helens Council/ ECF Sustainability Strategy.

### Layout

1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
2. The development is expected to be delivered up to or close to the plot boundary to maximise interaction with the street and for

consistent building line with the other plots.

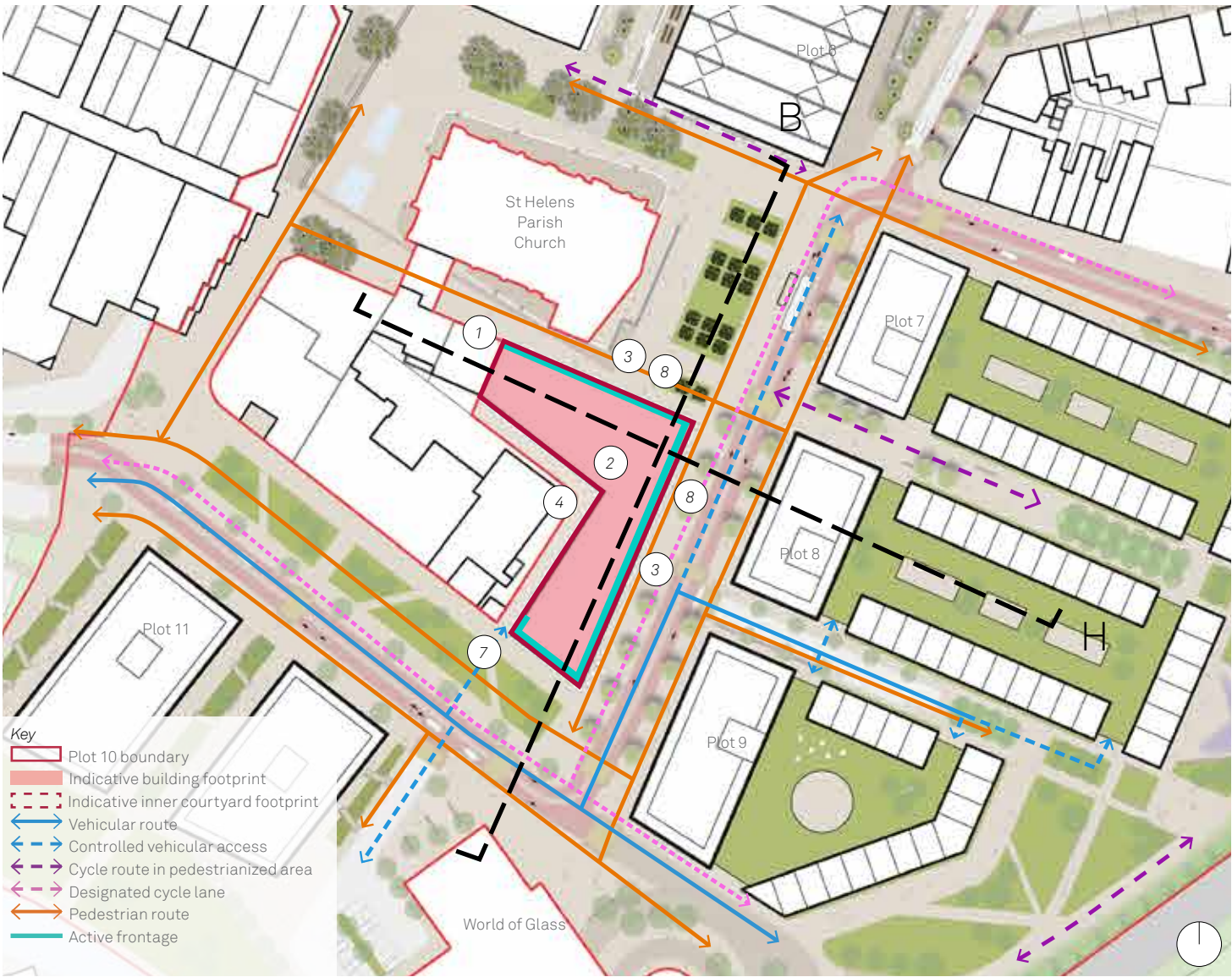
3. The primary frontage must face onto Church Square and Hall Street South.
4. The secondary frontage overlooks the BT block accessed by an alley.

### Access and Movement

5. Main entrances on the primary elevations for retail and offices.
6. Pedestrian routes are expected to circulate round 2 sides of the plot with controlled access to the rear.
7. Service access to be from Discovery Drive providing access to bin stores, cycle store and deliveries.
8. A zone in front of the retail units should be considered for seating and spill out.

### Massing

9. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the lift overrun and any roof plant.
10. The block should be of sufficient scale to hold Church Square and be the back drop to the Church.



Plot 10 Illustrative Masterplan



Thomas Street, Northern Quarter Manchester, JMA: A modern interpretation of a city centre terrace and warehouse, with active retail frontage.



9.9 Development Plot 10  
Retail & Office



Moors Nook, Coffey Architects: Traditional pitched roofs with textured brick facade.



Office, London, Morris & Co: Active shop frontage, contrast of old and new, simple clear palate of materials.



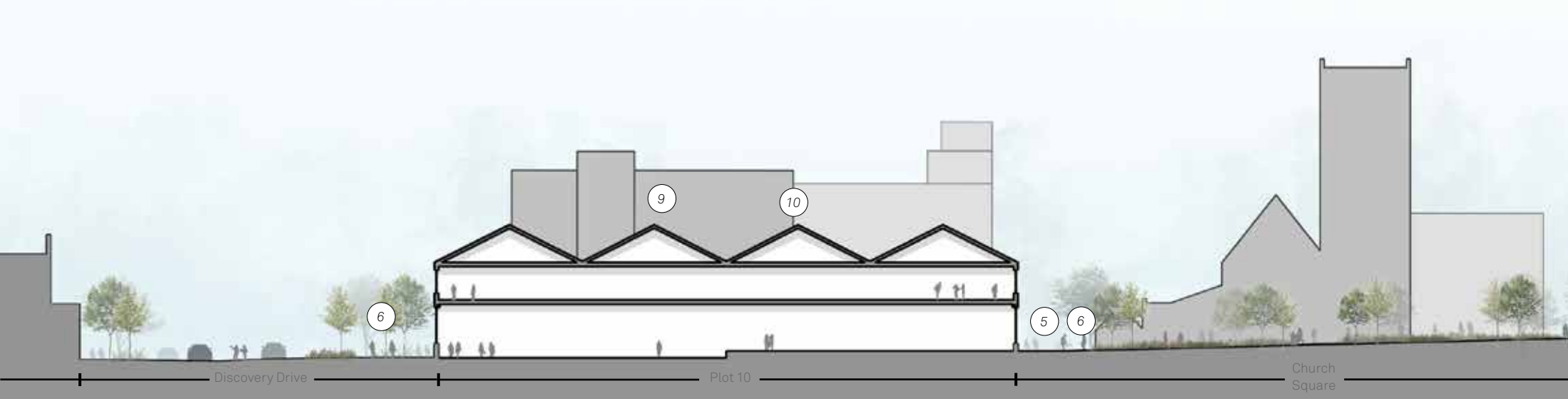
Offices/ Retail, Tariff Street Manchester: Simple palate of materials timber, wood and glass, open and active street frontage at every level.



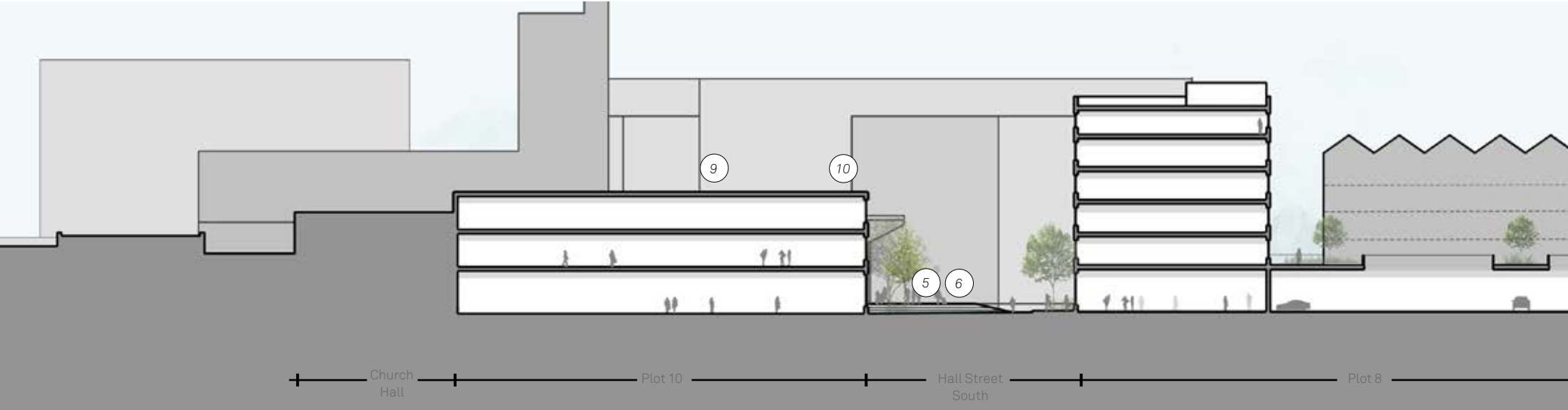
Timekeepers Square, town houses by Muse: Rooftop amenity also creates extra scale

**Materiality**  
A simple palate of materials is anticipated with the predominant building material expected to be brick to reflect the wider town centre character. Brick choice will have to be carefully chosen to complement the surrounding buildings given the proximity of the listed St Helens Church.

The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Section B, illustrative section showing maximum development heights



Section H, illustrative section showing maximum development heights



Social Housing, Tottenham, Morris Co Architects: contrasting scales.



9.10 Development Plot 11  
Residential/ Office

Located between St Helens Canal and Discovery Drive to the west of The World of Glass Plot 11 links the town centre back with the canal. The former site of the now demolished Chalon Way multi-storey car park. The plot forms a key active frontage with Discovery Drive and the canal and its tow-path.

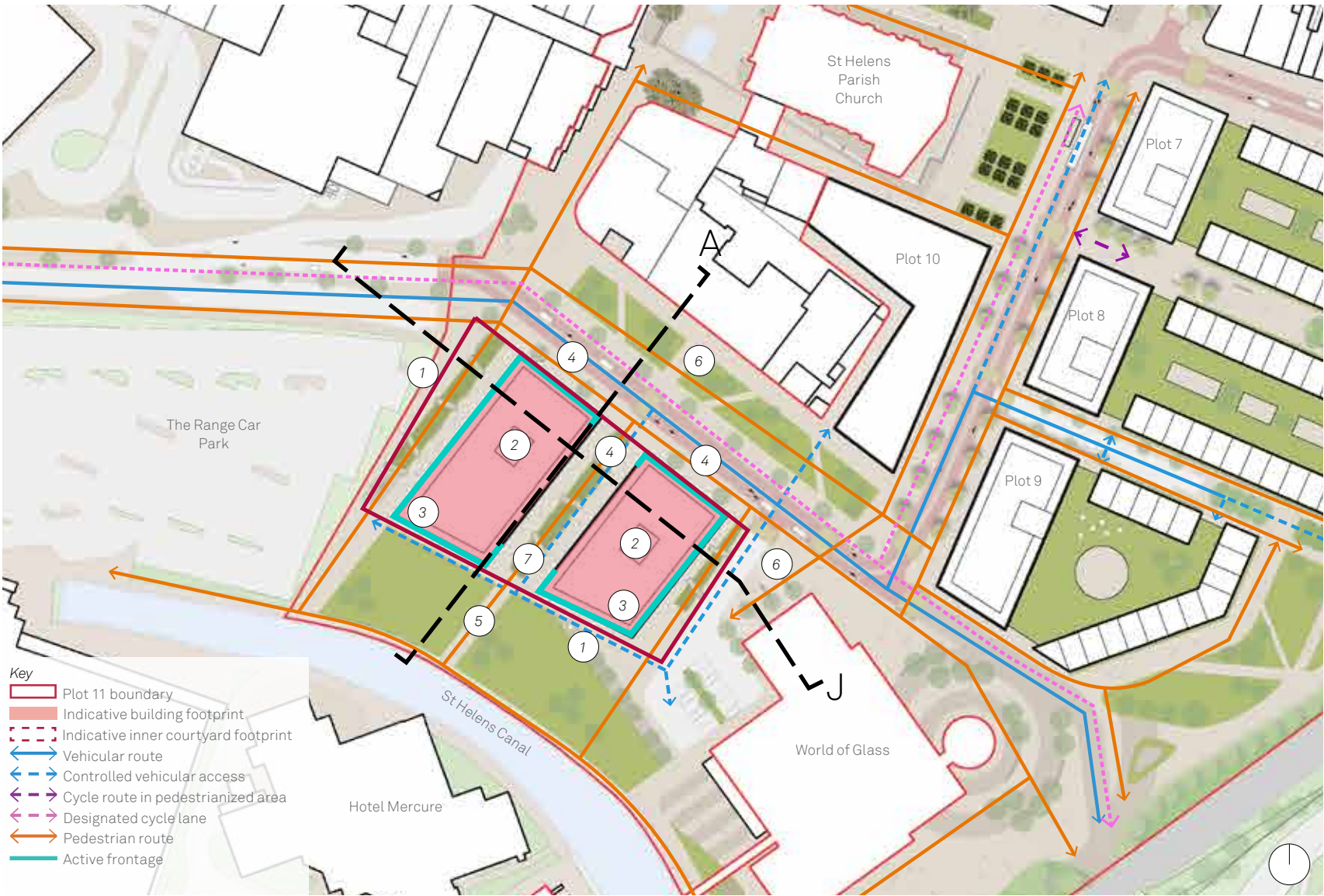
At ground floor it will contain some retail/ F&B offer the with residential or offices above. The plot should be broken down into a number of blocks to create the links through to the canal. The development height ranging from ground plus 2 to 5 storeys.

Plot 11 will Deliver:  
Retail/ F&B services at ground floor, use class E (a-f).  
Commercial office or Residential, use class , Use class E(g i-ii) or C3.  
45 Parking spaces.

- Brief Objectives**  
All residential buildings should follow and meet the targets set: ECF Sustainability Strategy
- Maximise carbon reduction at the point of completion and during operation.
  - Enhanced Biodiversity -Delivering a net gain in biodiversity, whilst providing high quality green spaces
  - Circular Economy - Eliminating waste and improving resource efficiency through circularity.
  - Health and Wellbeing - Delivering high quality spaces where people can live, work and thrive while positively impacting their physical and mental health.
  - Wider Sustainability - Capturing wider sustainability issues such as Water, Climate Adaptation, Transport & Certification
  - Residential units to be Passivhaus Standards and where

- this conflicts with the ECF Sustainability Strategy the higher standard is to be adopted.
- Part M (90% Visitable & 10% adaptable)
  - Achieve Building for Life Gold Standard.
  - Follow the Lifetime Homes 16 Design Criteria.
  - Nationally Described Space Standards.
  - All residential units to be constructed using non-combustible materials.
- Layout**
1. The parameter plot boundary sets out the maximum building line allowance for the development. The building footprint should remain entirely within the parameter plot boundary.
  2. The development could be broken down into a number of

- permeable blocks linking Discovery Drive with the Canal.
3. The plot is in the round but the primary frontage are seen to face onto Discovery Drive and St Helens Canal.
- Access and Movement**
4. Main entrances on the primary elevations.
  5. Pedestrian routes are expected to circulate round and through the plot perimeter, the majority of the perimeter of the building are pedestrianised.
  6. Cycle lane and parking to be accommodate along Discovery Drive and adjacent to the World of Glass.
  7. Service access to be from between the blocks, providing access to bin stores, cycle store and deliveries.



Plot 11 Illustrative Masterplan



The Crescent, Manchester, JMA: Active street frontages attention to detail.



9.10 Development Plot 11  
Residential/ Office

Massing

8. Building mass must not exceed the maximum parameter allowance set in the Parameters Plans, measured above Ordinance Survey Datum. The maximum height shown takes into account the lift overrun and any roof plant.



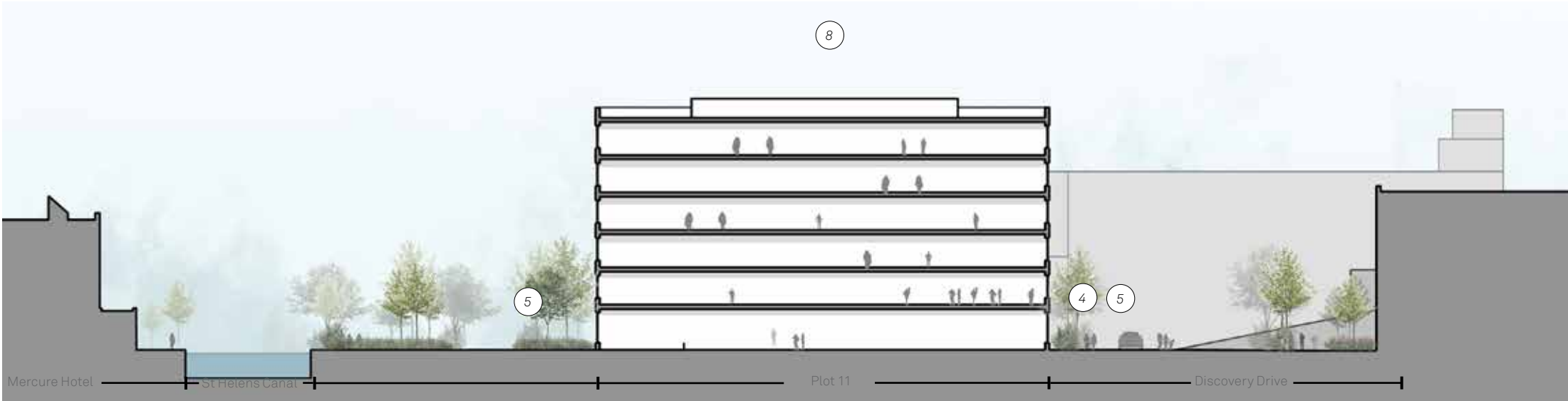
Turnmill, London, Piercy & Co Architects: Celebrating the corner, facade depth and detail.



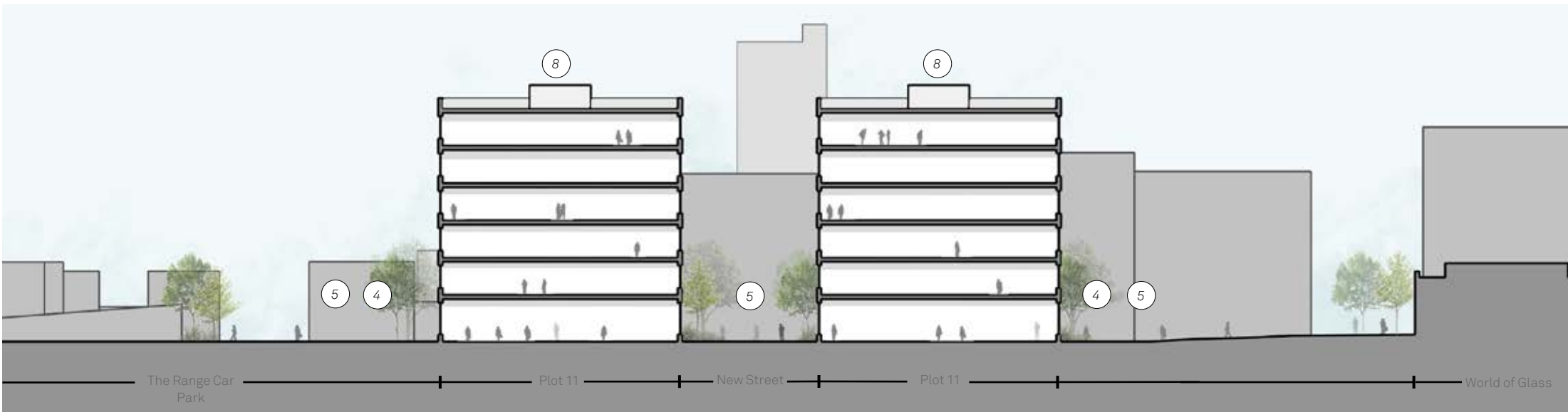
Brentford Lock West, Duggan Morris Architects: Animation of the skyline and facade with balconies, providing amenity space.



Blackfriars Trinity, Manchester, JMA: Town houses with apartments above.



Section A, illustrative section showing maximum development heights



Section J, illustrative section showing maximum development heights

Materiality

A simple palate of materials is anticipated with the predominant building material expected to be brick to reflect the wider town centre character. Brick choice will have to be carefully chosen to complement the surrounding buildings such as the The World of Glass and BT block.

The public realm materials will align with the materiality of the rest of the masterplan area to ensure consistency across the town centre.



Housing, Niall McLaughlin Architects: Facade depth

## 10.0 Conclusion



The principles in this document set out the vision and parameters to deliver a sustainable, mixed-use community, providing new homes and employment space along with associated leisure and amenity to serve this emerging destination and create a sustainable thriving town centre.

The masterplan has been developed around a number of zones or key character areas, opportunity sites, public realm and transport and movement considerations, as well as the consideration of specific urban design principles within each zone. The proposal respects the heritage of St Helens, it repairs post war damage to the town centre whilst respecting historic street patterns and re-activating streets and spaces.

One of the moves that will deliver this rationalisation of north/south and east/west permeability to create a clear legible street pattern that links up the towns cultural and historic assets. This will be facilitated by the demolition of the Hardshaw Shopping Centre and St Mary's Shopping Centre and Market.

The north south permeability connecting the civic and heritage area with new bus station through the central retail area and beyond to the St Helens Canal is achieved by:

- The creation of New Market Street linking Bickerstaffe Square (bus station/ transport hub) with St Helens Church tower it creates a key wayfinding point drawing people into the heart of the town.
- Hall Street is given back its residential character and scale with active street frontages.
- Hall Street is extended down to the World of Glass becoming Hall Street South, putting this key asset back in the town centre, not hidden behind St Mary's Shopping Centre.
- The de-engineering of Chalon Way and its junction with Church Street allows the creation of Discovery Park. Effectively extending Shaw Street south to the St Helens Canal. A key route to the Totally Wicked Stadium.

The east west permeability linking up key transport nodes and beyond is achieved by:

- A new Train Station Plaza welcomes you to St Helens, a place to breath and take stock of your surroundings. The landscape extending west along Bickerstaffe Street, improving wayfinding and linking Victoria Square via the bus station.
- The pedestrianization of Bickerstaffe Street alongside the new bus station creates a place to dwell. Linking directly with New Market Street and Hall Street, and Hall Street South beyond.
- Claughton Street previously only gave you access to the rooftop parking it now extends east past the new market hall through to the George Street Quarter.
- 2 new streets extend east from Church Square to Discovery Park, linking up this key new asset and green corridor running north south.

Secondly the re-population of the town centre with high quality housing is critical to establishing a sustainable community economically, socially and environmentally. New residential developments are placed in key locations:

- Hall Street is given back its residential character with new town houses, previously the blank facade of the shopping centre.
- Apartments overlook New Market Street and the bus station activating the streets and provides passive surveillance.
- A hotel overlooking the bus station also provided passive surveillance.
- Apartments overlook Church Square and Hall Street South.
- Town houses extend back from Church Square linking and overlooking Discovery Park.
- New apartments enjoy the St Helens Canal and Discovery Drive.

Thirdly the rationalisation of the oversupply and underutilised retail units will create space for modern exciting retail and leisure businesses. The new market located at the heart of the town will accommodate existing operators with modern exciting food court. De-engineering Chalon Way into a new Discovery Park will resoundingly change the environment of the eastern edge of the town.

All development coming forward within the application site will adhere to planning policy and consider the key design principles set out:

- Create positive active frontages throughout the town centre, such as leisure, retail and residential..
- Open up the desire lines improving natural wayfinding and maximising existing landmarks and focal points.
- Create opportunities to provide experiences through a range of placemaking activities and events which animate the spaces and create interest.
- Improve and link public space, historic, cultural buildings, the canal and existing positive streets.
- Increase the footfall throughout the town centre.
- Create new and improved dwell spaces.
- Create a density that is appropriate to a town centre location.
- Draw upon the heritage analysis of the site and street patterns, seeking opportunities to restore the historic street grain pattern with a network of interconnecting shared surface streetscapes.
- Consolidate car parking supply to make car parks more efficient, reduce circulating vehicles in the town centre and promote sustainable travel.
- Adhere to latest guidance on 'Building Better, Building Beautiful'.

Key

Application Boundary

1

Plot Number



Illustrative Masterplan

# 11.0 Appendix

## Shadow Study Analysis



11.1 Existing Site  
Summer Solstice

21 June 9AM

JMA have undertaken a shadow study as part of the deign development based on the maximum and minimum proposed building heights which we are presenting as an appendix to this document. The existing town centre shadow study forms the baseline for the assessment/ comments. As the design of the plots develop in more detail a more detailed sunlight and daylight analysis maybe required by a specialist consultant to full test the designs.

The testing times for the model are the summer solstice (June 21st) and the Winter Solstice (21st December) at 9am, 12pm and 3pm. The 21st June the first official day of summer the sun is at its highest in the sky and the shadows are at their smallest on this day.

The streets are generally in sunlight except where the streets run in a north-easterly direction they are overshadowed by roughly between 25 and 75%. Barrow Street, a historic street, with 2 to 3 storey buildings is almost fully in shade. Contrasting this Hall Street is barely 25% in shadow but the street width is much wider, this width is not characteristic of the town centre. Its western edge being formed by the blank facade of the Hardshaw Shopping Centre which doesn't respect the historic street pattern.

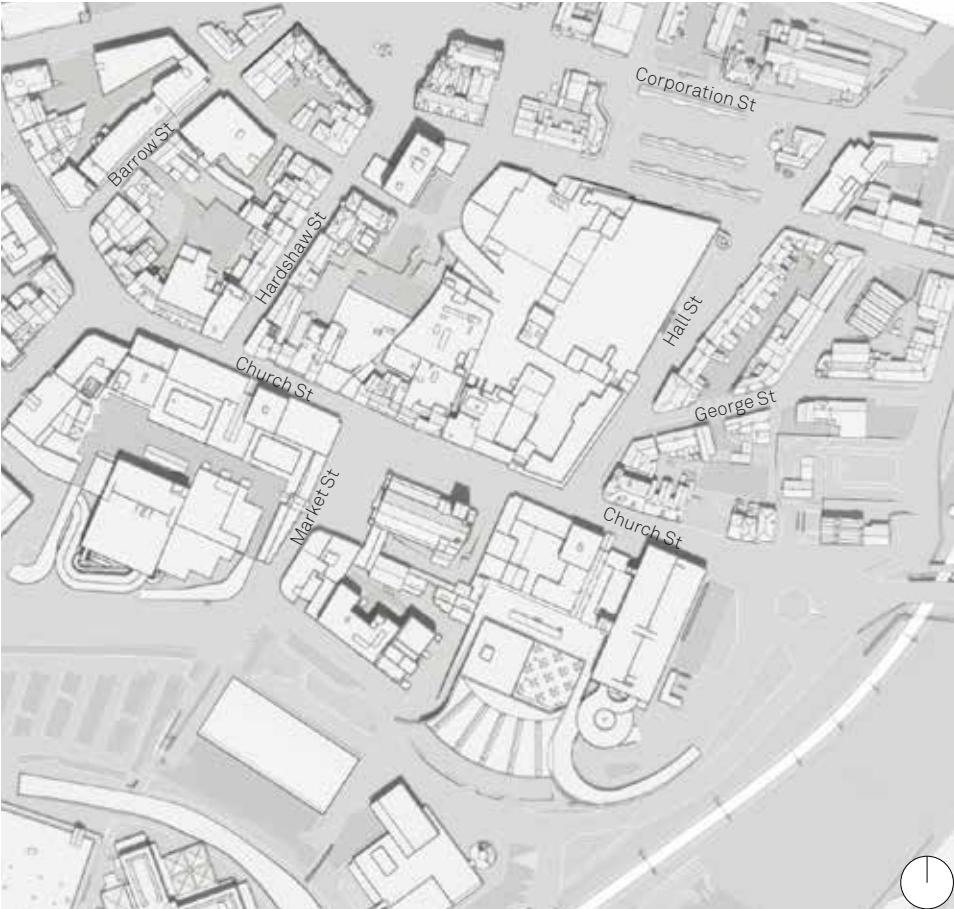
Key  
1 Plot Numbers



21 June 12PM

The sun at this time of year and day is its highest point so the shadows caste are at their smallest.

Just a small sliver of a shadow is cast to the north of the buildings with the streets in full sunlight. The shadow of the St Mary's Church just being visible reaching north towards Church Street.

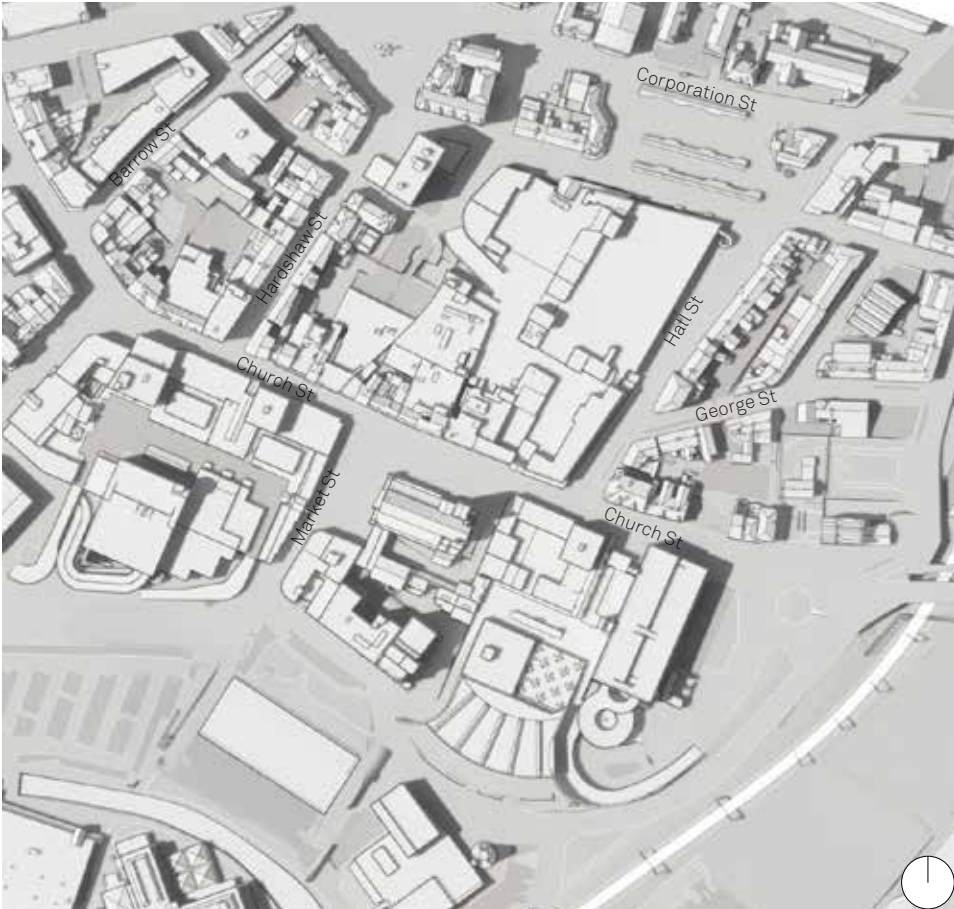


21 June 3PM

The streets are generally in sunlight except where the streets run in a north-easterly direction they are overshadowed by roughly between 15 and 60%.

Barrow Street, a historic street, with 2 to 3 storey buildings is 50% in the shade on average. Contrasting this Hall Street is barely 15% in shadow but the street width is much wider and not characteristic of the town. Its western edge being formed by the blank facade of the Hardshaw Shopping Centre which doesn't respect the historic street pattern.

The shadows of the taller buildings such as the Gamble Institute, Century House, St Mary's multi-storey car park and St Helens Parish Church Tower stand out against the unusually low rise town centre.





## 11.2 Maximum Proposed Height Summer Solstice

21 June 9AM

The sun overshadowing analysis is for the maximum height of the development plots. The extent of the overshadowing is worse than would be expected as the maximum height (top storey) is to accommodate lift overruns and roof top plant which would be set back from the building edge. In addition, the study is based on the development encompassing the maximum extent of the development plots, which is a worse case scenario. The development is unlikely to occupy the full extent of the plot parameters, particularly for Plots 2 and 11.

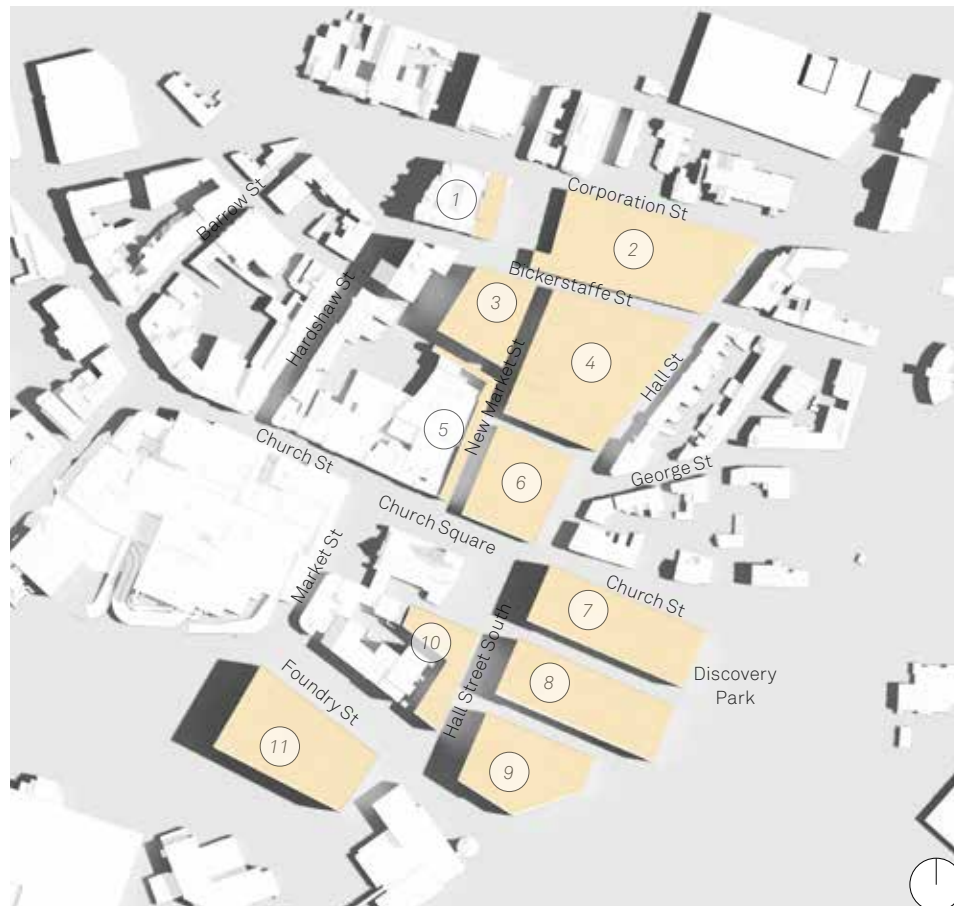
Barrow Street and Hardshaw Street a good reference point for overshadowing at this time of day for the centre of St Helens because of its historic scale and orientation running northeast, it is almost fully in shadow.

New Market Street on the same orientation is also in full shade. Similarly Hall Street South is almost fully in shade broken up by the sun flooding right across the street from down the roads linking it with Discovery Park.

The increase in the size of Church Square at its eastern edge means there is significantly more square which isn't overshadowed.

Key

1 Plot Numbers



21 June 12PM

The sun at this time of year and time of day is its highest point so the shadows cast are at their smallest.

The new blocks south of Bickerstaffe Square overshadow the street by just over 50% with the rest of the square being in full sunshine. The extent of the overshadowing is similar for the streets connecting Hall Street South with Discovery park.



21 June 3PM

The overshadowing of New Market Street at this time of day is similar to at 9am but this time is fully in the shade at its northern end rather than at its south.

The shadows the buildings cast along Bickerstaffe Street extend to the edge of plot 2. It should be noted that the street is narrower in the plot plan it will be 2 to 3m wider as set out in the public realm parameters plan.

The northern end of Hall Street is now fully in shadow as the width has been reduced to mirror the street widths of the George Street Quarter, giving it back its residential character. As you move south past the market hall into Church Square and Hall Street South the roads show very little overshadowing.

At this time of day the streets connecting Halls Street South with Discovery Park are 75% overshadowed leaving the southerly elevations of the blocks bathed in light.

Discovery Park is only slightly effected by overshadowing of the 3 residential blocks.





11.3 Minimum Proposed Height  
Summer Solstice

21 June 9AM

The extent of the overshadowing for the minimum heights is not significantly different than that of the maximum plot heights.

New Market Street is only 50% over shadowed at its southern end but at its northern end towards the bus station is fully in shade, as it is for the maximum heights.

Hall Street South the shadows extend about 50% across the street with the sun breaking this up by flooding down the adjoining streets linking through to Discovery Park.

The increase in the size of Church Square at its eastern edge means there is significantly more square unaffected by overshadowing.

21 June 12PM

The sun at this time of year and time of day is its highest point so the shadows caste are at their smallest.

Plots 3 and 4 south of Bickerstaffe Square overshadow the street by just 30% with the rest of the square being in full sunshine. The extent of the overshadowing is similar for the streets connecting Hall Street South with Discovery park.

21 June 3PM

The overshadowing of New Market Street at this time of day is similar to at 9am but this time is fully in the shade at its northern end rather than at its south.

The shadows the buildings cast along Bickerstaffe Street extend 75% across the street almost reaching the bus station canopy (Plot 2).

The northern end of Hall Street is just less than 40% over shadowed, even with the width having been reduced to mirror the street scale of the George Street Quarter, giving it back its residential character.

As you move south past the Plot 6 into Church Square and Hall Street South the roads show very little overshadowing.

At this time of day the streets connecting Halls Street South with Discovery Park are less than 50% overshadowed.

Key  
1 Plot Numbers





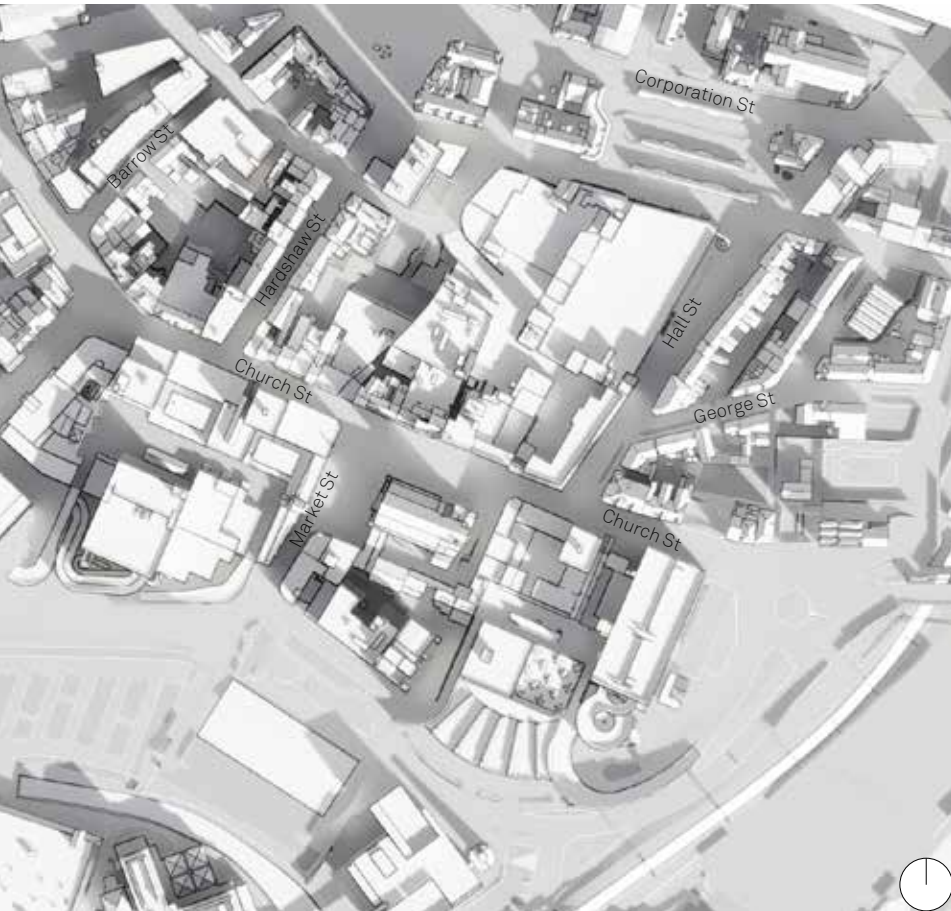
11.4 Existing Site  
Winter Solstice

21 December 9AM

The shortest day of the year when the sun is at its lowest in the sky and the overshadowing at is maximum.

The majority of the streets can be seen to be fully overshadowed its only the open spaces such as the bus station (proposed Bickerstaffe Square), a small part of Church Square and Chalon Way which remain in sunlight.

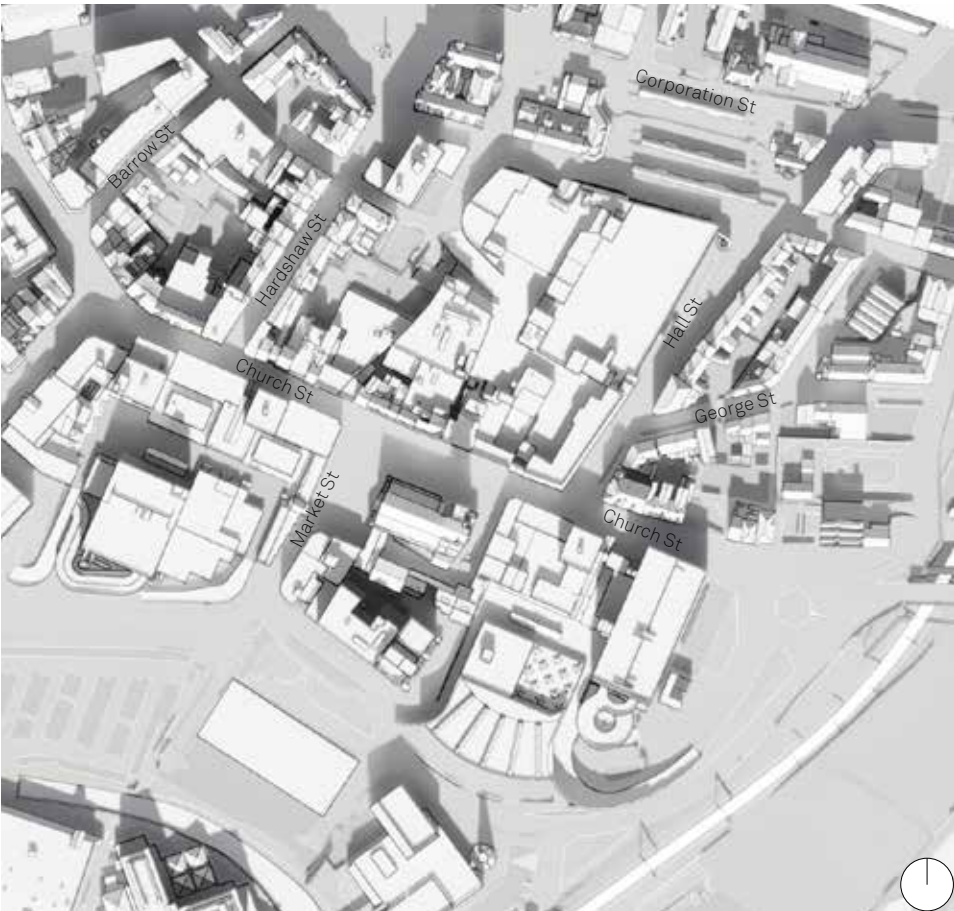
Key  
1 Plot Numbers



21 December 12PM

The sun at its highest point in the day for this time of year yet the streets are still significantly overshadowed, Church Square is roughly 50% in the shade while Chalon Way East and the Bus Station (proposed Bickerstaffe Square) remain largely unshaded.

Barrow Street, Church Street and Bickerstaffe Street are fully in the shade while Hall Street remains 50% unshaded largely due to its uncharacteristic width.

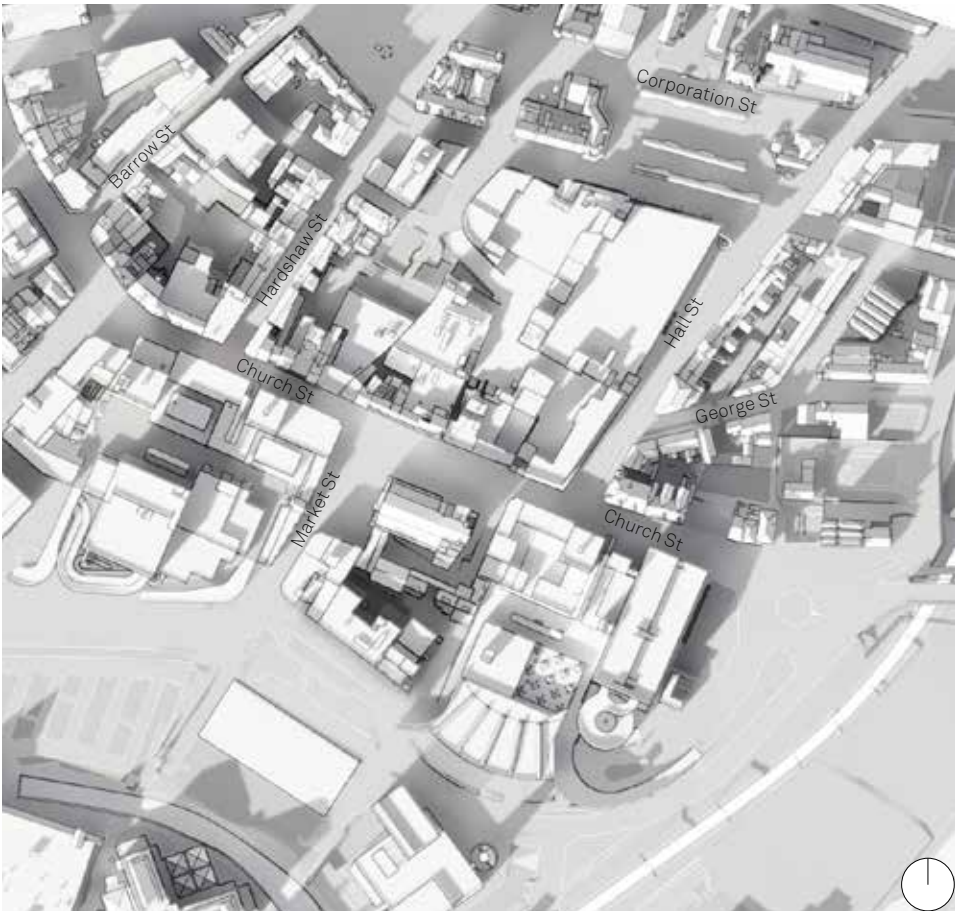


21 December 3PM

The streets running east west are entirely overshadowed even the larger open spaces such as the bus station are in shade due to the shadows cast by the Hardshaw Centre which extend all the way to Corporation Road. Similarly the shadow cast by St Helens Church extends all the way across church square.

The streets running north east provide some relief to the overshadowing of these larger open spaces.

Chalon Way East remains in sunlight little effected by the time of year and surrounding buildings.





11.5 Maximum Height Proposed  
Winter Solstice

21 December 9AM

The extent of the overshadowing is worse than would be expected as the maximum height (top storey) is to accommodate lift overruns and roof top plant which will be set back from the roof building edge. In addition, the study is based on the development encompassing the maximum extent of the development plots, which is a worse case scenario. The development is unlikely to occupy the full extent of the plot parameters, particularly for Plots 2 and 11.

The majority of the streets can be seen to be fully overshadowed its only the open spaces that catch some sun. The exception being Discovery Park which catches the full effect of the morning sun as will the new residential blocks bounding the edge of the park, plots 7,8 & 9.

The extent of the overshadowing of Bickerstaffe Square (Bus Station) looks worse than it will be in reality, The bus station being 2 liner lightweight canopies will allow the sun into the square.

Key  
1 Plot Numbers

Church Square is mainly in shadow the extra width to the square matched by the extra height of the proposed plots that hold the edge of the square.



21 December 12PM

The sun at its highest point in the day for this time of year yet the streets are still significantly overshadowed, Church Square is roughly 50% in the shade while Discovery Park remain unaffected.

New Market Street, Hall Street South and Hall Street whilst overshadowed are still penetrated by the sun at this time of day despite the extra building plot heights.

The extent of the over shadowing in Church Square is little effected due to the increase in its size.

The roads linking Discovery Park to Hall Street South are overshadowed with the shadows extending up the southern elevations of the blocks.

The shadow cast across Foundry Street from the canal side Plot 11 stretches all the way to the building line. If the building is broken down into separate blocks to achive better connections with the canal the overshadowing will be broken up.



21 December 3PM

The streets running east west are entirely overshadowed even the larger open spaces such as the bus station are in shadow, the shadows cast by Plots 3 and 4 extends all the way to Corporation Street.

Similarly the shadow cast by St Helens Parish Church extends all the way across church square. The streets running north east provide some relief to the overshadowing of these larger open spaces.

Discovery Park remains in sunlight little effected by the time of year and surrounding buildings.

The shadow cast across Foundry Street from the canal side Plot 11 stretches all the way to the building line. If the building is broken down into separate blocks to achive better connections with the canal the overshadowing will be broken up.





11.6 Minimum Height Proposed  
Winter Solstice

21 December 9AM

The over shadowing is not significantly different than the maximum heights for this time of year due to the low angle of the sun.

The majority of the streets can be seen to be fully overshadowed its only the open spaces that catch some sun. The exception being Discovery Park which catches the full effect of the morning sun as will the new residential blocks bounding the edge of the park.

The extent of the overshadowing of Bickerstaffe Square (Bus Station Plot 2) looks worse than it will be in reality, The bus station being 2 liner lightweight canopies will allow the sun into the square.

Church Square is mainly in shadow the extra width to the square matched by the extra height of the proposed plots that hold the edge of the square.

21 December 12PM

The sun at its highest point in the day for this time of year yet the streets are still significantly overshadowed, Church Square is roughly 50% in the shade while Discovery Park remain unaffected.

New Market Street, Hall Street South and Hall Street whilst overshadowed are still penetrated by the sun at this time of day despite the extra building plot heights.

The extent of the over shadowing in Church Square is little effected due to the increase in its size.

The roads linking Discovery Park to Hall Street South are overshadowed with the shadows extending up the southern elevations of the blocks.

The shadow cast across Foundry Street from the canal side Plot 11 stretches to discovery drive leaving the adjacent buildings unaffected.

21 December 3PM

The streets running east west are entirely overshadowed even the larger open spaces such as the bus station the shadows cast by the hotel and office extends all the way to Corporation Street.

Similarly the shadow cast by St Helens Parish Church extends all the way across church square. The streets running north east provide some relief to the overshadowing of these larger open spaces.

Discovery park remains in sunlight little effected by the time of year and surrounding buildings.

The shadow cast across Foundry Street from the canal side Plot 11 stretches all the way to the building line. If the building is broken down into separate blocks to achive better connections with the canal the overshadowing will be broken up.

Key  
1 Plot Numbers





Simple, elegant design.